

Planning applications committee

Date: Thursday, 13 October 2016

Time: 10:30

Venue: Mancroft room, City Hall, St Peters Street, Norwich, NR2 1NH

Committee members:

Councillors:

Herries (chair)
Driver (vice chair)
Bradford
Button
Carlo
Henderson
Jackson
Lubbock
Malik
Peek
Sands (M)
Woollard

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Informal briefing

Please note that there will be an informal briefing/discussion on governance issues for members of the committee at 9:30 in the Mancroft room.

Information for members of the public

Members of the public and the media have the right to attend meetings of full council, the cabinet and committees except where confidential information or exempt information is likely to be disclosed, and the meeting is therefore held in private.

For information about attending or speaking at meetings, please contact the committee officer above or refer to the council's website



If you would like this agenda in an alternative format, such as a larger or smaller font, audio or Braille, or in a different language, please contact the committee officer above.

Agenda

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To receive apologies for absence	
2 Declarations of interest	
(Please note that it is the responsibility of individual members to declare an interest prior to the item if they arrive late for the meeting)	
3 Minutes	5 - 26
To agree the accuracy of the minutes of the meetings held on 8 September 2016 and 22 September 2016.	
4 Planning applications	
Please note that members of the public, who have responded to the planning consultations, and applicants and agents wishing to speak at the meeting for item 4 above are required to notify the committee officer by 10:00 on the day before the meeting.	
Further information on planning applications can be obtained from the council's website: http://planning.norwich.gov.uk/online-applications/	
Please note:	
<ul style="list-style-type: none">• The formal business of the committee will commence at 10.30;• The committee may have a comfort break after two hours of the meeting commencing.• Please note that refreshments will not be provided. Water is available• The committee will adjourn for lunch at a convenient point between 13:00 and 14:00 if there is any remaining business.	
Summary of planning applications for consideration	27 - 28

	Standing duties	29 - 30
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Date of publication: **Thursday, 06 October 2016**

Planning applications committee
09:30 to 15:40
8 September 2016

Present: Councillors Herries (chair), Driver (vice chair), Ackroyd (substitute for Councillor Lubbock), Bogelein (substitute for Councillor Henderson) (to the middle of item 18, below), Bradford (to the end of item 9 below), Button, Carlo, Jackson, Malik, Peek, Sands (M) and Woollard

Apologies: Councillors Lubbock and Henderson

1. Declarations of interest

Councillor Jackson declared a predetermined view in item 5 (below), Application no 16/00699/F - 36 - 42 Duke Street, Norwich, NR3 3AR because he had commented on the application.

Councillor Carlo declared that she had a predetermined view in item 7 (below) Application no 16/00928/U - 145 and 147 Earlham Road, Norwich, NR2 3RG because she had commented on the application.

Councillor Bogelein declared a pecuniary interest in item 10 (below), Application no 16/00835/F - 120 - 130 Northumberland Street, Norwich, NR2 4EH because she lived in the street.

2. Minutes

RESOLVED to agree the accuracy of the minutes of the meeting held on 11 August 2016.

3. Application no 16/00790/F - 30 All Saints Green, Norwich, NR1 3NA

The policy team leader (projects) presented the report with the aid of plans and slides and an animated presentation displaying the proposed building in context with its surrounding buildings and varying views, including at street level. She also referred to the supplementary report of updates to reports, which was circulated at the meeting and contained a summary of two further letters of representation and an amendment to paragraph 8 of the main report.

A representative of Aviva addressed the committee and said that the company supported the principle of development on this site but was concerned about the proposed height of the building which was considered to be harmful to the street-scene and the townscape. The height should be reduced from fourteen storeys to

eleven storeys to align the proposed development with the Aviva building. There was no evidence of financial viability to justify the height of the building. Historic England and the Norwich Society also considered that the proposed building should be reduced in height by several storeys. Aviva was also concerned about the site specific impact of the development on its office buildings which included loss of sunlight, daylight and outlook.

The managing director of the development company explained that it specialised in the provision of high quality student accommodation. He explained the economic benefits to the city in providing purpose built accommodation for students of the city's higher educational institutions and that this alleviated pressure on family houses in the residential areas of the city. He explained that there were thirteen storeys and that daylight/sunlight studies demonstrated a minimal impact on the Aviva building. Construction would commence in early 2017 for delivery in 2018.

During discussion the planning policy team leader (projects) referred to the report and responded to the issues raised by the speakers and answered members' questions. Members considered the viability of the site, the effect of reducing the height of the building and sought clarification that an affordable housing contribution was not applicable to this application. The committee noted that the development of a vacant brownfield site would be beneficial to the streetscene and enhance the adjacent listed buildings. Reassurance was given that parking for the disabled would not be lost but would be repositioned. Members were also advised that ground source heat pumps had been considered as a form of renewable energy but this was not considered feasible due to the nature of the site.

Discussion ensued in which members commented on the application. Members generally considered that concerns about the height of the building and its impact on the streetscene were outweighed by the need to provide good quality accommodation for students and by bringing a brownfield site back into use.

Councillor Bradford said that he was concerned about the sustainability of the use for students when there was a demand for good quality housing in the city centre and that he was concerned about the mass and height of the building.

RESOLVED with 11 members voting in favour (Councillors Herries, Driver, Ackroyd, Bogelein, Button, Carlo, Jackson, Malik, Peek, Sands and Woollard) and 1 member abstaining (Councillor Bradford) to approve application no. 16/00790/F - 30 All Saints Green Norwich NR1 3NA and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Contamination
4. Unknown contamination
5. Imported material
6. Archaeological written scheme of investigation
7. Materials
8. Details to be agreed of materials including doors, windows, shopfronts, rainwater goods.
9. Lighting

10. Fire hydrants
11. Disabled access
12. Boundary treatment
13. Obscure glazing
14. Heritage interpretation / public art
15. Energy efficiency
16. Water efficiency
17. SuDS details submission and implementation
18. Landscaping details
19. Landscape provision
20. Street trees
21. Parking / servicing
22. Provision of cycle parking and bin storage
23. TRO required
24. Removal of permitted development rights – ground floor uses
25. Removal of permitted development rights – details of plant and machinery
26. Restricted delivery hours
27. Construction method statement
28. Provision of litter bins and waste collection facilities
29. Travel plan
30. Arboricultural works to facilitate development
31. Details of management arrangements to be agreed
32. S278 agreement

Informatives

1. Construction working hours
2. Asbestos
3. Landscape management plan

Article 35(2) Statement

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined in the officer report.

4. Extraordinary meeting of the planning applications committee – 22 September 2016 at 12 noon

In reply to a question, the planning team leader (development) (outer area) confirmed that the Lead Local Flood Authority would comment on the planning application prior to the date of the meeting.

RESOLVED to hold an extraordinary meeting of the planning applications committee on Thursday, 22 September 2016 at 12 noon in the council chamber to consider Application No 15/01928/F – St Peters Methodist Church, Park Lane, Norwich, following a site visit at 10:30 to the application site.

5. Application no 16/00699/F - 36 - 42 Duke Street, Norwich, NR3 3AR

(Councillor Jackson having declared a pre-determined view, left the meeting at this point and did not take part in the determination of this item.)

The senior planner (development) presented the report with the aid of plans and referred to the supplementary report of updates to reports which was circulated at the meeting and contained a summary of further letters of representation that had been received in objection to the scheme and a consultation response from Heritage England.

Councillor Jackson, Mancroft ward councillor, addressed the committee on behalf of residents. He said that he was concerned that a number of objectors had not known when the meeting would be considered and that it was essential that residents had the opportunity to speak at meetings and that the committee should defer consideration of this meeting and hold a site visit. He then listed the residents' concerns to the proposed development which included concerns about overlooking; that it was overdevelopment and was detrimental to the character of the conservation area; and concerns about the pavement being too narrow and that driver visibility would be impaired.

The agent addressed the committee in support of his clients' application. He explained that the design of the scheme was sympathetic to the conservation area and adjacent listed buildings; no building would be above three storeys and would be of traditional design and of sustainable materials. The proposal had been amended to mitigate concerns about overlooking from adjacent residents.

(Councillor Jackson left the meeting at this point.)

The planning team leader explained that the council acknowledged comments to planning applications and advised members of the public how to track applications. The council did not have the resources to contact people to advise them when an application came to committee. The report and presentation provided adequate information for the committee to make a decision. The Duke Street site was an open and accessible site which members could have visited.

During discussion the senior planner referred to the report and answered member's questions and the comments from the speakers. There would be a brick wall with railings on Duke Street, but the pavement width would remain the same. The application could not be used to put right a poor situation. The Highways Agency did not object to the vehicle access/egress arrangement which was on to a one-way street. The scheme would be of good quality materials comprising red brick and pantiles, with stepped back ridge heights, which would enhance the conservation area. In reply to a question, she explained that main issue 3 was incorporated into main issue 4 of the report.

Members commented that the scheme provided much needed housing for the city and was sympathetic to the conservation area.

RESOLVED, unanimously, to approve application no. 16/00699/F - 36 - 42 Duke Street Norwich NR3 3AR and grant planning permission, subject to the completion of a satisfactory legal agreement and subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Details to include: materials to be used in external construction of development (including samples), external walls and railings, all external joinery and fenestration including rooflights, rainwater goods, infilling of openings on western side of the site;
4. Landscaping scheme including all soft and hard landscape, boundary treatments, finished site levels and management measures;
5. Works to be undertaken in accordance with the protection measures as outlined in the submitted arboricultural report;
6. Construction Method Statement;
7. Solar panels;
8. Parking, EV charging and cycle/ bin storage details;
9. Obscure glazing of windows in the south elevation as shown on plan reference 4876 C received on 05/08/2016 to be permanently retained in that form;
10. Noise mitigation measures in accordance with the submitted noise report;
11. Contamination measures;
12. Travel Plan;
13. Water efficiency;
14. Lifetime homes; and
15. Archaeology.

Informatives:

1. Protection of noise from balconies.
2. Note to remind the use of permeable paving in courtyard to assist with surface water drainage.

Article 35(2) Statement

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined in the officer report.

(Councillor Jackson was readmitted to the meeting at this point.)

6. Application no 16/00536/F - 5 - 9 Haymarket, Norwich, NR2 1QD

The senior planner (development) presented the report with the aid of plans and slides. She explained that the works would ensure that there was no impact on the undercroft of the adjacent building.

During discussion the senior planner referred to the report and answered members' questions. A member referred to the Norwich Society's comments and said that whilst he agreed that there could have been a better designed building it was an

improvement on the existing “ugly façade”. Members commented on the creation of one very large store rather than two retail stores and its sustainability in the future.

RESOLVED, unanimously, to approve application no. 16/00536/F - 5 - 9 Haymarket Norwich NR2 1QD and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Materials;
4. Cycle storage;
5. Energy/Air source heat pumps;
6. Refuse and servicing arrangements;
7. The flood risk measures as outlined in the submitted FRA;
8. Archaeology condition – Written Scheme of Investigation with monitoring of works.

Article 35(2) statement

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined in the officer report.

7. Application no 16/00928/U - 145 & 147 Earlham Road, Norwich, NR2 3RG

(Councillor Carlo, having declared a pre-determined view, left the meeting at this point and did not take part in the determination of this item.)

The senior planner (development) presented the report with the aid of plans and slides, including visualisations displaying the garden with the refuse stores in place.

Three local residents addressed the committee and outlined their objections to the application, which included: concern about that the number of occupants would be too great, particularly if some rooms were occupied by couples, and that there would be fewer occupants than the current use of a family house and a bed and breakfast. The proposed change of use was considered to be detrimental to the character of the conservation area, a view shared by the Norwich Society. The applicant had already left rubble in the front garden of one of the properties for some time. The residents also expressed concern that the bin storage would have an adverse visual impact on the streetscene for residents and pedestrians, would emit smells and attract vermin and would take up much of the front garden, require levelling and be difficult to use. Councillor Carlo referred to the planning history and said that the bin storage would still be visible above the wall; that future residents would have cars and exacerbate pressure on parking in the area. She suggested that the scheme be car free as it was in a sustainable location for cyclists and pedestrian, on bus routes and within access of the car club.

(Councillor Carlo left the meeting at this point.)

The agent addressed the committee on behalf of the applicant. He referred to the planning history and said that the current application would reduce the potential occupants from 29 to 14. This halved the number of car journeys that could be expected and reduced the amount of refuse that would be generated.

Discussion ensued in which the senior planner, together with the planning team leader, referred to the report and answered members' questions. He said that there was potential to reduce the size of the bins if larger bins were not considered necessary. Members noted that rooms were en-suite and there were shared kitchens and living spaces. The applicant was targeting young professional people. It was in a sustainable location, on bus routes and residents could easily walk or cycle into the city. It would not be possible to enforce this development as car free because it was not in a controlled parking zone. There had been no complaints to environmental health about debris in the front garden but officers would investigate this. The removal of garden vegetation and internal building works did not require planning permission. The applicants could revert to the current building use if this application was not successful.

The committee discussed the size of the bins and the applicant's arrangements for private contractors to clean the communal areas and service the bins. Members considered that if it were possible then smaller bins should be provided and asked that this should be added to the conditions for the application.

Councillor Jackson moved and Councillor Button seconded that the application be approved subject to an additional condition requiring the details of bin storage to be amended to stipulate the maximum number and size of bins to meet the needs of the development, and it was:

RESOLVED, unanimously, to approve application no. 15/01867/F - 145 & 147 Earlham Road Norwich NR2 3RG and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Detailed landscaping scheme to ensure adequate screening of refuse storage area and planting to mitigate for that lost in the front gardens which is easy to maintain and attractive;
4. No occupation of development until details of cycle storage have been agreed and implemented.
5. Any hardstanding to be constructed of porous material;
6. Compliance with the Management Strategy;
7. Installation of obscure glazing;
8. Each property shall be occupied by no more than 7 tenants, on a 1 tenant per lettable room basis, at any one time;
9. Details of bin storage to be amended subject to agreement on the number and size of bins.

Article 35(2) statement

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations

with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined in the officer report.

(Councillor Carlo was readmitted to the meeting at this point.)

8. Application no 16/01033/F – 23 Orchard Close, Norwich, NR7 9NY

The planning team leader (development) (outer area) presented the report with plans and slides. He explained that the application was for multigenerational family use to provide accessible accommodation for an older family member. He referred to the concerns of the adjacent neighbours and recommended that the application should be approved subject to the conditions stated in the report and two additional conditions to mitigate overlooking: partial obscure glazing to the windows to the side of the building to prevent overlooking of the driveway of no 25 and screening to prevent overlooking of the adjacent gardens.

The neighbour at 35 Orchard Close addressed the committee and outlined her concerns about loss of privacy as her property (kitchen and garden) would be overlooked by 23 Orchard Close when the patio was raised. She also expressed concerns about the large footprint of the proposal and flooding caused by surface rainwater and provision of a soakaway 2.5m from the boundary of her property. She considered the gable end, instead of a hipped roof as in the original planning permission, was out of character and would result in loss of outlook.

The agent addressed the committee and explained that the original planning permission had not been built and that subsequently the family's needs had changed and they now needed a wet-room, bedrooms for two boys, a kitchen with wheelchair access. There was a public sewer which constrained the location of the extension. She confirmed that applicant would accept the additional conditions.

During discussion the planning team leader referred to the report and commented on the issues raised by the speakers. He explained that the rear garden was not level and the decking raised the height of the garden by 1.8m, and, therefore, screening was proposed to prevent the overlooking into the neighbouring gardens. He explained that the gable end would not have an undue effect on overlooking or amenity. The extension would have guttering which led into the main drainage and there should be no increase in surface water drainage.

During discussion members had no questions but by consensus agreed to not to move the condition requiring partial glazing of the side windows overlooking the driveway of no 25. Councillor Jackson, moved, and Councillor Bogelein, seconded, to approve the application subject to the additional condition for screening and it was:

RESOLVED, unanimously, to approve application no. 16/01033/F – 23 Orchard Close, Norwich, NR7 9NY and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Details of landscaping and screening to be provided.

Article 35(2) statement

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined above.

9. Application no 16/00808/F – 1 Branksome Close, Norwich, NR4 6SP

The planning team leader (development) (outer area) presented the report with plans and slides.

Two residents of the close spoke in objection to the proposal. Nine neighbours and the Norwich Society had objected to this proposal. Their concerns included concerns that the extension was out of scale and character of the 1930s' development; would come beyond 2.5m beyond the building line; and, that the design was not harmonious to the surrounding houses and the double front door was confusing.

The applicant explained that the extension was to improve the domestic internal arrangements of the house and that it was essential that it was sympathetic to the appearance of the house and replace an ugly flat room. The front door was to provide access to the house without going through the kitchen. There would be an extra bedroom but it would not impact on traffic in the close and there was sufficient parking on the drive of the house for it not to affect parking on the highway.

During discussion the planning team leader referred to the report and replied to members' questions and the issues raised by the speakers. He explained that this was a large house that was set back, with a large front garden, from the street and that the breach of the building line by the extension was considered acceptable.

Some members expressed concern that the design of the extension could be more sympathetic to the 1930s' building and that the extension could have been moved back from the building line into the rear of the property.

RESOLVED with 8 members voting in favour (Councillors Herris, Driver, Button, Carlo, Peek, Sands, Woollard and Bradford) and 4 members voting against (Councillors Bogelein, Jackson, Ackroyd and Malik) to approve application no. 16/00808/F – 1 Branksome Close, Norwich, NR4 6SP and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;

Article 35(2) statement

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined above.

(Councillor Bradford left the meeting at this point.)

10. Application no 16/00835/F - 120 - 130 Northumberland Street, Norwich, NR2 4EH

(Councillor Bogelein declared an interest in this item and left the room at this point.)

The senior planner (development) presented the report with the aid of plans and slides and referred to the supplementary report of updates to reports circulated at the meeting which contained a correction to the report to take account of a petition signed by 33 people objecting to the three storey block of flats and summarising the comments of Anglian Water.

During discussion the senior planner referred to the report and answered members' questions. He explained that there would be pedestrian priority near the Nelson School and the material used would indicate that this was a shared space. Members were reassured that it was intended to screen the bin storage to the rear of the development.

RESOLVED, unanimously, to approve application no. 16/00835/F - 120 - 130 Northumberland Street, Norwich, NR2 4EH and grant planning permission subject to the completion of a satisfactory legal agreement to include provision of affordable housing and subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Details to include: materials to be used in external construction of development(including samples and specifications where necessary), external joinery, rainwater goods;
4. Detailed landscaping scheme to reflect homezone design and include details of permeable paving, demarcation of parking spaces, biodiversity enhancements (hedgehog fencing, bird/bat boxes), lighting, planting (including replacement tree planting), boundary treatments;
5. Contamination – Risk assessment;
6. Contamination – Verification plan;
7. Contamination – Long term monitoring;
8. Contamination – Unknown contamination;
9. Contamination – Imported material;
10. Contamination – Piling methodology;
11. Details of secure and covered cycle storage, refuse storage across the site and EV charging;
12. Compliance with AIA and submission of TPP and method statement as recommended in AIA;
13. Operations on site to take place in accordance with the mitigation/compensation measures outlined in section 7 of the ecological report.
14. No hard-standing areas to be constructed until the works have been carried out in accordance with the surface water strategy so approved unless otherwise agreed in writing by the Local Planning Authority;
15. Scheme for renewable energy;

16. Construction Method Statement;
17. Grampian condition. No occupation of the dwellings until vehicle access incorporating pedestrian priority has been provided from Northumberland Street in accordance with a scheme to first be agreed in writing with the local planning authority;
18. 10% Lifetime homes;
19. Water efficiency;
20. Restricted construction times

Informatives:

- 1) Considerate construction
- 2) Details of refuse storage are conditioned. The applicant is advised that disabled access should be provided to the communal stores.
- 3) EA advice;
- 4) Asbestos;

Article 35(2) statement

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined in the officer report.

(Councillor Bogelein was readmitted to the meeting at this point.)

11. Application no 16/00788/F – 21 Hellesdon Road, Norwich, NR6 5EB

The planning team leader (development) (outer area) presented the report with plans and slides.

Councillor Jackson stated that this application demonstrated the need for the council to have a policy on garden grabbing. However, there was existing permission for a single dwelling on this site and therefore he would support this application for two dwellings.

RESOLVED, unanimously, to approve application no. 16/00788/F – 21 Hellesdon Road, Norwich, NR6 5EB and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Details of windows to north and west elevations (glazing and method of opening)
4. To remain ancillary accommodation to main house.

Article 35(2) statement

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations

with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined above.

12. Application no 16/00765/F – 31 St Clements Hill, Norwich, NR3 4DE

The planning team leader (development) (outer area) presented the report with plans and slides.

During discussion the planning team leader referred to the report and answered members' questions. He explained that the application was retrospective and if members were to refuse the application the applicant would be subject to enforcement action to reinstate the property to its original condition. The garden was large and other surrounding properties had large outbuildings. The standard of the upper floor of the outbuilding had restricted headroom but was suitable for a playroom and as an ancillary use to the main house.

RESOLVED, with 9 members voting in favour (Councillors Herries, Driver, Button, Bogelein, Carlo, Malik, Sands, Peek and Woollard), and 2 members voting against (Councillors Jackson and Ackroyd) to approve application no 16/00765/F – 31 St Clements Hill, Norwich, NR3 4DE and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Details of windows to north and west elevations (glazing and method of opening)
4. To remain ancillary accommodation to main house.

Article 35(2) statement

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined above.

(The committee adjourned for lunch at 13:35 and reconvened at 14:05 with the following members present as above, with the exception of Councillor Bradford who had left the meeting after item 9, above.)

13. Application No 16/00782/F and 16/00783/L - Sainsbury Centre for Visual Arts, University of East Anglia, Earlham Road, Norwich

The senior planner (development) presented the report with the aid of plans and slides.

During discussion the senior planner referred to the report and answered members' questions. He explained the need to provide more car parking for visitors to the Sainsbury Centre as a separate entity from the University of East Anglia. Members noted the comments of the Twentieth Century Society and noted that a travel plan would be required as part of the application.

RESOLVED, with 7 members voting in favour (Councillors Herries, Driver, Button, Carlo, Peek, Sands and Woollard) and 4 members abstaining from voting (Councillors Bogelein, Jackson, Ackroyd and Malik) to:

- (1) approve application no. 16/00782/F - Sainsbury Centre for Visual Arts, University of East Anglia, Earlham Road, Norwich and grant planning permission subject to the following conditions:
 1. Standard time limit;
 2. In accordance with plans;
 3. Submission of landscape details for tree planting and landscape implementation. Subsequent maintenance;
 4. Submission of cycle parking details;
 5. Tree officer meeting.
 6. Submission of AMS for hand dig specification and any alternative land grading
 7. Tree works in accord with AIA/AMS;
 8. Retention of tree protection measures during works;
 9. Parking for use by visitors to the SCVA only;
 10. Submission of car park management and travel planning details/information

Article 35 (2) statement

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments at the application stage the application has been approved subject to appropriate conditions and for the reasons outlined within the officer's committee report with the application.

- (2) approve application no. 16/00783/L - Sainsbury Centre for Visual Arts, University of East Anglia, Earlham Road, Norwich and grant listed building consent subject to the following conditions:
 1. Standard time limit;
 2. In accordance with plans;

Reasons for Approval:

Car parking in the proposed location could result in a degree of harm to the significance of the grade II* listed Sainsbury Centre and ziggurats and grade II listed teaching wall and walkway in terms of the NPPF. Misgivings have previously been expressed about the prospect of allowing parking either as a temporary car park or within close proximity to the Sainsbury Centre. Earlier assessment has helped inform the larger debate about locations for smaller, permanent additional car parking designated for the Sainsbury Centre elsewhere on campus and for providing managed solutions which are aimed at avoiding causing substantial harm to the setting of the listed buildings or river valley character area.

Although the change in the design of the landscape setting could be considered to result in a degree of harm when it is altered, the possibility that the works present an opportunity to allow better access and beneficial continued use of the building does

help to outweigh the harm that will be caused. Subject to appropriate replacement landscaping the alterations will relate satisfactorily to the area and will respect the special architectural character of the Sainsbury Centre and other listed buildings. Subject to suitable operation of the parking area the alterations on balance result in less than substantial harm to the significance of the heritage asset and will help to secure the optimum viable use of the building.

The scheme improves the operation of the building and overall should not have an adverse impact on design or amenities in the area. As such the development and works to the listed building, subject to conditions, are considered to be appropriate and in accordance with the objectives of the NPPF, policies 1 and 2 of the Joint Core Strategy for Broadland, Norwich and South Norfolk (2011) and policies DM3 and DM9 of the adopted Development Management Policies Plan (December 2014).

14. Application no 16/00425/F - 2 Fairmile Close, Norwich, NR2 2NG

The planning assistant (development) presented the report with the aid of plans and slides.

Two neighbours addressed the committee and highlighted their objections to the proposal. This included concern about overlooking of bedrooms and living rooms and that the extended building would block natural light to the neighbouring property. The extension would breach the building line. The modernist design was considered to be out of keeping of the 1960s' buildings in the close and would clash with the "characterful and distinct" appearance of the close. The Norwich Society had objected to the proposal. Members were asked to undertake a site visit.

The applicant spoke in support of the application and said that the houses in the close had not been built to a "harmonious plan". He referred to planning policy and said that he had employed an architect to design the extension. He also referred to the report and said that paragraph 20 set out the results of the shadow assessment and said that he had worked with the planning officer to mitigate the concerns of the neighbours.

The planning assistant referred to the report and answered members' questions. He confirmed that the new extension would be 6.0m at its highest point while the existing house was 6.4m. (Plans displaying the new structure superimposed on the existing house were presented to the committee.)

Discussion ensued. Members had varied views. Some members welcomed the design and considered that there was sufficient space for it. A member commented that the building was innovative but should be refused because it did not fit into the context of the surrounding houses, which although individual had shared features such as materials and landscaping. Other members were concerned that the alterations were so great as to merit a new build rather than an extension.

During discussion a member highlighted that the house was in a critical drainage area and suggested that the flat roof should be a sedum roof. The planning team leader (development) (outer area) said that this would be subject to the agreement of

the applicant, who then indicated that he would agree for this amendment to the proposal.

The committee then voted on the recommendations to amend the conditions to require a sedum (green) roof.

RESOLVED with 7 members voting in favour (Councillors Herries, Driver, Button, Bogelein, Carlo, Malik and Woollard) and 4 members voting against (Councillors Jackson, Ackroyd, Peek and Sands) to approve application no. 16/00425/F - 2 Fairmile Close Norwich NR2 2NG and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans, subject to amendment to include a sedum roof;
3. External Materials

Informative:

1. Construction working hours.

15. Application no 15/01540/F - Land to the south of Merchants Court, St Georges Street, Norwich

(Councillor Bogelein left the meeting during this item.)

The planning assistant (development) presented the report with the aid of plans and slides and referred to the supplementary report of updates to reports which was circulated at the meeting. Following recent changes to the application, The Playhouse had withdrawn its objections to the scheme, subject to the timing of planned works being considerate of matinee performances.

During discussion the planning assistant referred to the report and answered members' questions. Members expressed concern that the car park had become "landlocked" as an outcome of permitted development rights when office accommodation was changed to a school. Members noted that the residents needed to access the car park but considered it regrettable that there would be loss of public amenity space and trees.

Councillor Carlo said that she did not consider the proposed access acceptable and that it would be detrimental to the safety of pedestrians.

RESOLVED, with 8 members voting in favour (Councillors Herries, Driver, Button, Jackson, Ackroyd, Peek, Woollard and Sands) and 2 members voting against (Councillors Carlo and Malik) to approve application no. 15/01540/F - Land to the south of Merchants Court, St Georges Street, Norwich and grant planning permission subject to the following conditions (and deleting condition 3 as set out in the report because previous approval on this site is now expired so condition preventing its implementation has been removed):

:

1. Standard time limit;
2. In accordance with plans;
3. In accordance with the submitted Arboricultural Impact assessment.
4. Subject to submission and subsequent approval of an Arboricultural Method Statement.
5. Specification of replacement planting.

Article 35(2) Statement

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined in the officer report.

16. Application no 16/00924/F - 3 Ampthill Street, Norwich, NR2 2RG

The planning assistant (development) and the arboricultural officer (TPO) presented the report, together with the report for the following related item.

The applicant addressed the committee and explained that she needed secure off-street parking to protect her company car from vandalism and, as she worked shifts, was easily accessible. Other residents had converted their front gardens to provide off-street parking. The area would be landscaped and could include a silver birch or rowan tree. Neighbours had complained about the crab apple tree when it dropped its fruit.

During discussion the planning assistant, arboricultural officer and the planning team leader (development) (outer area) referred to the reports and answered members' questions. The proposal was recommended for refusal. The arboricultural officer said that in his opinion there was not room for off street parking and a tree in the front garden. The tree could be maintained so that it was easier to manage.

Discussion ensued in which members considered that the area was in a controlled parking zone and the difficulty of the applicant in finding a parking space near her home late at night or early morning and her personal safety. A member suggested that the applicant could apply to rent a garage and pointed out that there were garages in the vicinity. Some members considered that the tree was in the wrong location and that landscaping would be more sympathetic to the streetscene.

The chair moved the recommendation to refuse the application as set out in the report and with 3 members voting in favour (Councillors Carlo, Jackson and Ackroyd) and 6 members voting against (Councillor Herries, Driver, Button, Peek, Sands and Woollard) and 1 member abstaining (Councillor Malik) the motion to refuse application no 16/00924/F – 3 Ampthill Street, Norwich, NR2 2RG, was lost.

Councillor Sands moved and Councillor Woollard seconded that the application be approved, subject to conditions (proposed by the planning team leader) and on being put to the vote it was:

RESOLVED, with 6 members voting in favour (Councillor Herries, Driver, Button, Peek, Sands and Woollard), 3 members voting against (Councillor Carlo, Jackson and Ackroyd), and 1 member abstaining (Councillor Malik) to approve application no Application no 16/00924/F - 3 Ampthill Street, Norwich, NR2 2RG (contrary to officer recommendation) and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Details of landscaping and replacement planting to be agreed;
4. Gates not to open into highway.

17. Tree Preservation Order [TPO], 2016. City of Norwich Number 505; 3 Ampthill Street, Norwich, NR2 2RG

The chair moved and Councillor Sands seconded that the committee went straight to the vote and with a majority voting in favour the procedural motion was carried.

Councillor Sands moved and Councillor Woollard seconded that Tree Preservation Order no 505 was not confirmed as a consequence of the decision taken in respect of the above item - Application no 16/00924/F - 3 Ampthill Street, Norwich, NR2 2RG.

RESOLVED, with 6 members voting in favour (Councillors Herries, Driver, Button, Peek, Sands and Woollard), 3 members voting against (Councillors Carlo, Jackson and Ackroyd), and 1 member abstaining from voting (Councillor Malik), to not to confirm Tree Preservation Order [TPO], 2016. City of Norwich Number 505; 3 Ampthill Street, Norwich, NR2 2RG, (contrary to officer recommendation).

18. Tree Preservation Order [TPO], 2016. City of Norwich Number 506; 166a St Clements Hill, Norwich, NR3 4DG

The arboricultural officer (TPO) presented the report with the aid of plans and slides.

A member of the public addressed the committee and said that the birch tree blocked the entrance to the drive because its root structure. The drive way was 2.4m wide and although the tree officer maintained that there was adequate room to access the drive "with care", he considered that there was insufficient room for an ambulance. He also considered that the amenity value of the tree had diminished.

Discussion ensued in which a member suggested that the consideration should be given to remove the brick pillars at the entrance to the drive as this could widen it sufficiently to overcome the resident's concerns.

RESOLVED, unanimously, to confirm Tree Preservation Order [TPO], 2016. City of Norwich Number 506; 166a St Clements Hill, Norwich, NR3 4DG.

19. Application no 16/01118/F - Garages opposite 2 Oxford Street. Norwich

The senior planner (development) presented the report with the aid of plans and slides. He referred to the supplementary report of updates to reports which was circulated at the meeting, and said that two representations had been received which were summarised with the officer response in the report. Members were also advised that there was an amended plan and that an additional condition regarding tree protection measures was recommended.

During discussion the senior planner referred to the report and answered members' questions. The committee considered the issues raised in response to the consultation/publication of the report. Members noted that 20 of the 24 garages were let to tenants and that alternative provision was available within 800m of the site.

Members welcomed the provision of five affordable housing units in the area.

RESOLVED, unanimously, to approve application no 16/01118/F and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Details of facing and roofing materials; windows; joinery; boundary treatments, walls and fences; external lighting;
4. Details of hard and soft landscaping, planting, biodiversity enhancements.
5. Implementation of sustainability measures/energy efficiency measures as outlined in application
6. Contamination risk assessment and report to be submitted
7. Unknown contamination to be addressed
8. Control on imported materials;
9. Works on site to be in accordance with the submitted Arboricultural Implications Assessment, Arboricultural Method Statement and Tree Protection Plan.

Article 35(2) statement

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined in the officer report.

20. Application no 16/00290/F - Eaton Hand Car Wash Ipswich Road Norwich NR4 6QS

The planning team leader (development) (outer area) presented the report with the aid of plans and slides.

Councillor Ackroyd, Eaton Ward councillor, said that it was important that the applicants reinstated the boundary.

RESOLVED, unanimously, to approve application no. 16/00290/F - Eaton Hand Car Wash Ipswich Road Norwich NR4 6QS and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans
3. Details of the boundary treatments specified on PDB/16/07/02A must be submitted within 8 weeks and a supplementary AIA/AMS for the installation of these.
4. Opening hours restricted to 08:00 – 19:00 Monday to Saturday and 10:00 – 16:00 Sunday and Bank Holidays.

Informative

It should be noted that a separate application would be required should any development (which requires consent) be undertaken on the land within the same ownership that is outlined in blue on the site location plan.

Article 35(2) statement

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined in the officer report.

CHAIR

Planning applications committee
12:15 to 12:35
22 September 2016

Present: Councillors Herries (chair), Bogelein (substitute for Councillor Henderson), Bradford, Button, Carlo, Jackson, Lubbock, Malik, Peek

Apologies: Councillors Driver (vice chair), Henderson, Sands (M) and Woollard

(The following members attended the site visit to St Peters Methodist Church, Park Lane and no 79 Park Lane: Councillors Herries, Bradford, Button, Carol, Jackson, Lubbock, Malik and Peek.)

1. Declarations of interest

There were no declarations of interest.

2. Application no 15/01928/F – St Peters Methodist Church, Park Lane, Norwich, NR2 3EQ

(The meeting had been delayed to give members an opportunity to read through the supplementary report of updates to reports and a letter received from Norfolk County Council, as lead local flood authority, which were circulated at the meeting.)

Councillor Carlo referred to the county council's objection to the scheme and the supplementary report of updates to reports, circulated at the meeting, and said members did not have time to fully comprehend the technical five page objection from the county council and the implications raised in late representations from local members in relation to the city council's economic viability assessment for affordable housing. She then moved, seconded by Councillor Bogelein, that consideration of the application should be deferred to allow members an opportunity to consider the issues raised and for a response from the applicant and officers.

The senior planner (development), referred to the supplementary report and the revised officer recommendation, and explained that when the application was received last year, the local authority did not identify the site as a known flooding location and the site did not therefore constitute one where the lead local flood risk authority should be consulted. The flood risk of the surrounding area was subsequently brought to the attention of the local authority, which meant that it was necessary to consult the county council as lead local flood risk authority and its response had been received on 21 September 2016.

During discussion members spoke in support of the motion to defer consideration of the application and expressed their concern about the need to fully understand the implications of the county council's objections and consider a full assessment of the issues raised in its letter and the supplementary report before making a decision on

the application. Members considered that were the county council's objection to be removed there would be conditions which should be approved by members rather than delegating to officers.

The planning team leader (development) (outer area) advised members that the applicant could instigate an appeal for non-determination if there was a further delay.

RESOLVED, with 7 members voting in favour (Councillors Carlo, Bogelein, Bradford, Button, Jackson, Lubbock and Malik) and 2 members voting against (Councillors Herries and Peek), to defer consideration of the application to allow members to digest the information circulated at the meeting and for further information to be provided in response to the issues raised by the lead local flood authority (Norfolk Council) regarding the applicant's flood risk assessment and in late representations from local members and residents regarding the viability assessment provided by the applicant.

(The application is therefore expected to come before the committee on 10 November 2016, at the earliest.)

CHAIR

Applications for submission to planning applications committee

ITEM 4

13 October 2016

Item No.	Case Number	Location	Case Officer	Proposal	Reason for consideration at Committee	Recommendation
5(a)	16/00606/F	297 Aylsham Road	Kian Saedi	Demolition of existing buildings and the erection of a foodstore (Lidl), formation of access, car parking	Objections	Approve
5(b)	16/00456/F	BT Telephone Exchange Westwick House, 70 Westwick St.	Lee Cook	Demolition of former Norwich Telephone Repeater Station and redevelopment of site to provide 42 dwellings	Objections	Approve subject to S106 agreement
5(c)	16/00759/F	137 Unthank Road	Charlotte Hounsell	Demolition of shop. Construction of 1 No. retail (Class A1) unit, 1 No. hot food takeaway and restaurant (Class A3 and Class A5) and 1 No. first floor dwelling.	Objections	Approve
5(d)	16/01117/F	Land West Of 3 Beaumont Place	Robert Webb	2 No. dwellings	Objections + Council land	Approve
5(e)	16/01098/F	Garages adjacent 56 Sotherton Road	Robert Webb	Demolition of 14 No. garages and erection of 2 No. dwellings.	Objections + Council land	Approve
5(f)	16/01106/F	Car park adjacent To 69 Armes Street	Robert Webb	Erection of 4 No. flats	Objections + Council land	Approve
5(g)	16/01109/F	Land used for car parking adjacent To 99 Armes Street	Robert Webb	3 No. dwellings.	Objections + Council land	Approve

Item No.	Case Number	Location	Case Officer	Proposal	Reason for consideration at Committee	Recommendation
5(h)	16/01121/F	Garages adjacent To 110 Quebec Road	Robert Webb	Demolition of existing garages and the erection of 3 No. dwellings.	Objections + Council land	Approve
5(i)	16/01115/F	Garages opposite 46 Goldwell Road	Robert Webb	Demolition of garages and construction of 6 No. flats.	Objections + Council land	Approve
5(j)	16/00563	Kingdom Hall of Jehovah's Witnesses, Clarke Road	Katherine Brumpton	Demolition of existing building and erection of 3 No. dwellings	Objections	Approve
5(k)	16/01156/F	70 Grove Walk	Sam Walker	Second storey side extension and extension of roof.	Objections	Approve
5(l)	16/00761/F	17 and 19 Neville Street	Stephen Polley	Replacement UPVC windows and external doors.	Objections	Approve

STANDING DUTIES

In assessing the merits of the proposals and reaching the recommendation made for each application, due regard has been given to the following duties and in determining the applications the members of the committee will also have due regard to these duties.

Equality Act 2010

It is unlawful to discriminate against, harass or victimise a person when providing a service or when exercising a public function. Prohibited conduct includes direct discrimination, indirect discrimination, harassment and victimisation and discrimination arising from a disability (treating a person unfavourably as a result of their disability, not because of the disability itself).

Direct discrimination occurs where the reason for a person being treated less favourably than another is because of a protected characteristic.

The act notes the protected characteristics of: age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The introduction of the general equality duties under this Act in April 2011 requires that the council must in the exercise of its functions, have due regard to the need to:

- Eliminate unlawful discrimination, harassment and victimisation and other conduct prohibited by this Act.
- Advance equality of opportunity between people who share a relevant protected characteristic and those who do not.
- Foster good relations between people who share a relevant protected characteristic and those who do not.

The relevant protected characteristics are: age; disability; gender reassignment; pregnancy and maternity; race; religion or belief; sex; sexual orientation.

The council must in the exercise of its functions have due regard to the need to eliminate unlawful discrimination against someone due to their marriage or civil partnership status but the other aims of advancing equality and fostering good relations do not apply.

Crime and Disorder Act, 1998 (S17)

- (1) Without prejudice to any other obligation imposed on it, it shall be the duty of each authority to which this section applies to exercise its

various functions with due regard to the likely effect of the exercise of those functions on, and the need to do all that it reasonably can to prevent, crime and disorder in its area.

- (2) This section applies to a local authority, a joint authority, a police authority, a National Park authority and the Broads Authority.

Natural Environment & Rural Communities Act 2006 (S40)

- (1) Every public authority must, on exercising its functions, have regard, so far as is consistent with the proper exercise of those functions, to the purpose of conserving biodiversity.

Planning Act 2008 (S183)

- (1) Every Planning Authority should have regard to the desirability of achieving good design

Human Rights Act 1998 – this incorporates the rights of the European Convention on Human Rights into UK Law

Article 8 – Right to Respect for Private and Family Life

- (1) Everyone has the right to respect for his private and family life, his home and his correspondence.
- (2) There shall be no interference by a public authority with the exercise of his right except such as in accordance with the law and is necessary in a democratic society in the interests of national security, public safety or the economic well-being of the country, for the prevention of disorder or crime, for the protection of health or morals, or for the rights and freedoms of others.
- (3) A local authority is prohibited from acting in a way which is incompatible with any of the human rights described by the European Convention on Human Rights unless legislation makes this unavoidable.
- (4) Article 8 is a qualified right and where interference of the right can be justified there will be no breach of Article 8.

Report to Planning applications committee
 13 October 2016

Report of Head of planning services

Subject 16/00606/F - 297 Aylsham Road Norwich NR2 3RY

Reason for referral Objection

Item

4(a)

Ward:	Catton Grove
Case officer	Mr Kian Saedi - kiansaedi@norwich.gov.uk

Development proposal		
Demolition of existing buildings and the erection of a foodstore, formation of access, car parking, landscaping and associated works.		
Representations		
Object	Comment	Support
2	1	0

Main issues	Key considerations
1 Principle of development	Allocated site (R21) - impact on deliverability of housing, principle of retail development, sequential justification for site location, retail impacts
2 Design and Heritage	Impact on setting of heritage assets, impact on character of adjacent conservation area
3 Trees, landscaping and biodiversity	Loss of trees, ecological impacts, landscaping, replacement tree planting
4 Transport	Highway impacts and improvements, car parking, accessibility
5 Amenity	Noise, overbearing, outlook and overshadowing
6 Contamination	Response to comments received from the Environment Agency.
Expiry date	12 August 2016 extended to 20 October 2016
Recommendation	Approve

The site and surroundings

1. The site is located to the north of the city centre and is relatively flat. It has a frontage to Aylsham Road (A140) and is surrounded by residential, commercial, retail and entertainment uses. The site has been used for storage and distribution of heating oil since the 1960s. The rear part of the site accommodates fuel storage tanks, parking for a fleet of delivery vehicles and administrative and support facilities. Management functions and fleet servicing are also carried out on the site.
2. The site lies within the setting of the Grade II* listed– St Catherine’s Church, Grade II Listed St Catherine’s Church Hall and neighbouring locally listed vicarage and Mile Cross Library all located along the western side of Aylsham Road immediately opposite the application site. The Mile Cross Conservation Area is also located to the west.
3. The part of the site with a frontage to Aylsham Road has been used variously as a car showroom and bath store but currently stands empty, and in recent years the area to the south of the showroom has been used for car and van sales / hire.
4. The site is not within a defined centre but is adjacent to the Aylsham Road District Centre. As such the site is defined as edge-of-centre in terms of NPPF definitions.
5. The application site forms part of the allocation (R21) for the wider site, for mixed use redevelopment. The allocation states that development at the allocated site will:
 - include retail provision on the street frontage, up to a maximum of 2,500 square metres (gross), with a maximum of 300 square metres (net) for comparison goods sales and appropriate parking provision;
 - include housing development (in the region of 100 dwellings) in the north of the site;
 - minimise impacts on setting of heritage assets;
 - protect trees within the site and provide landscaping and site linkages;
 - be designed to mitigate noise impact from the main road. A noise assessment will be required.
6. The above and other material considerations are considered in the following sections of this report.

Constraints

7. Critical Drainage Area
8. Site is located adjacent to the Mile Cross Conservation Area, the Grade II listed St Catherine’s Church and Church Hall and neighbouring locally listed vicarage and Mile Cross Library, which are all located on the western side of Aylsham Road immediately opposite the application site.

Relevant planning history

9.

Ref	Proposal	Decision	Date
13/01928/F	Demolition of existing buildings and redevelopment of site to construct a new foodstore with associated landscaping and car parking. Reconfiguration of site access and highway works to accommodate.	APPR	12/06/2014
14/01225/D	Details of Condition 14: Verification plan proposals for remediation of near-surface/subsoil contamination; Condition 16: Long term monitoring/maintenance plan for confirming completion of near-surface/subsoil contamination treatment; Condition 17: Detailed Quantitative Risk Assessment for the dissolved phase / floating product groundwater contamination; Condition 18(a): Remediation Verification Plan for treatment of dissolved phase / floating product groundwater contamination; Condition 18(b): Remediation Verification Report into treatment of dissolved phase / floating product groundwater contamination; Condition 19(a): Longer term monitoring/maintenance plan for groundwater/floating product/dissolved contamination product; Condition 19(b): Submission of longer term monitoring reports and any contingency actions taken, for groundwater/floating product/dissolved contamination; Condition 19(c): Final Detailed Quantitative Risk Assessment of groundwater/floating product/dissolved phase contamination; Condition 21: Drainage Strategy (a) Infiltration testing; (b) storage; (c) infiltration drainage; (d) surface water drainage; and (e) modelling of on-site surface water network; of previous permission 13/01928/F 'Demolition of existing buildings and redevelopment of site to construct a new foodstore with associated landscaping and car parking. Reconfiguration of site access and	APPR	01/09/2015

	highway works to accommodate.'		
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The proposal

10. The applications seeks consent for the demolition of existing buildings and the erection of a foodstore, formation of access, car parking, landscaping and associated works.

Summary information

Proposal	Key facts
Scale	
Total floorspace	Gross floorspace - 2,755 sq.m Net retail floorspace – 1,690 sq.m For the sale of convenience goods – 1,352 sq.m For the sale of comparison goods – 338 sq.m
No. of storeys	2
Max. dimensions	Ridge height of 7.57 metres and eave height of 5.28 metres.
Appearance	
Materials	White mineral render, silver metallic finish cladding, standing seam aluminium sheet roofing, graphite grey powder coated aluminium framed windows.
Operation	
Opening hours	07:00 – 22:00 (Mon – Sat) 10:00 – 17:00 (Sun)
Ancillary plant and equipment	Located in enclosed compound in south-east corner of store as well as on first floor above the freezer/chiller area.
Transport matters	
Vehicular access	From Aylsham Road – New vehicle access.
No of car parking spaces	156, including 10 disabled parking spaces.
No of cycle parking	20 indicated onsite plan. Details to be secured by

spaces	condition.
Servicing arrangements	Enclosed loading bay in south-east corner of site.

Representations

11. Advertised on site and in the press. Adjacent and neighbouring properties have been notified in writing. One letter of representation has been received citing the issues as summarised in the table below. All representations are available to view in full at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Issues raised	Response
Objections	
The development is not in accordance with an up to date local plan and paragraph 26 of the NPPF requires applicants to provide a Retail Impact Assessment to assess the impact of the development upon current, committed and planned private and public investment in the centres within the catchment of the proposed foodstore, and the impact of the proposal on town centre vitality and viability up to five years out from the project.	Main Issue 1
Paragraph 27 of the Framework requires a local planning authority to refuse permission where an application fails to satisfy the sequential test or where an application will have a significant adverse impact on either the current, committed and planning proposals, or on the vitality and viability of town centres. Given the applicant has failed to provide any evidence that the development proposal would not have a severe impact, and has failed to undertake the sequential test, instead mistakenly trying to argue that the application meets the local policy R21, the council would be well within its rights to refuse the application on these grounds alone.	Main Issue 1
Loss of trees	Main Issue 3
The applicant has failed to assess how removing the existing trees from the site will	Main Issue 3

impact the green infrastructure network throughout the city, which is recommended as a response to JCS1.	
The development proposal put forward by the applicant does not respect, enhance or even respond to the character and local distinctiveness of the area. The proposal does not have regard to the character of the surrounding neighbourhood, or to the elements contributing to its overall sense of place. The proposal does not give any weight to the uses and activities around it, or to the historic context of the streetscape.	Main Issue 2
The applicant's Travel Plan is ambiguous and flawed, and should not be considered satisfactory to fulfil the obligations under Policy JCS6 and supporting text in Paragraph 5.49 in the JCS, Policy 8 of the Norwich Area Transportation Strategy (NATS), and Policy DM28 of the <i>Local Plan</i> .	Main Issue 4
Comment	
We support the comments made by Historic England regarding the street presence.	

Consultation responses

12. Consultation responses are summarised below the full responses are available to view at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Design and conservation

13. "It is acknowledged that the past approval may have created a better street presence and offered more to enhance the setting of neighbouring heritage assets (particularly in terms of car parking). Nevertheless, this application has to be determined on its own merits and provided that the LPA can come to some agreement on the following issues, the proposals would be considered to comply with Local Plan and NPPF policy including paragraphs 132 and 137. The scheme will result in an improvement upon the status-quo; it will result in the re-development of a rather dilapidated and unattractive site and the setting of neighbouring heritage assets will be positively enhanced.
14. Issues to be resolved:-
- Location of the disabled parking area;

Landscaping to the western boundary (we would want to condition that this must be completed before store operation commences);

Street trees will be maintained/relocated;

Replacement trees will be agreed across the site;

Proposed hardstanding to be agreed;

Signage and lighting across the site;

Proposed materials.”

15. The conservation officer has subsequently agreed (verbally) that the proposed materials and disabled parking is acceptable. A detailed landscaping scheme will be secured by condition but the indicative plan exhibits much merit in terms of providing an attractive frontage to Aylsham Road.

Historic England

16. “The proposed development will have an impact on the setting of the listed church and church hall and of the adjacent conservation area. While we have previously accepted the development of a large food store on this site we do not feel the current design will fully realise its potential to enhance the setting of heritage assets. The NPPF, paragraph 137 asks local planning authorities to favour proposals that do achieve real enhancement. We would therefore not support the proposals as they stand and recommend the Council consider a revised design that brings the new building closer to the street and has a more positive relationship with the public realm and redesigning the western elevation.”

Environmental protection

17. With respect of contamination:

“As the EA are now satisfied with the situation re contamination, I have no further comments to add.”

18. With respect of noise:

19. “I have looked at this application and believe it is necessary to restrict delivery times as below;

No trade deliveries or collections, including trade waste or clinical waste shall take place before 07:00 hours and after 23:00 hours Monday to Saturday. There shall be no trade deliveries or collections, including trade waste or clinical waste, on Sundays or Bank or Public Holidays.”

Environment Agency

20. “Thank you for providing the drainage strategy and assessment of residual risk and mitigation measures dated July 2016 by CSG Consulting Engineers. We have had the opportunity to consider the information in these documents and are able to recommend the removal of the holding objection outlined in our previous letter (AE/2106/120451/01, dated 3/6/2016). Without these conditions, the proposed

development on this site poses an unacceptable risk to the environment and we would object to the application.”

21. Conditions are recommended to deal with contamination.

County Council (Lead Local Flood Authority (LLFA))

22. “I can confirm that the County Council as Lead Local Flood Authority has no comments to make”.

Anglian Water

23. “The sewerage system has available capacity for these flows. If the developer wishes to connect to our sewerage network they should serve notice under S106 of the Water Industry Act 1991.
24. From the details submitted to support the planning application the proposed method of surface water management does not relate to Anglian Water operated assets. As such, we are unable to provide comments on the suitability of the surface water management. The Local Planning Authority should seek the advice of the Lead Local Flood Authority or the Internal Drainage Board. The Environment Agency should be consulted if the drainage system directly or indirectly involves the discharge of water into a watercourse.
25. Should the proposed method of surface water management change to include interaction with Anglian Water operated assets, we would wish to be re-consulted to ensure that an effective surface water drainage strategy is prepared and implemented.”

Highways (local)

26. Agree with comments submitted by County as strategic highways authority.
27. The travel plan for the application is considered to be acceptable. The site is adjacent to a district centre meaning that it is already highly accessible by bus, walking and cycling and will not be entirely car dependent. It is also considered that by relocating from the current site closer to Mile Cross and Hellesdon area make it more accessible by these sustainable modes.

Highways (strategic)

28. “The application is for the relocation of a Lidl store on Aylsham Road, Norwich. The Aylsham Road at this point is a strategic route. The site previously had permission for a Morrison’s Supermarket.
29. Access is proposed using a Right Hand Turn Lane with the access road having a central D splitter to encourage pedestrian and cycle access. The City Council has raised concerns that a more appropriate access would be a narrow access road without the D splitter. If the applicant can demonstrate that this can be achieved then the strategic highway authority is happy to accept the City Councils recommendations. Should a revised access strategy not work, then that proposed is considered acceptable. There will also be revisions required to the waiting restrictions along Aylsham Road. These will need to be secured via a Traffic Regulation Order which the city will promote.

30. The strategic highway authority recommends no objection subject to conditions and subject to an access strategy being agreed with the City Council. The City Council may wish to add conditions if they consider them necessary particularly in relation to the required TROs”
31. The applicant has subsequently amended the scheme to remove the D-splitter at the vehicular access and this strategy is considered to be acceptable as confirmed verbally by the council’s Principal Transport Planner. The removal of the D-splitter and narrower access carry benefits in terms of creating a shorter distance for pedestrians to walk across and for deterring vehicles from swinging into the site at high speeds.

Landscape

32. Several recommendations made relating to replacement landscaping and protection of street trees and trees located on adjacent sites. The indicative landscaping scheme has subsequently been amended to address these recommendations. A detailed landscaping scheme will be secured by condition.

Norfolk county planning obligations

33. The strategic highways authority has confirmed that there will be no S106 requirement and that all off-site works including the TRO will be secured by condition.

Norfolk historic environment service

34. No archaeological implications.

Norfolk police (architectural liaison)

35. Recommendations have been made to enhance the security of the site.

Tree protection officer

36. Concerns initially raised with respect to the level of information that had been submitted and the potential impact of the scheme on street trees and trees on adjacent sites. Additional information has subsequently been submitted and adequately addresses the concerns of the tree officer. Planning permission will be conditioned to require compliance with the approved arboricultural information. A comprehensive scheme of tree replacement come forward as part of the landscaping scheme which is to be conditioned.

Norfolk Fire and Rescue

37. No objections, providing the proposal meets the necessary requirements of Building Regulations 2010 – Approved Document B.

Assessment of planning considerations

Relevant development plan policies

38. **Joint Core Strategy for Broadland, Norwich and South Norfolk adopted March 2011 amendments adopted Jan. 2014 (JCS)**

- JCS1 Addressing climate change and protecting environmental assets
- JCS2 Promoting good design
- JCS3 Energy and water
- JCS4 Housing delivery
- JCS5 The economy
- JCS6 Access and transportation
- JCS12 The remainder of the Norwich urban area including the fringe parishes
- JCS18 The Broads
- JCS19 The hierarchy of centres
- JCS20 Implementation

39. **Norwich Development Management Policies Local Plan adopted Dec. 2014 (DM Plan)**

- DM1 Achieving and delivering sustainable development
- DM2 Ensuring satisfactory living and working conditions
- DM3 Delivering high quality design
- DM4 Providing for renewable and low carbon energy
- DM5 Planning effectively for flood resilience
- DM6 Protecting and enhancing the natural environment
- DM7 Trees and development
- DM9 Safeguarding Norwich's heritage
- DM11 Protecting against environmental hazards
- DM15 Safeguarding the city's housing stock
- DM18 Promoting and supporting centres
- DM21 Protecting and supporting district and local centres
- DM28 Encouraging sustainable travel
- DM30 Access and highway safety
- DM31 Car parking and servicing

40. **Norwich Site Allocations Plan and Site Specific Policies Local Plan adopted December 2014 (SA Plan)**

- R21 - Land at Aylsham Road – mixed use development

Other material considerations

41. **Relevant sections of the National Planning Policy Framework March 2012 (NPPF):**

- NPPF0 Achieving sustainable development
- NPPF1 Building a strong, competitive economy
- NPPF2 Ensuring the vitality of town centres
- NPPF4 Promoting sustainable transport
- NPPF7 Requiring good design
- NPPF10 Meeting the challenge of climate change, flooding and coastal change
- NPPF11 Conserving and enhancing the natural environment
- NPPF12 Conserving and enhancing the historic environment

42. **Supplementary Planning Documents (SPD)**

- Main town centre uses and retail frontages SPD adopted December 2014

Case Assessment

43. Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. Relevant development plan policies are detailed above. Material considerations include policies in the National Planning Framework (NPPF), the Councils standing duties, other policy documents and guidance detailed above and any other matters referred to specifically in the assessment below. The following paragraphs provide an assessment of the main planning issues in this case against relevant policies and material considerations.

Main issue 1: Principle of development

44. Residential - Key policies and NPPF paragraphs – JCS 4, DM15, R21, NPPF paragraphs 17, 47 -50
45. Non Residential - Key policies and NPPF paragraphs – JCS19, DM 18, DM21, NPPF 17, 23-27
46. The site forms part of a wider allocated site for mixed use development (R21) and part of the allocation specifies that development should include retail provision on the street frontage. Policy R21 also aims to deliver housing development for up to 100 dwellings, specifically to be located in the north of the site. It is envisaged that redevelopment of the site presents an opportunity to strengthen the Aylsham Road District Centre.
47. In considering the principle of the development there are two main issues to consider. The first is whether the proposed development will harm the deliverability of the remainder of the allocation for housing and the second relates to the principle of retail development on the site.

Impact on the deliverability of housing:

48. The allocation is clear that the north side of the site should be redeveloped for housing and it also states that retail development should be provided on the street frontage. The design merits of the scheme are discussed in the following section of this report, but plans show that the store is located in the south area of the site and set slightly back from the main road. In doing so the proposal avoids prejudicing the deliverability of housing on the remainder of the site.
49. Vehicular entrance to the site is provided via the existing access off Aylsham Road, but the entrance will be widened to allow access/egress and to accommodate larger servicing vehicles. The proposals retain the opportunity to provide access to the remainder of the site from the existing northern service route across Smith and Pinching land. It is envisaged that future vehicle access to the housing allocation should come via the north and from Arminghall Close, which will reduce any traffic impact on Aylsham Road.
50. It is considered therefore that the proposed store is located appropriately with respect of ensuring the deliverability of the remaining housing allocation in the northern part of the site and providing a potential link to the housing in the future.

Principle of retail development:

51. The site forms part of a mixed use residential and retail redevelopment allocation in the *Local Plan*, specifically allocation R21 within the *Site Allocations and Site Specific Policies Development Plan Document*. The store is proposed on the south of the site which is in line with the allocation to position residential in the northern part of the site. The site straddles the Aylsham Road/Glenmore Gardens Local retail Centre to the south and is adjacent to the Aylsham Road/Mile Cross District Centre to the north. Aylsham Road forms part of the major road network.
52. While the site is not located directly within a local or district centre, the fact that it straddles a local centre and is located in close proximity to a district, mean that the site is as close to being within a defined centre as possible without being located within one. The site can therefore be said to be edge-of-centre with respect of NPPF policy classification. R21 also recognises the opportunity to strengthen the Aylsham Road District Centre by providing additional retailing to meet every day needs and development in the proposed location will effectively extend the adjacent local retail centre to the south.
53. Planning permission was granted in June 2014 for a Morrison's supermarket on the application site and this permission is still extant. The approved retail use was for a larger store than that being proposed under the current application with the gross floorspace being 3,435 sq.m (compared to 2,755 sq.m under current assessment), with 423 sq.m used for the sale of comparison goods (compared to 338 sq.m under current assessment).
54. R21 states that retail development at the site will provide a maximum of 2,500 sq.m (gross) and 300 sq.m (net) for comparison goods. Policy DM18 of the *Local Plan* states that proposals for main town centre uses which are not located within a defined centre (other than those forming part of a specific development allocation within the Site allocations plan) will be permitted where:
 - (a) the proposal would not conflict with the overall sustainable development criteria set out in policy DM1 of this plan, and
 - (b) the proposal is justified by a sequential site assessment (and where applicable, impact assessment) applying to the scale of development proposed.
55. The current proposal is considered to be broadly adherent to the principles of sustainable development outlined in DM1. A sequential site assessment has been submitted with the application but a retail impact assessment is absent. Both the sequential assessment of the site and retail impacts of the proposal are discussed below.

Sequential site assessment

56. The site straddles a local retail centre and is located adjacent to a district centre. The site is also allocated under policy R21 for retail development and planning permission already exists for a larger foodstore to be constructed at the application site (13/01928/F). R21 sets maximum thresholds for gross floorspace and floorspace given to the sale of comparison goods and the current proposal exceeds these thresholds by 255 sq.m and 38 sq.m respectively. The current proposal is however considered to largely be in accordance with the allocation and given the fact that the thresholds are only marginally exceeded, that the site is allocated for

retail development in an up-to-date development plan and that planning permission already exists at the site for a larger foodstore, the need for a sequential assessment is perhaps questionable.

57. A sequential assessment was nevertheless requested in the understanding that market availability of sequentially preferential sites may have changed in the time since the approval of the 2014 approval for a foodstore at the site, especially given the smaller size of the foodstore under current assessment. It was agreed that a scaled down sequential assessment would suffice due to the proximity of the site to the local and district centre and likelihood that city centre sites would not be suitable as they wouldn't serve the intended catchment area.
58. The current application effectively represents an intended relocation of the existing Lidl store located ~300 metres south of the application site in the Half Mile Road local retail centre. The applicant contended therefore that the sequential assessment should only look at sites/premises larger than that at Copenhagen Way (or at least capable of accommodating the increase in floorspace that Lidl require to meet the needs of their customers) and that since Lidl was proposing to improve its offer on Aylsham Road, that any sequential assessment should not be remote from its existing catchment. The local planning authority agreed that this represented a reasonable basis for the sequential site assessment and a search within a one mile radius of the application site was agreed as appropriate.
59. A sequential assessment of sites within district and local centres within a one mile radius of the site was undertaken and revealed that there were no more suitable sites that were sequentially preferential to the application site.
60. Policy R21 for the 3.48 hectare site reflects a long term aspiration to regenerate and enhance the Aylsham Road district centre by providing for a mixed development of housing and retail uses, which integrates with and enhances the existing centre and its facilities.
61. The site's location adjoins an existing district centre which is identified in the hierarchy of centres in the adopted JCS (Policy 19) and is on a high frequency bus route with a high degree of accessibility to adjoining residential areas at Mile Cross and New Catton. In addition to the results of the sequential site assessment it is therefore concluded that the location is appropriate for retail development

Retail impacts:

62. A retail impact assessment was included as part of previous permission 13/01928/F for the development of a 3,435 sq.m (gross) foodstore at the site. The NPPF advises that local planning authorities should request a retail impact assessment when assessing applications for retail development outside of a defined centre which are not in accordance with an up-to-date Local Plan.
63. As previously stated, the *Local Plan* allocates the site for retail development and sets maximum thresholds of 2500 sq.m (gross floorspace) and 300 sq.m (comparison goods). The current proposal marginally exceeds these thresholds and is broadly considered to accord with the site allocation, unlike the previous application (13/01928/F), which proposed significantly more gross floorspace and sale of comparison goods.

64. The retail impacts of the previous permission were considered extensively during the 2014 assessment of the application and found to be acceptable. The previous permission is still extant, which technically would allow a retailer to implement the consent for the larger approved store.
65. The previous retail assessment looked at the impact on the Aylsham Road district centre and determined that the existing stores in the centre did not have the “range nor extent of goods needed to serve large catchments or attract people from further afield”. This influence was found to be dictated by the size of unit as opposed to the type of foodstore operator. The role of a larger store (like the approved and like the one being proposed), is to serve a much larger catchment. The impact of the larger store is unlikely to harm existing smaller outlets, which are regarded more as ‘top-up’ shops only. Further, and as stated in the assessment of the previous application, “the proposal carries the potential to enhance the vitality and viability of the adjacent centres by providing another destination for people to visit and create linked trips”. This is reflected in the wording of R21 which states that retail development will “strengthen the Aylsham Road district centre by providing additional retailing to meet every day needs”.
66. In terms of the impact on adjacent centres and nearby centres, the assessment of approved application 13/01928/F concluded that the degree of impact was acceptable and that any detrimental impacts experienced, are likely to be trade drawn away from larger nearby stores, which the NPPF encourages for promoting competitive centres. It is not considered that the impact of the smaller foodstore under current assessment will be any greater than the extant permission for the much larger foodstore as granted under 13/01928/F.
67. The proportion of convenience to comparison goods for the current proposal is 80% convenience (1,690 sq.m) to 20% comparison (338 sq.m). In order to ensure no impact on other retail units/outlets with regards to the sale of comparison goods, planning permission will be conditioned to ensure that the proportion of comparison goods sales does not exceed that which is proposed. This is consistent with the assessment made for previous permission 13/01928/F.
68. An additional factor in considering the retail impacts of the current proposal concerns the existing Lidl store on Copenhagen Way. The application states that while the current store trades well, it is now too small to accommodate Lidl’s operational model or meet customer needs. It is intended to effectively upgrade the existing store by relocating into modernised premises on the application site. This will allow Lidl to serve the same catchment area and customer base. The current proposal must therefore be considered in this context, whereby the impact of the proposal is offset by the existing Copenhagen Way store which would cease operating with the establishment of the proposed store. This of course differs from the previously approved foodstore, which presented an additional impact on top of the Copenhagen Way store, and the associated retail impacts were still found to be acceptable. The applicant’s intention to cease operating the existing store is demonstrated by a recently submitted planning application to the local planning authority, seeking outline consent for residential redevelopment of the site.

Main issue 2: Design and Heritage

69. Key policies and NPPF paragraphs – JCS2, DM3 and DM9, NPPF paragraphs 9, 17, 56, 60-66 and 128-141.
70. Policy DM3 requires development to respect, enhance and respond to the character and local distinctiveness of the surroundings. Accordingly development should be designed having regard to the context, with appropriate attention to height, scale, massing, layout and appearance. Given the location of the site immediately adjacent to designated conservation areas, design considerations are extended and strengthened, as DM9 requires development to protect and enhance the historic environment.
71. The site lies within the setting of the Grade II* listed– St Catherine’s Church, Grade II Listed St Catherine’s Church Hall and neighbouring locally listed vicarage and Mile Cross Library all located along the western side of Aylsham Road immediately opposite the application site. The Mile Cross Conservation Area is also located to the west the impact of the proposal upon the setting of the conservation area must also be considered. Aylsham Road is characterised by street trees on either side of the road which contribute positively to the landscape character of the surrounding area.
72. The site is currently occupied by mid 20th Century industrial buildings of 1-2 storey and of little architectural merit. The buildings close to the street frontage with Aylsham Road are in a state of disrepair and make a negative contribution to the character of the wider area. The demolition of these buildings is therefore welcomed and provides an opportunity to enhance the appearance of the site.
73. The current scheme proposes the erection of a 2-storey foodstore, with a rectangular form and mono-pitched roof. The building is set back from the main road and positioned at an angle to the street owing to the irregular form of the site. The front of the building is glazed and fronts onto Aylsham Road. It is proposed to provide disabled car parking in front of the store adjacent to the main entrance to the store. Landscaping is then proposed along the frontage of the site with Aylsham Road on either side of the main vehicular access to the site. The main car park is then located behind the landscaping to the side of the store. It is apparent that the design of the store is based upon the current business model for Lidl, the internal configuration of which seeks to achieve operational efficiency and functionality.
74. Allocation R21 states that development will include retail provision on the street frontage and minimise impacts on the setting of the adjacent heritage assets. The current application includes both a Design and Access Statement and Heritage Impact Assessment, which seek to justify the design of the scheme in relation to its impact on the wider historic environment.
75. It is noted that the store would be set back from Aylsham Road and further back from the street than the previously approved Morrison’s scheme (13/01928/F). The application states that the store has been pushed closer to the street in response to pre-application advice, but it was still hoped that a stronger frontage might be created by positioning the site closer to Aylsham Road and efforts were made during negotiations for the scheme to be amended to further address this issue. However, the applicant has resolved to maintain the set-back position of the store and has sought to justify the position of the store in terms of its impact on the setting

of adjacent listed buildings, continuing the historic pattern of development and providing a strong landscaped frontage to complement the character of the wider area.

76. Historic England have provided comments on the application and whilst they have not expressly objected to the proposal, do not support the current proposals because it is not felt that the current design will “not fully realise the potential to enhance the setting of the heritage assets”. It is recommended that the council consider a revised design that brings the building closer to the street to provide a more positive relationship with the public realm and to redesign the western elevation. While it is agreed that such revisions might improve the design of the scheme, negotiations to bring the store closer to the street frontage to create a stronger street presence have unfortunately not proved successful and so the application must be assessed in its current form.
77. Development at the site has historically been set back from the street frontage, with the exception of the car showroom which is a more modern addition. However, development at the site has been industrial in character and has not contributed positively to the character of the wider area. It does not therefore represent a form of development which should necessarily be echoed in the redevelopment of the site. However, it is considered that the removal of the existing buildings and redevelopment of the site as proposed will enhance the appearance of the site and setting of the adjacent heritage assets. It is noted that the listed heritage assets opposite the site are also set back from the street frontage.
78. The store is predominantly glazed on the western front elevation and is otherwise clad in modern materials. The mono-pitched roof ensures that the height of the building is minimised with the scale effectively indicative of 1.5-storey along the south boundary of the site. The design of the building is contemporary and has a regular and uniform built form. In positioning the building against the south boundary of the site, the building has been configured at an angle to Aylsham Road and a larger space is created in front of the building. While this reduces the street presence of the building, especially when compared to that created by the approved scheme, it does provide an opportunity to incorporate landscaping to the site, which will prevent a large ‘gap’ being created in the streetscape and will help to temper the impact of the development upon the setting of the neighbouring heritage assets opposite the site.
79. An attractive landscaped frontage will carry the opportunity to enhance the appearance of the site and contribute positively to the surrounding area, which is characterised by street trees and areas of landscaping in front of buildings, especially further north along Aylsham Road. As such the scheme will enhance the existing appearance of the site and subject to agreeing a detailed and high quality landscape scheme, will also enhance the character and setting of the adjacent heritage assets as well as presenting an opportunity for biodiversity enhancements and replacement tree planting.
80. Materials have been indicated on plans but a sample and manufacturers specification and joinery details for the glazed western elevation, will be required by condition to ensure a satisfactory appearance of the development.
81. Disabled parking has been provided in front of the building adjacent to the main entrance doors. Alternative locations for disabled parking were explored during the

assessment of the application which could still have provided close entry to the front doors of the store. This would have enabled the building to move closer to the street and removed any opportunity for the front of the site to be car dominated. In discussions with the agent for the application it was stated to be important to the operational requirements of the business to have parking clearly visible in front of the store. The number of disabled spaces is relatively small and it is anticipated that the spaces will not be fully occupied for the majority of time, and so it is not considered that the proposals will result in a car-dominated frontage. .

82. In summary therefore, while the concerns of Historic England are noted, subject to agreeing landscaping details, the current scheme will greatly improve the appearance of the site from its existing state and will enhance the setting of the adjacent heritage assets. The proposals are therefore considered to comply with policies DM3 and DM9 of the *Local Plan* and NPPF policy, including paragraphs 132 and 137.

Main issue 3: Trees, landscaping and biodiversity

83. Key policies and NPPF paragraphs – JCS1, DM3 and DM6 and DM7, NPPF paragraphs 109 and 118.
84. R21 states that the design of any development “should take account of trees with preservation orders at 295-297 Aylsham Road and other landscape features within the site worthy of retention”. The Tree Removal Plan shows that the trees served by TPO are located just beyond the north boundary of the application site and will not be affected as part of the development proposals.
85. The landscape character of Aylsham Road is enhanced by a healthy coverage of street trees. The Tree Protection Plan (TPP) shows that protective fencing will be placed around the street trees adjacent to the vehicular entrance to the site to ensure their wellbeing and retention. Planning permission will be conditioned to ensure compliance with the TPP.
86. In order to facilitate the development all trees within the application site are to be removed. Arboricultural information submitted with the application shows that the majority of these trees are classified as C or U (low and no retention value). There is potential for birds to be nesting in the trees and hedges proposed for removal and it is therefore recommended that the mitigation measures outlined in the ecology report are fully complied with to ensure that clearance works on these habitats are carried out outside of the bird nesting season.
87. The loss of such a large number of trees is clearly regrettable and it is therefore paramount that a suitable scheme of replacement planting be secured as part of any planning consent. Indicative landscaping schemes have been submitted with the application which generally reflect a high standard of replacement planting. However, the final landscaping scheme will be secured by condition to ensure adequate tree replacement, tree planting within the car park, suitable boundary treatments and the introduction of plant species of wildlife value. The final landscaping scheme will also ensure that an attractive frontage is created, which will enhance the setting of the adjacent historic environment as well as providing public amenity benefits. The opportunity to provide further biodiversity enhancements such as bird/bat boxes and hedgehog friendly fencing will also be explored in the assessment of the final landscaping scheme.

88. The application also includes indicative landscape proposals of the landscaped areas at the front of the site will provide connectivity to the site as well as publically accessible amenity spaces. The landscape proposals include planting around the border of the site, which will provide an ecological corridor linking to adjacent residential gardens. Subject to condition securing landscaping details therefore, the proposal carries the potential to enhance biodiversity at the site and contribute to the green infrastructure network of the city by providing areas of landscaped open spaces.
89. During the assessment of the application several trees were identified as being located on neighbouring land in the rear gardens of Palmer Road. These trees were omitted from the tree survey and concern was therefore raised that the construction works might interfere with the Root Protection Area (RPA) of these trees. The arboricultural assessment has subsequently been updated to take account of these trees. At present the trees are located behind one of the buildings to be demolished and it has not therefore been possible to fully establish the extent to which development works might affect the tree. The trees will be reviewed when the building is being demolished and assessment of the potential root impacts will be taken. The applicant is advised that once the building has been demolished, any machinery used to break up the remaining hardstanding shall work back from the boundary with Palmer Road. This will ensure that the area adjacent to the garden will not be compacted which could otherwise result in harm to these trees.
90. Planning permission will be conditioned for full compliance with the approved Arboricultural Impact Assessment (AIA) and TPP. Planning permission will also be conditioned for compliance with the mitigation and compensation/enhancement measures outlined in section 6 of the Ecological Appraisal, in order to ensure the protection of bird, bat, badger and hedgehog populations.

Main issue 4: Transport

91. Key policies and NPPF paragraphs – JCS6, DM28, DM30, DM31, NPPF paragraphs 17 and 39.
92. The application includes a Transport Assessment, which considers the transport and highway impacts of the proposal. Norfolk County Council as the Strategic Highway Authority has taken the lead in assessing the transport implications of the scheme and their comments have been supported by the council's transport officer.
93. The original submission of the scheme featured a wider vehicular access into the site and included a 'D-splitter'. It was recommended that the entrance be narrowed and the D-splitter removed and plans have subsequently been amended to reflect this. The removal of the D-splitter and narrower access carry benefits in terms of creating a shorter distance for pedestrians to walk across and for deterring vehicles from swinging into the site at high speeds. The narrowing of the access has also moved development further away from the street trees, which is beneficial for ensuring their protection.
94. The application proposes a series of off-site highway improvement works including the construction of pedestrian refuge islands, widening of existing pedestrian islands, introduction of yellow lines and the provision of a raised table across the main vehicle access to the site to aid pedestrians. Revisions to the waiting restrictions along Aylsham Road will also be required. These will be secured via a Traffic Regulation

Order as part of a scheme to be submitted to and approved in writing by the Local Planning Authority in consultation with the Highway Authority.

95. Further conditions are recommended to ensure the following:

- Appropriate gradient for the vehicle access into the site;
- Permanent availability of parking/manoeuvring area;
- That a scheme be agreed detailing provision of on-site parking for construction workers for the duration of the construction period and that wheel cleaning facilities for construction vehicles are provided.

96. Car parking numbers are ~20 spaces greater than maximum standards recommended in the *local plan*. However, the number is considered to be acceptable and mark a significant reduction in the number of spaces approved under the existing permission.

97. The County Council have confirmed that there will be no Section 106 requirement and that all off-site works, including the TRO can instead be secured by condition.

98. The application includes a Travel Plan which has been reviewed by the council's transport officer. The Travel Plan is considered to be acceptable and will ensure that the development supports sustainable modes of transport. Planning permission will be conditioned to ensure that the travel plan is implemented and carried forward. The site is adjacent to a local centre and district centre meaning it is already highly accessible by bus, walk and cycling and it will not therefore be entirely car dependent. The relocation of the store from the current site closer to the Mile Cross and Hellesdon area makes it more accessible by these sustainable modes.

99. Details of cycle parking for staff and customers will be secured by condition in order to ensure adequate specification and capacity.

Main issue 5: Amenity

100. Key policies and NPPF paragraphs – DM2, DM11, NPPF paragraphs 9 and 17.

Noise:

101. A noise assessment has been submitted with the application and has been reviewed by the council's environmental protection officer. Several residential properties neighbour the application site, most notably to the east on Palmer Road and to the south where flats are located above some of the commercial units. These properties are picked up in the noise assessment, which compares existing ambient noise levels at the nearest noise sensitive locations with the increased noise levels resulting from plant, deliveries and customer vehicle movement.

102. The proposed plant is to be located within a compound to the south-east side of the delivery bay. The noise report recommends that a 2.6m high acoustic barrier be installed on the northeast, southeast and southwest sides of the plant compound to prevent noise disturbances to the surrounding area. The report also makes recommendations for how the acoustic fences could be constructed but provides no firm construction details for either the compound or acoustic fencing. A condition is therefore recommended to require details of the compound and acoustic fencing to

be submitted to the local planning authority for approval. The noise assessment shows that with the recommended acoustic mitigation measures in place, noise from the plant measured at the noise receptor locations will actually be lower than existing night time background noise levels. A daytime assessment is then made which takes account of peak customer vehicle movements, plant noise and deliveries and shows only a 1dB(A) increase above existing background noise levels at one of the noise receptor locations (R3), measured adjacent to the north boundary of the application site in the remaining section of the allocated site. The increase in noise from plant in this location is so marginal any future residential development in this location would not be significantly affected. It is also worth noting that for the peak daytime measurements, the anticipated measurements were unchanged from existing background levels at one of the residential locations and reduced noise levels by 10 dB at the other, representing an improvement to the present noise environment.

103. An assessment was then made of the impact of night time plant noise and deliveries, which showed significant adverse impacts at two of the noise receptor locations when compared against existing background levels. The study therefore showed a potential significant noise impact from night time deliveries (23:00 – 07:00). The assessment then measures the impact of night time plant noise and deliveries against night-time ambient noise levels, which is a different way of measuring existing noise levels. Against existing ambient noise levels the noise impact of night time deliveries and plant was found to increase only marginally. The noise report contends that consideration should be given to the granting of unrestricted deliveries to the site.
104. The results of the study have been considered extensively and it is considered necessary to restrict deliveries so that none shall take place between the hours of 23:00 and 07:00 (Mon – Sat) and none between the hours of 10:00 and 18:00 (Sundays and Bank/Public holidays). The council's environmental health officer has confirmed that such restrictions will be sufficient to avoid any significant noise disturbance to the surrounding area.
105. The noise assessment also fails to set any management measures to mitigate for noise disturbances resulting from delivery activities. Given the presence of residential properties in the surrounding area and ambition for the site to the north to be developed for residential in the future, it is considered necessary to require a delivery management plan to demonstrate how noise disturbance from delivery activities will be minimised. The delivery management plan might include operational arrangements to minimise noise, including use of broadband reversing alarms, engines being turned off during deliveries, the use of a delivery shroud (linking the HGV to the delivery dock), switching off refrigeration units before delivery vehicles enter the site and contact with the store prior to entering Norwich to ensure the store is ready for the arrival.

Overbearing, outlook and overshadowing

106. The majority of residential properties in the surrounding area are located far enough away from the proposed building that they will not be affected by any impact of overshadowing or overbearing. It was noted however that several of the commercial units located on Aylsham Road to the south of the site carried the potential to have flats in upper floors, which might be affected by the proximity to the foodstore. The

applicant was advised to explore the situation and provide additional information to demonstrate the amenity impacts of the scheme.

107. It was subsequently found that that a flat exists on the upper floor of 291 Aylsham and a plan was produced which illustrates the relationship between 291 Aylsham Road and the proposed application building. While the proposal will affect views from 291, there is a distance of 17.5 metres looking directly forward from the nearest window to a habitable room. The lowest height of the building is positioned adjacent to the southern boundary of the site and it can be seen that outlook will not be significantly affected. Furthermore, the space between the rear of 291 and the application site is occupied by single storey buildings associated with the commercial use of the premises and then service yard. The space between does not serve as a residential garden, which might otherwise be affected by overbearing. This also means that any impact of overshadowing will not be significant, because it will largely impact upon the roofs of the single-storey buildings and only during morning/early afternoon hours.

Main issue 6: Contamination

108. Key policies and NPPF paragraphs – DM11, NPPF paragraphs 120-122.
109. Significant and known groundwater and soil contamination risks have previously been identified at the site, associated with the old fuel depot. As part of previous application 13/01928/F, considerable site investigation, detailed Quantitative Risk Assessment and remediation was undertaken to deal with hydrocarbon contamination of soils and groundwater at the site.
110. The EA initially submitted a holding objection to the proposal subject to the applicant providing documents for the assessment of contamination at the site which formed part of the previous application. The drainage strategy and ‘assessment of residual risk and mitigation measures’, documents were subsequently submitted to the EA and the holding objection has been removed.
111. The EA have recommended two conditions to deal with the remaining contamination on site. They also offer advice to the applicant and local planning authority in respect of the soakaway location and surface water management strategy. It is recommended that a revised surface water drainage strategy be agreed by condition to address the recommendations of the EA. This will include the need to undertake soil testing at the base of the proposed soakaway to ensure the installation is into natural (impacted) ground. If any unexpected contamination is found then it will be necessary for the results and details of contamination removal be submitted to the local planning authority for approval prior to commencing development. The EA also advise that the SUDS management train for the interceptor is revisited to ensure any hydrocarbons are adequately treated prior to discharge.

Compliance with other relevant development plan policies

112. A number of development plan policies include key targets for matters such as parking provision and energy efficiency. The table below indicates the outcome of the officer assessment in relation to these matters.

Requirement	Relevant policy	Compliance
--------------------	------------------------	-------------------

Cycle storage	DM31	Yes subject to condition
Car parking provision	DM31	Yes subject to condition
Refuse Storage/servicing	DM31	Yes subject to condition
Energy efficiency	JCS 1 & 3 DM3	Yes subject to condition. An energy scheme has been submitted with the application but relates to a different site. The applicant has agreed that a site specific energy scheme should be agreed by condition and this will need to demonstrate that 10% of the site's energy will come from decentralised and renewable or low carbon sources.
Water efficiency	JCS 1 & 3	Yes subject to condition
Sustainable urban drainage	DM3/5	Yes subject to condition. While a drainage strategy has been submitted with the application, revisions are needed to address the recommendations of the EA
Flood risk	JCS1 DM5	Yes. The drainage strategy will ensure that the development does not increase the risk of surface water flooding in the surrounding area. Norfolk County council as Lead Local Flood Authority have reviewed the application and confirmed that they have no comments to make.

Equalities and diversity issues

113. There are no significant equality or diversity issues. Level access is provided into the store and disabled parking is provided adjacent to the main entrance to the store.

Local finance considerations

114. Under Section 70(2) of the Town and Country Planning Act 1990 the council is required when determining planning applications to have regard to any local finance considerations, so far as material to the application. Local finance considerations are defined as a government grant or the Community Infrastructure Levy.

115. Whether or not a local finance consideration is material to a particular decision will depend on whether it could help to make the development acceptable in planning

terms. It would not be appropriate to make a decision on the potential for the development to raise money for a local authority.

116. In this case local finance considerations are not considered to be material to the case.

Conclusion

117. The development is in accordance with the requirements of the National Planning Policy Framework and the Development Plan, and it has been concluded that there are no material considerations that indicate it should be determined otherwise.

Recommendation

To approve application no. 16/00606/F - 297 Aylsham Road Norwich NR2 3RY and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Details – materials and joinery;
4. Comprehensive landscaping scheme (soft and hard) – to include boundary treatments, high quality landscaping to frontage, biodiversity enhancements, replacement tree planting including trees to be planted in the car park, seating, trolley parking;
5. Compliance with the AIA and TPP;
6. The development shall provide a maximum of 1,690sq.m. net retail floorspace, of which no more than 338sq.m. / 20% floorspace shall be used for comparison goods sales, whichever is the greater;
7. There shall be no future subdivision of the retail store into smaller units;
8. There shall be no mezzanine floorspace added to the store, even through the usual permitted development allowance of 200sq.m;
9. There shall be no use of the comparison goods floorspace separately from that of the main retailer or as a separate unit / via a separate entrance;
10. Restriction on store opening hours – 07:00-22:00 (Mon-Sat), 10:00-17:00 (Sun);
11. Restriction on delivery hours – No trade deliveries or collections, including trade waste or clinical waste shall take place before 07:00 hours and after 23:00 hours (Mon-Sat) and not before 10:00 hours and after 18:00 hours (Sundays and Bank Holidays);
12. Plant compound and acoustic fencing- full details and provision;
13. Delivery management plan;
14. Cycle parking and servicing details;
15. No occupation shall take place on the site until the changes to waiting restrictions facilitated by a Traffic Regulation Order have been secured by the Highway Authority. No occupation of the development hereby approved shall take place unless and until the provisions required within the Traffic Regulation Order have been put in place;
16. Gradient of the vehicle access shall not exceed 1:12 for the first 10 metres into the site;
17. Prior to the commencement of the use hereby permitted the proposed access / on-site car and cycle parking / servicing / loading, unloading / turning / waiting area shall be laid out, demarcated, levelled, surfaced and drained in accordance with the approved plan and retained thereafter available for that specific use;

18. Development shall not commence until a scheme detailing provision for on site parking for construction workers for the duration of the construction period has been submitted to and approved in writing by the Local Planning Authority. The scheme shall be implemented throughout the construction period;
19. (a) No works shall commence on site until the details of wheel cleaning facilities for construction vehicles have been submitted to and approved in writing by the Local Planning Authority in consultation with the Highway Authority;
- (b) For the duration of the construction period all traffic associated with the construction of the development permitted will use the approved wheel cleaning facilities provided referred to in part (a);
20. (a) Notwithstanding the details indicated on the submitted drawings no works shall commence on site unless otherwise agreed in writing until a detailed scheme for the off-site highway improvement works as indicated on drawing number SCP/16013/SK02 Rev A (as included within the Transport Statement) have been submitted to and approved in writing by the Local Planning Authority in consultation with the Highway Authority;
- (b) Prior to the commencement of the use hereby permitted the off-site highway improvement works referred to in Part (a) of this condition shall be completed to the written satisfaction of the Local Planning Authority in consultation with the Highway Authority.
21. Details of plant and machinery
22. Details of ventilation & fume extraction
23. Upon first use of the store, the Travel Plan to be implemented and carried forward;
24. Unknown contamination;
25. Surface water drainage strategy. No systems to be installed until details have been agreed. Details must address issues raised by the EA and demonstrate no resultant unacceptable risk to controlled waters.
26. Energy scheme;
27. Water efficiency details and provision;
28. Compliance with mitigation measures in ecology report;
29. Provision of charging point for vehicles (minimum of three).

Informatives:

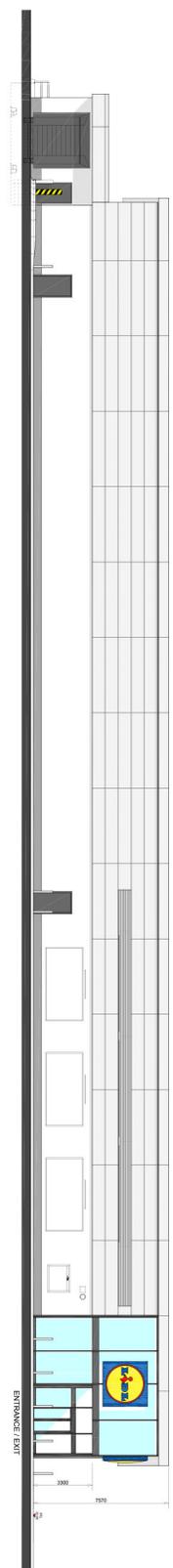
- 1) An application to discharge trade effluent must be made to Anglian Water and must have been obtained before any discharge of trade effluent can be made to the public sewer. Anglian Water recommends that petrol / oil interceptors be fitted in all car parking/washing/repair facilities. Failure to enforce the effective use of such facilities could result in pollution of the local watercourse and may constitute an offence. Anglian Water also recommends the installation of a properly maintained fat traps on all catering establishments. Failure to do so may result in this and other properties suffering blocked drains, sewage flooding and consequential environmental and amenity impact and may also constitute an offence under section 111 of the Water Industry Act 1991."
- 2) EA – Advice for LPA (first sentence)
- 3) Detailed landscaping plan indicative only and needs to make provision for adequate tree replanting and trees in car park

- 4) Advertising – needs to form part of a separate application
- 5) In addition to the need to explore biodiversity enhancements and adequate replacement tree planting, the final landscaping scheme should provide a strong front boundary line with the introduction of low level hedging or similar.
- 6) The applicant is advised to refer to BS8545:2015 'Trees: from nursery to independence in the landscape', with respect to ensuring that the tree planting to be detailed on the final landscape plan is of a high quality.
- 7) The applicant is advised that once the building adjacent to Palmer Road has been demolished, any machinery used to break up the remaining hardstanding shall work back from the boundary with Palmer Road. This will ensure that the area adjacent to the garden will not be compacted which could otherwise result in harm to these trees.
- 8) Works to highway
- 9) Works to highway

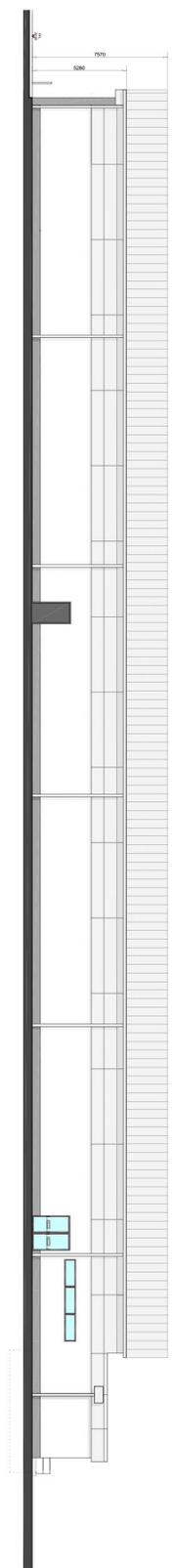
Article 35(2) statement

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined in the officer report.

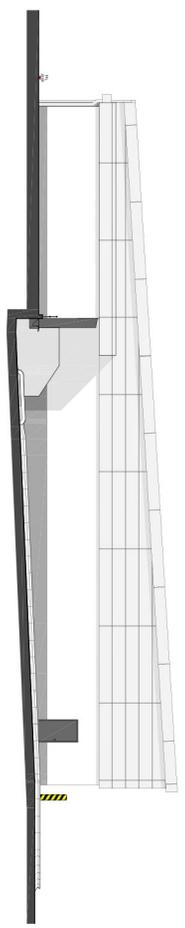
18/000
NORTH ELEVATION



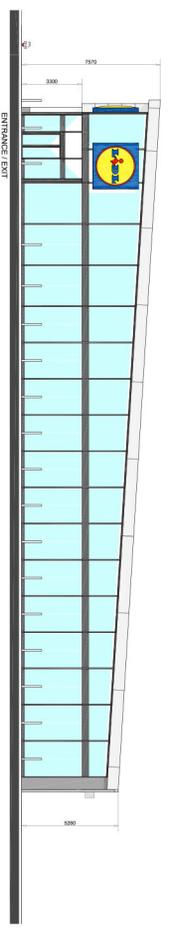
18/000
SOUTH ELEVATION



18/000
EAST ELEVATION



18/000
WEST ELEVATION



PROPOSED MATERIALS

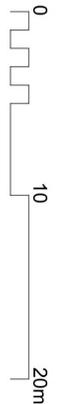
WALLS
 200mm concrete masonry covered with RAL 8010
 with two wheel paths in grey RAL 7028

4th & 5th Floor Building Systems Freedom 1
 aluminium cladding with aluminium
 in powder coated aluminium finish RAL 7024

ROOF
 Standing seam aluminium sheet roofing
 in silver RAL 9006 finish

SHOPFRONT GLAZING, ENTRANCE / EXIT
 Powder coated aluminium finish double
 glazed system in Graphite Grey RAL 7024

EXTERNAL DOORS
 Single external escape door and automatic door
 in aluminium in Graphite Grey RAL 7024



POOLE & PATILE chartered architects

1100 @ A0

MAR 2018

3398 08 A

SITE AT AVISHAM ROAD, NORWICH

ELEVATIONS AS PROPOSED



SCHEDULE OF ACCOMMODATION

	GIA ground	2,555	sq m
	GFA ground	2,636	sq m
	Sales area	1,690	sq m
	Warehouse	467	
	Bakery Prep	84	
	Bakery Warehouse	64	
	Additional Chillers	64	
	Office	9	
	Public wc, etc	33	
	Utility	14	
	Circulation	67	
	Internal partitions	63	



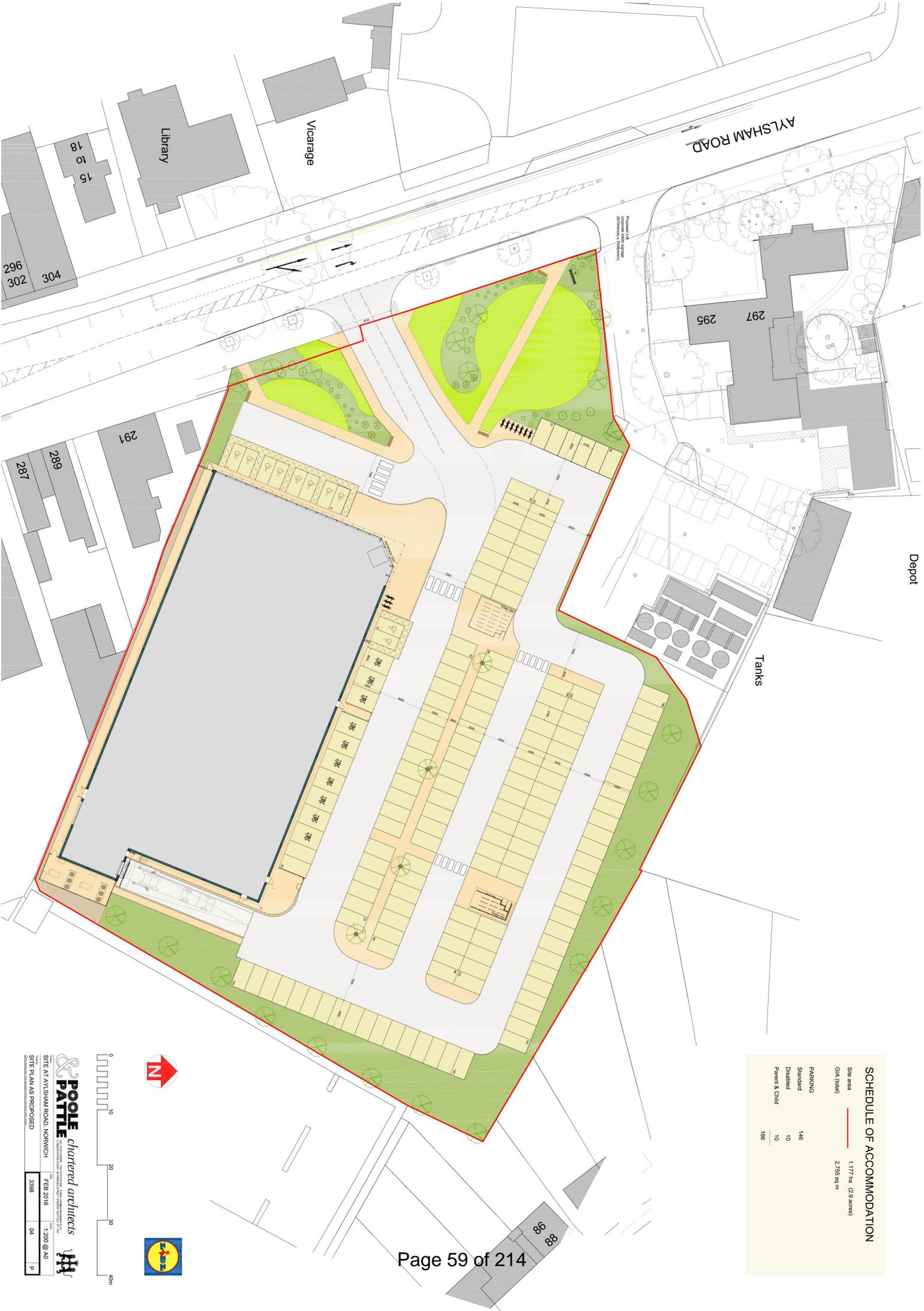
POOLE *chartered architects*

TELEPHONE: 01628 821111 FAX: 01628 821112
 EMAIL: info@poolepatterns.co.uk
 WEBSITE: www.poolepatterns.co.uk

SITE AT AYLSHAM ROAD, NORWICH

DATE	DESCRIPTION
FEB 2016	1:200 @ A3
3/8/8	05

GROUND FLOOR PLAN AS PROPOSED



SCHEDULE OF ACCOMMODATION

Site area: 1.177 ha (2.9 acres)
 GIA (total): 2,756 sq m

PARKING	146
Standard	10
Disabled	10
Parent & Child	166



POOLE PATTLE chartered architects
 1200 @ 40
 FEB 2016
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Report to Planning applications committee
 13 October 2016

Report of Head of planning services

Subject 16/00456/F - BT Telephone Exchange Westwick House
 70 Westwick Street Norwich NR2 4SY

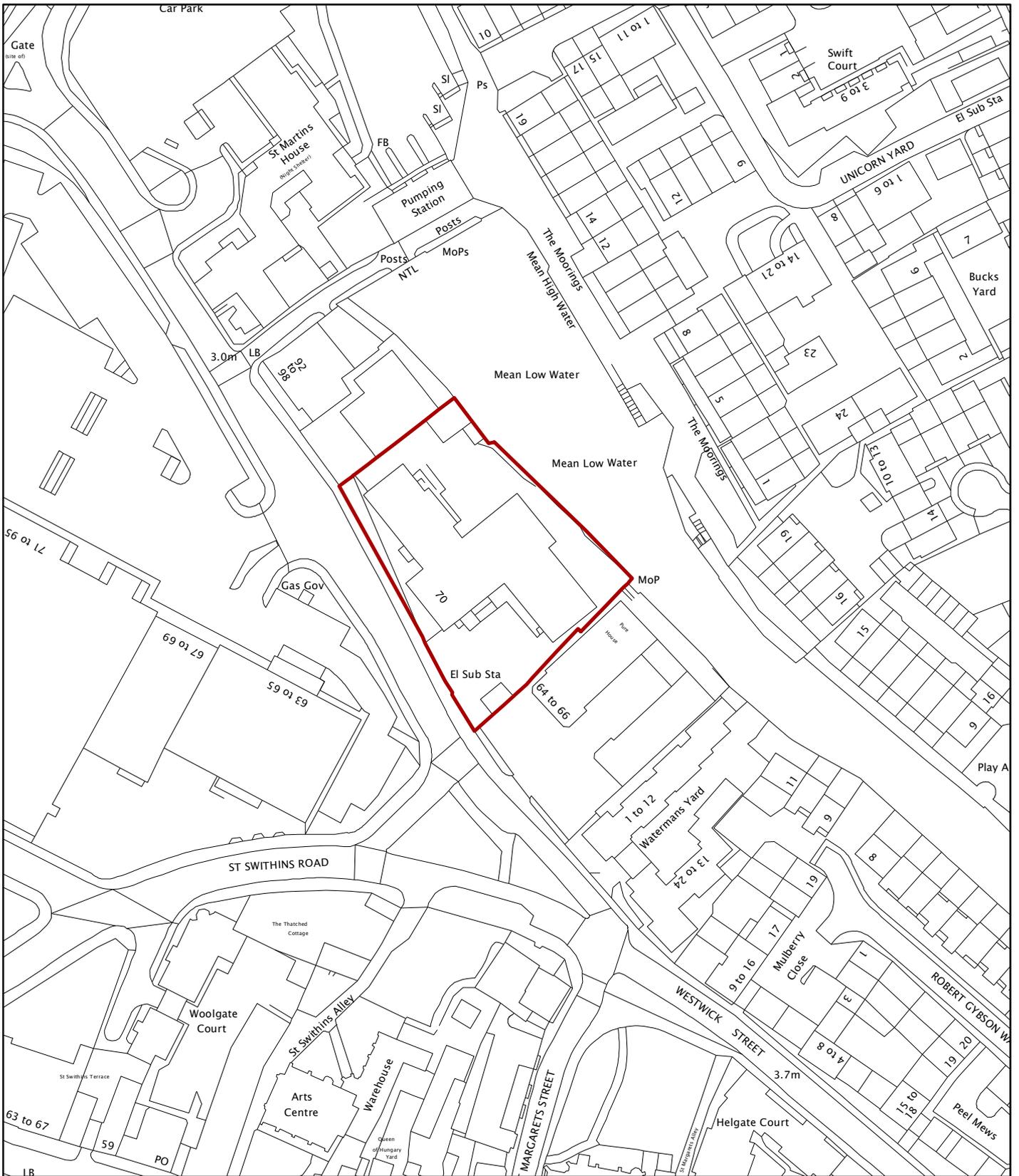
Reason for referral Objections

Item
4(b)

Ward:	Mancroft
Case officer	Mr Lee Cook - leecook@norwich.gov.uk

Development proposal		
Demolition of former Norwich Telephone Repeater Station and redevelopment of site to provide 42 dwellings with associated amenity areas, car and cycle parking and pedestrian and vehicular access.		
Representations		
Initial proposal		
Object	Comment	Support
2	1	1
First revised proposal		
Object	Comment	Support
2	1	0

Main issues	Key considerations
1 Principle	Provision of housing; Loss of employment use; Flood risk
2 Heritage	Demolition of existing buildings; Impact on the setting of listed buildings close to site; Design in the context of surroundings including the conservation area.
3 Design	Scale, appearance, layout. Space/design standards. Amenity space. Character of area.
4 Amenity	Impact on amenities of neighbouring properties (outlook, privacy, building impact). Amenity spaces. Business impacts on future residents.
5 Landscaping and open space	Streetscape, open space, planting mitigation and appropriate screening.
6 Transport	Provision of parking and servicing. Suitable access.
7 Viability	Whether provision of affordable housing is viable
Expiry date	29 June 2016
Recommendation	Approve subject to S106 agreement



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Planning Application No 16/00456/F

Site Address

BT Telephone Exchange

Westwick House

70 Westwick Street

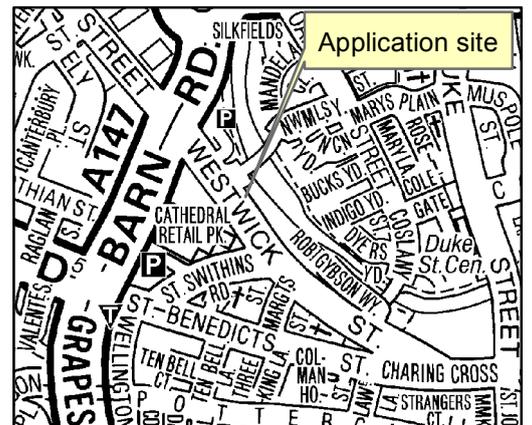
Scale

1:1,250



NORWICH
City Council

PLANNING SERVICES



The site and surroundings

1. The application site is located on the north-eastern side of Westwick Street to the west of the City centre. The north side of the site is bounded by the River Wensum. On the opposite side of the river are recent areas of residential development. Westwick Street runs in a north west to south east alignment. The site has two vehicular access points from Westwick Street into approximately 16 standard car parking spaces and one disabled space.
2. The site is located in close proximity to a number of key connection routes into and out of the city. To the west is the junction of Westwick Street with the Inner Ring Road. Pedestrian and cyclist access is available to the north of the river across New Mills Yard Bridge. Surface parking is located adjacent to the ring road and a number of one and two storey scale buildings run up to the site on the north side of the street. The existing site building is a one and two storey structure ranging from approximately 7.25m for curtilage buildings and 9.85m stepping to 11.35m to 13.24m tall for the main building with additional basement and overall designed for the purposes of accommodating the Telephone Repeater Station. Other storage buildings are located on the site.
3. To the south west of the site is the Cathedral Retail Park which includes large scale buildings housing retail shops such as Toys R Us, Matalan and Farm Foods. Either side of these buildings are in use as further surface parking/hard standing. To the south is St Benedict's Street, which runs eastwards from the ring road junction of Dereham Road, Barn Road and Grapes Hill to where Westwick Street meets Charing Cross.

Constraints

4. The site is within the City Centre Conservation as part of the Northern Riverside Character Area and within the area of main archaeological interest (DM9). Nearby listed buildings include the New Mills Pumping Station which sits on the bridge to the North and further to the south is the former Bullards 'Anchor' Brewery building adjacent to St Miles Coslany Bridge. Key landmarks in the area viewed across the site include City Hall, the Roman Catholic Cathedral and the church towers of St Giles and St Gregory's. The end of St Benedicts is marked as a "gateway" to City centre (DM3).
5. The Environment Agency flood risk map shows that the development is within flood risk zone 1 but part of the site lies within flood risk zones 2 and 3. The site also falls within a critical drainage area (DM5). The north edge of the site is part of the river wall. The Broads Authority is local planning authority for development on the river.
6. An electrical sub-station sits within the site on its south-east corner. This substation, whilst within the ownership of the applicant Telereal Trillium, is subject to a long lease to the statutory undertaker UK Power Networks.
7. The site is within an area for reduced car parking (DM29). The existing riverside walk is located to the north side of the river (DM28). The nearby public car parks are designated under site allocations plan for City centre site specific allocations under sites CC22 (Barn Road Car Park) and CC30 (Westwick Street Car Park). The Retail Park is designated as part of a secondary retail area (DM18, DM20).

Relevant planning history

8. The site is occupied by a purpose designed telephone repeater station dating from the mid-20th Century. The building is sui generis in use not being a specific industrial building as designated within the Use Classes Order although did hold ancillary office space. Earlier planning history relates predominantly to minor changes to this building and is not directly related to this planning application.
9. A pre-application presentation related to proposals for the site was made to Members of planning applications committee in September 2015.

The proposal

10. The scheme includes the demolition of the former Norwich telephone repeater station and redevelopment of the site to provide 42 dwellings. with associated amenity areas, car and cycle parking and pedestrian and vehicular access.

Summary information

Proposal	Key facts
Scale	
Total no. of dwellings	42 units, with a mix of 9 x one bed flats and 33 x two bed flats. One bed flats are 2 person (1 double bedroom). Two bed flats are 3 person (1 single, 1 double (nine flats in total)) and 4 person (2 doubles (twenty four flats in total)).
No. of affordable dwellings	None on site – offer of 10% of a contribution for affordable housing provision off-site via a commuted sum.
Total floorspace	Gross internal floor area of approximately 3603m ² .
No. of storeys	Four storeys with pitched roofs for east block. For west block this rises from three storeys (with roof garden) next to the river, stepping four up to five storey with flat roof on south-west corner and five storey within pitched roof/dormers central to the site along Westwick Street.
Max. dimensions	Blocks approximately for north-south aligned elements (east block) 10m wide x 31.5m deep and (west block) approximately 10.6m wide x 29.7m deep. For side wings (east block) 14.8m wide x 10.7m deep and (west block) 17m wide x 9.8m deep. Plus balconies for each element. For height above existing levels (east block) 15.265m to ridge; (west block) stepping from three storeys adjacent river 10.35m to parapet, four storey 13.055 to parapet and five storey 16.255 to parapet/ridge.
Density	Site area of approximately 0.25 hectares. Overall density approximately 168 dwellings per hectare (dph).

Appearance	
Materials	Red facing brick with brickwork detail to walls and openings. Mix of open/recessed balconies. Metal standing seam to roofs and metal accent panels to walls.
Construction	Fabric first approach to enhance the overall energy performance of the scheme. Materials specified to have lower environmental impact ratings, as set out in the construction Green Guide
Energy and resource efficiency measures	PV scheme to deliver 11% of the sites energy requirement from on-site renewable technology. Water efficiency targets. Specification of a site waste management plan. Planning of material quantities and delivery timings. Where possible, use of locally sourced materials.
Transport matters	
Vehicular access	Rearranged via revised/new access point from Westwick Street. New pedestrian/cycle linkages through to the river edge. This will not form part of any riverside walk as this is provided within areas north of the site.
No of car parking spaces	16 car parking spaces including 2 disabled spaces. Electrical charging point with two outlets.
No of cycle parking spaces	56 spaces within covered cycling racking building plus Sheffield cycle hoops providing for 24 visitor bikes.
Servicing arrangements	Communal bin store provided adjacent to Westwick Street. Internal layout allows for emergency vehicle access and turning within the site.

Representations

11. Advertised on site and in the press. Adjacent and neighbouring properties have been notified in writing.
12. 2 letters of representation, 1 observation and 1 comment of groups or societies have been received in response to the initial scheme. 2 letters of representation and 1 observation have been received in response to the revised proposals citing the issues as summarised in the table below. All representations are available to view in full at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Issues raised initial proposed scheme	Response
The proposed development, by reason of its layout, height, scale and massing would be an unduly dense and visually dominant form of development, with buildings of excessive mass and scale	Main issue 1, 2, 3 and 5

which are out of character with the ancient heritage of this Main Gateway to the City, the existing Conservation Area, Westwick Street, Anchor Brewery, New Mills Pumping Station and the River Wensum.	
Proposals represent an inappropriate overdevelopment of the site. The proposed construction is out of scale and context with the rest of the area and overbearing. In particular height, flat roof and associated roof top utilities, is out of keeping with the Moorings development opposite. Would leave a feeling of claustrophobia and enclosure for the current residents. The plans require substantial modification in terms of height and general scale.	Main issue 1, 2, 3 and 4
Proposals are contrary to policies DM3 (delivering high quality design), DM9 (safeguarding Norwich's heritage), DM12 (ensuring well planned housing development) and DM13 (communal development and multiple occupation) of the Norwich Development Management Policies Local Plan and the Conservation Area Appraisal.	Main issue 1, 2 and 3
NPPF and DM9 require all development to have regard to the historic environment and maximise opportunities to preserve, enhance or better reveal the significance of designated assets. Rooted in Planning (Listed Buildings and Conservation Areas) Act 1990 which imposes a duty on Local Authorities to have special regard to development affecting Listed Building and their settings and Conservation Areas.	Main issue 2 and 3
Should be rejected and to resubmit proposals that includes a balanced and accurate Heritage Statement, redesign for a maximum of 4 storeys to Westwick Street and maximum of 3 Storey to the Riverside. Designed to be commensurate with the historic past of the area with terraced mixed pitched roofs utilising gables, hips, dormers, mansards and lucans with the introduction of timber cladding into the fold of vernacular materials.	Main issue 1, 2, 3 and 5 Scheme assessment is based on submitted proposal
Agree removal of negative buildings and replacing with appropriate housing would be positive but development needs to have particular and specific regard to the heritage of this area, developments executed so far and those intended in the future.	Main issue 1, 2 and 3
Block 1 is industrial, flat roofed and angular in appearance creates an alien structure of unnecessary dominance and tower appearance. Historically there is little evidence that such industrial tower like flat roofed structures existed at all on the river, even on Riverside.	Main issue 2 and 3
Block No 1 will be in direct conflict with the open Gateway to the City at Upper and Lower Westwick Street including St Benedicts.	Main issue 2 and 3
Planning policies managed to retain heritage of this area and any new developments have sympathetically been integrated of which Watermans Yard is a good example along with the bottom of St Benedicts which has included a sympathetic as evidence of the Old St Benedicts Gate.	Noted
Developments of the Barn Road and Westwick Street car parks based upon interpretation of the historic and conservation area with reference to 4 and 3 storey dwellings with a taller structure on the Westwick Street /Barn Road junction. 6 storey high tower	Main issue 2, 3 and 5

on Westwick Street will have a detrimental effect on any planned future development in particular the marking of one of the Historic Gateways to the City (Heigham Gate).	
Some of the existing mill buildings would have been higher along the river, buildings fronting Westwick Street would have not been 4 storeys. Suggest that set back of Block 2 should be maximum of 3 storeys. Original proposals were for 3 Storeys and revised without any explanation.	Main issue 2 and 3
Block 1 river frontage accords exactly with the existing height of the flat roofed visually obtrusive TRS building, then proposes to step up to 6 storeys. Has the effect of replicating to a larger scale negative structure of the existing TRS.	Main issue 2, 3 and 5
Heritage Statement fails to consider the Conservation Area as a Major Gateway or importance of designated heritage assets and does not adequately assess the impact of the proposed development.	Main issue 2 and 3
Development in close proximity to listed New Mill, Anchor Brewery and Conservation Area will have adverse impact which cannot be balanced by the new housing. The location introduces significant strong design considerations which have not been addressed and there is insufficient reasons to forgive the substantial detriment that would be caused.	Main issue 1, 2 and 3
Accepted that the Yard will give vision lines and breaks the site up but the appearance should be more like the photographs of Barkers Yard. Massing and height of Block 1 will result in a substantial change of the site views from further afield; major City landmarks will be obstructed and this will result in substantial harm. Development at the Westwick St side of the tiered scheme will impede the valuable amenity of view to the Roman Catholic cathedral. Proposals do not respond positively to the area, Norwich City or its Residents.	Main issue 2, 3 and 5
There will be an unacceptable impact on the amenities as a result of loss of light, overshadowing, overlooking and loss of privacy. Overlooking will be possible from the proposed windows and balconies. Change in outlook will be substantial, views to the Catholic Cathedral and City Landscape replaced with urban development.	Main issue 2 and 4
Should provide for 33% of dwellings to be affordable. The scheme does not attempt to provide for any affordable housing or any form of contribution. Policies require affordable housing be provided for reasons of integration and other social benefits and this should not be deferred or bought out.	Main issue 7
Applicant suggests they have publicly consulted the residents and proposals reflect their views. Not true; of the 21 negative comments listed there is no evidence of any being addressed.	Noted
Issues Raised 2nd proposed scheme	Response
Repeat of general comments above	See above
Revised proposal do nothing to improve or address the contravention of planning policies and guidelines as previously pointed.	Main issue 2, 3 and 5 Scheme

	assessment is based on submitted proposal
Contemporary treatment and materials is outwit of the traditional approach and vernacular materials.	Main issue 2 and 3
Have made no attempt to revise their Heritage Statement to encompass this important historic area. Should include as a minimum a balanced and accurate Heritage Statement and redesign.	Scheme assessment is based on submitted proposal
There is no large scale development in this Conservation/Heritage area. Introduce further significant strong design considerations, which still have not been addressed within the revised design proposals.	Main issue 2, 3 and 5
Not within your and government policies to replace 1 negative with this development, which if approved will be another negative structure.	Main issue 2 and 3
Revised proposals go no way to satisfy any of the concerns that we previously raised, other than to remove the roof plant.	Main issue 2, 3 and 5 Scheme assessment is based on submitted proposal

13. **Norwich Society:** We are impressed with this proposal which is imaginative, a good scale and will help to enhance the area.

Consultation responses

14. Consultation responses are summarised below the full responses are available to view at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Anglian Water

15. No objection in principle. Comments provided on local assets, foul drainage capacity, foul sewer connections, surface water disposal and connection should SW treatment change and also suggested informatives.

Broads Authority

16. No objection in principle. Comments on design that it is regrettable that view through to river is limited but overall layout when viewed from river is welcomed. Layout within central amenity space would help reduce impact and dominance of development on the river environment. Design and addition of balconies will encourage an active interface between new units and river. Also appreciated that buildings step back from the river corridor and avoid canalisation of this stretch of the river. Supports provision of public amenity space but questioned extent of riverside space and whether any encroachment onto river and navigation areas. Inclusion of landscape welcomed but asked for better screening of parking. Requested enhancements for bats and swifts.

17. On revised scheme noted that application has been the subject of substantial pre-application advice and the submitted scheme appears to have incorporated the majority of advice given. The position of the buildings on the site, and their relationship to each other creates a feeling of openness on the site and provides views through the site from Westwick Street to the river. This is welcomed. The setting back of the buildings from the river frontage, together with their height and massing avoids the canalisation of the river environment which is supported. The variation in the treatment of the roof types of the two blocks is in accordance with previous advice. Creation of a public amenity space on the river bank is welcomed, providing care is taken to ensure that there is no encroachment into the river corridor. Important to ensure in the detailed landscape proposals that views from the river of the cars parked on site are filtered. Broads Authority is generally happy with the development as proposed and has no further comment.

Design and conservation

18. No objection in principle. Detailed comments provided on Block 1 – Design. Building height. Demolition. Window openings. Large cut out. Weak corner. Roof plant – height. Proximity to river – extra modelling requested. External amenity space – limited. Extent of parking and visibility from river. Natural light to stair wells. Recommendations to remove plant area from roof; set back 3 storey element from river edge – boxy form as viewed from bridge; cut out terraces omitted and internal space created; obscure parking from views. Block 2 – Variety in scale. Natural light to stair wells. 3 and 4 storey rather than just 4. Materials/bond – condition. Metal cladding. Framing of fenestration. Splayed openings. Suggest no soldier course. Asked to demonstrate view straight across.
19. The demolition of the existing building is not opposed. The proposed materials and contemporary style of the replacement building is acceptable, however the proposed scale, height, bulk appears to have increased since pre-application. At present, it is questionable as to whether the development takes sufficient opportunities to ‘positively contribute to local character and distinctiveness’ in accordance with paragraph 126 of the NPPF. Whilst I would agree that the harm caused to the heritage assets is ‘less than substantial’, improvements to the existing design should be secured and relatively modest revisions could reduce the level of harm caused in order for it to be sufficiently outweighed by public benefits in accordance with paragraph 134 of the NPPF.
20. Advice given to applicant on suggested revisions to scheme in terms of detail and building height/roof top plant on Westwick Street elevation. Commented on interim design development. No further comments following submission of revised scheme which has had regard to earlier recommendations.

Historic England

21. No objection in principle. Application proposes the development of two multi-storey residential blocks on prominent site in the Norwich conservation area. The development has the potential to affect views of the wider area which features numerous landmark listed buildings. Are broadly content with the proposed development and its design, although attention to detail and particularly the quality of the masonry will be essential if the design is to be successful. The tallest part of the Westwick Street elevation is potentially out of scale with the surroundings and the new buildings will not fully define the waterfront with a building line due to the

way the internal courtyard between the buildings opens onto the riverside. Recommend the height Westwick Street elevation and ways the buildings relate to the riverside are given further consideration. Do not consider the development would result in harm to the significance of the conservation area and nearby listed buildings in terms of the NPPF so as to merit an objection we would recommend these issues are given further consideration.

22. Following revisions commented that the amendments contain some positive changes, including the reduction of the roof top plant enclosure and addition of a masonry screen at the Westwick Street side. I earlier commented on the degree of enclosure to the riverside of the development and while this has not been addressed in amendment of the built form the additional attention to the definition of the courtyard parking area on this side could help improve the riverside experience. Overall I welcome the amendments and while I would still like my earlier comments to be considered would not wish to object to the granting of consent.

Environment Agency

23. No objection in principle. Comments on ecology in relation to encouraging riparian species planting which provide attraction to pollinators and as a food source for birds/bats and for prevention of spread of invasive non-native species. Suggest condition for water framework directive compliance to allow protection of local assets and to encourage movement of species between suitable habitats. Suggest requirement for biodiversity enhancements and protection birds/bats.
24. Provided guidance on SUDS and, to avoid risk to the environment, suggest contamination condition. Identified flood area and advised that submitted flood risk assessment provides information necessary to make an informed decision. Noted finished floor levels which are in line with NPPG on probability events and noted emergency flood plan and have no objection subject to LPA satisfaction of suitable flood evacuation exists for lifetime of development. Advises that environmental permit might be required for works within 8m of the top of the bank of the designated main river. Provided guidance on requirements for waste exemption or permit from the EA, sustainability and climate change.

Environmental protection

25. No objection in principle. Noted findings of submitted reports and site not grossly contaminated. Report suggests potential pollution of controlled waters is low. Asked for EA sign off on contamination information. Some initial findings in relation to site works but request remediation method statement is developed to cover all points and ground gas. Suggests conditions related to contamination and importation of soil/material. Agrees conclusions of noise report for protection from noise for suitable design of building fabric and asks for development to adhere to report recommendations.

Environmental services team

26. No objection in principle. Recommendations on bin sizes and numbers.

Highways (local)

27. No objection in principle. Considers that overall the proposed residential use is acceptable in terms of traffic impact and landscape layout is excellent. Proportion

as % of parking to dwellings is acceptable for location. Suggest details required of parking management strategy e.g. management company or parking barriers and posts. Comments on cycle parking numbers acceptable in principle subject to detailed design. Requires bin area to be a secure store to avoid anti-social behaviour. Requests EV points to be increased. Waiting restrictions will require review in vicinity of site – condition suggested. Comment on street trees and that planting on forecourt would be acceptable as an alternative. Reminded that street naming cannot be determined by the developer and marketing. Suggested informatives.

28. Amended proposals as resubmitted are welcome. Noted the bin store appears to be unsecure; in the city centre this can attract anti social behaviour – the store should be secured with key pad entry locks. Tiered cycle parking is welcome; requests details that the tiers can be deployed without conflict with the opposing racks. Noted provision for only one electric car charging point and requested more within the space. Further informative: Properties at this development will not be entitled to on street parking permits.

Housing strategy

29. No objection in principle. Involved in discussions relation to viability including affordable housing values; marketing and sales agent fees; CIL; vacant building credit; cashflow and reduced interest payments; profit levels; and on what social housing may be achievable either on site or as a commuted sum.

Landscape

30. No objection in principle. Submitted proposals demonstrate layout provides openness through the courtyard and respects key views of local landmarks, extra visualisation should be provided for the more direct view from Westwick St to the river. Queried extent parking. Good level of amenity space is provided by terraces, decks, balconies, river edge etc. which is welcome. Queried amenity space for units A.0.5 lawn area allocated; A.4.2; A.4.1; A.4.3. Consideration needed to edge treatment to terraces/gardens to hide any clutter. Riverside amenity space is welcomed. Advised to seek to maximise amenity function – increase communal space area mainly by review of parking numbers and layout and location cycle store, suggested these are split or moved. Introduction of greenery into pavements is welcomed subject to details. Hard landscape approach is acceptable and support introduction of trees into the scheme. Would encourage choice of planting to benefit wildlife. Confirm that species listed are not part of the schedule 9 of the Wildlife and Countryside Act and have no objection to inclusion of non-native species to ensure a balance between aesthetics, functionality and biodiversity is reached. Suggest a native mix of planting next to the river. Requested landscape details are conditioned - detail grass grid system; tree choice; increase native mix adjacent river. Asked for a review of the line of entrance wall and size of some landscape spaces to increase planting and amenity opportunities.
31. On revised scheme happy that the proposals now address the landscape issues raised. The general principles for the landscape shown on the Proposed Landscape Plan 14-0115-210-A are acceptable, subject to detailed design and suggests that the standard landscape conditions are applied to any approval given, including condition for landscape management/maintenance. Comments provided

previously specifically for the landscape proposals still apply Landscape details would need to be conditioned as part of any approval.

Norfolk county planning and flood & water management team

32. Confirmed has no comments to make.

Norfolk historic environment service

33. No objection in principle. Standard condition in respect of archaeological investigation must apply (AH1). A photographic record of the building should also be secured.

Norfolk police (architectural liaison)

34. No objection in principle. Have provided detailed comments in relation to secured by design criteria in particular policy guidance and on construction design points e.g. access control, mail delivery to flats and in planning/layout terms issues of parking surveillance, cycle store and riverside amenity - space natural surveillance / landscape.

Natural areas officer

35. No objection in principle. The mitigation measures for bats and nesting birds have been adequately covered in the ecological report and the separate bat report. I would especially agree that any external lighting facing towards the river should be chosen carefully to minimise any wildlife impacts, especially on bat foraging activity. There might be scope for including a small number of bird boxes in the development, such as some Swift boxes on the river elevation.

Private sector housing

36. No comments.

Tree protection officer

37. No comments.

Assessment of planning considerations

Relevant development plan policies

38. **Joint Core Strategy for Broadland, Norwich and South Norfolk adopted March 2011 amendments adopted Jan. 2014 (JCS)**
- JCS1 Addressing climate change and protecting environmental assets
 - JCS2 Promoting good design
 - JCS3 Energy and water
 - JCS4 Housing delivery
 - JCS5 The economy
 - JCS6 Access and transportation
 - JCS9 Strategy for growth in the Norwich policy area
 - JCS11 Norwich city centre
 - JCS18 The Broads

- JCS20 Implementation

39. Norwich Development Management Policies Local Plan adopted Dec. 2014 (DM Plan)

- DM1 Achieving and delivering sustainable development
- DM2 Ensuring satisfactory living and working conditions
- DM3 Delivering high quality design
- DM4 Providing for renewable and low carbon energy
- DM5 Planning effectively for flood resilience
- DM6 Protecting and enhancing the natural environment
- DM7 Trees and development
- DM8 Planning effectively for open space and recreation
- DM9 Safeguarding Norwich's heritage
- DM11 Protecting against environmental hazards
- DM12 Ensuring well-planned housing development
- DM13 Communal development and multiple occupation
- DM17 Supporting small business
- DM28 Encouraging sustainable travel
- DM29 Managing car parking demand in the city centre
- DM30 Access and highway safety
- DM31 Car parking and servicing
- DM32 Encouraging car free and low car housing
- DM33 Planning obligations and development viability

Other material considerations

40. Relevant sections of the National Planning Policy Framework March 2012 (NPPF):

- NPPF0 Achieving sustainable development
- NPPF4 Promoting sustainable transport
- NPPF6 Delivering a wide choice of high quality homes
- NPPF7 Requiring good design
- NPPF10 Meeting the challenge of climate change, flooding and coastal change
- NPPF11 Conserving and enhancing the natural environment
- NPPF12 Conserving and enhancing the historic environment

41. Supplementary Planning Documents (SPD)

- Affordable housing SPD adopted March 2015
- Heritage interpretation SPD adopted December 2015
- Landscape and trees SPD adopted June 2016

Case Assessment

42. Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. Relevant development plan policies are detailed above. Material considerations include policies in the National Planning Framework (NPPF), the Councils standing duties, other policy documents and guidance detailed above and any other matters referred to specifically in the assessment below. The following

paragraphs provide an assessment of the main planning issues in this case against relevant policies and material considerations.

Main issue 1: Principle of development

43. Key policies and NPPF paragraphs – JCS1, JCS4, JCS9, JCS11, JCS20, DM1, DM5, DM12, DM13, DM33, NPPF paragraphs 9, 14, 17, 49, 50, 73-75, 100, 103, 109 and 129.
44. The site is not allocated for a specific type of land use or development within the Local Plan. The surrounding area is characterised by a mix of residential and commercial/retail uses. The site lies south of the former Northern City Centre Action Plan area and at the edge of an area on the eastern side of the city centre identified in the JCS, policy 11 as an area of change suitable for mixed use development and improved public realm. The delivery of residential development within the area is likely to increase through allocation sites CC22 (Barn Road car park) and CC30 (Westwick Street Car Park) and probably through other windfall sites.
45. The re-use of land is encouraged by the NPPF and the promotion of residential development on previously developed land in accessible locations addresses many key requirements of the Joint Core Strategy. In accordance with the NFFP and the national objective of boosting housing supply, DM12 is permissive of residential development except where sites are: designated for non-residential purposes; within a specified distance of a hazardous installation; within or immediately adjacent to the Late Night Activity Zone or at ground floor within the primary or secondary shopping area. None of these exceptions apply to this site.
46. The proposal will also meet JCS 11's requirements to promote neighbourhood based renewal, comprehensive regeneration and increase housing densities close to local facilities. In line with policies JCS4 and DM33 discussion has taken place with the developer to assess viability of the scheme and seek a suitable level of affordable housing by way of off-site contribution. Suitable triggers for reassessment of viability are also suggested to be incorporated within any S106 agreement. This matter is explained further in the report below.
47. Policies DM12 and DM13 require assessment of specific site requirements in relation to such issues as designing in adequate garden space, protecting amenity and providing for parking and servicing. The development provides for 42 dwellings in sympathy with the characteristics of the area and arranges the accommodation in such a way as to provide an attractive and well-designed scheme. The density is considered to be compliant with new policy requirements as detailed and dwellings are considered to be designed to respond to the concerns of local residents and officers in respect of application discussions and revisions. The site layout overall respects its context and provides adequate standards of amenity and outlook for residents.
48. The scheme would lead to the loss of an employment use building. DM17 seeks to safeguard suitable business premises for the local needs of business uses. With the application an assessment has been submitted in terms of site marketing and analysis of the sites attractiveness as a viable, feasible or practical building for future business use. The site was previously used up to December 2015 as telephone repeater station with associated facilities. The building has been vacated following a rationalisation of the operational needs of the company. The design of

the building means that its re-use will have limited attraction to possible alternative occupiers and is likely to continue to be left vacant. As such redevelopment of the site is considered to be beneficial to the wider regeneration of the area.

49. The NPPF and DM5 seek to direct new residential development to sites at the lowest risk of flooding. The EA flood map indicates that the site is at risk of flooding and extends across flood zones 2 and 3 (river edge) at medium and high flood risk. In accordance with policy the scheme should be assessed and determined having regard to the need to manage and mitigate against flood risk. A sequential test has been applied in order to assess whether the development could be accommodated on alternative site/s at lower flood risk. A number of sites have been allocated for residential development and some of these are in low risk areas. In addition given the nature of the area there is likely to be a number of brownfield / possible windfall sites which may be capable of redevelopment. These sites are theoretically available for residential development of a similar scale to that proposed by this application.
50. The development of these alternative sites might not result in the same level of wider sustainability benefits. These benefits include the development of a vacant site within an area suitable for regeneration and which supports the objectives and policies of the development plan; development of a negative site prominently located within City Centre Conservation Area and highly visible from the River Wensum; is of a scale suitable for this site; provision of access to the river frontage; provision of new homes; and enhanced public realm areas. The two parts to the Test require proposed development to show that it will provide wider sustainability benefits to the community that outweigh flood risk and that it will be safe for its lifetime, without increasing flood risk elsewhere and where possible reduce flood risk overall.
51. The approach to flood risk for site allocation CC30 is to a) ensure development would not increase the vulnerability of the site, or the wider catchment, to flooding from surface water run-off from existing or predicted water flows, and; b) would, whenever practicable, have a positive impact on the risk of surface water flooding in the wider area. A similar approach has been taken to flood defence for the proposed scheme and increased permeability, storage, suitable floor level design and safe access have been designed in and discussed with the EA. A condition is suggested to ensure agreement of a suitable emergency flood warning and evacuation plan for the site. On this basis the principle of development in an area of the city at flood risk is considered acceptable.
52. As such the scheme accords with local and national policies for development and re-use of land and is considered to be an appropriate and preferred development for the site.

Main issue 2: Heritage

53. Key policies and NPPF paragraphs – DM9, NPPF paragraphs 128-141.
54. The site lies within the northern riverside character area of the City centre conservation area. Relevant management and enhancement policies within the conservation area appraisal (CAA) include: 1- variation in scale, 4 - maximisation of views across, from and of the river and 5/6 - encourage river access and use.

55. The existing 1950's purpose built telephone repeater station is identified as a negative building within the CAA and demolition of the building is not opposed. Whilst it is typical of its date/style, it does not contribute to the character and appearance of the conservation area. Its low level boxy form, horizontal emphasis and white bricks jar against the prevailing traditional form, scale, proportions and materials employed upon both residential & industrial buildings within this part of the conservation area.
56. The area has evolved over the course of the last 3 centuries from a largely industrial area into a largely residential one, with C20th residential accommodation of traditional domestic form and scale with elements that are reminiscent of the area's industrial past. The prevailing building heights are 2-3 & 3-4 storeys fronting onto the river. Site history and interpretation supplied with the application indicate that the JS Read corn mill, which was up to 5 storey with projecting bays, and Barkers Yard, 3 to 5 storey, occupied the site in the 19th/early 20th centuries then being replaced by the current building. The mid-late C20th development upon the development site and neighbouring sites to the east and retail units to the south are negative features in this area.
57. Views up and down the river from St Miles Bridge and along the riverside walk are particularly attractive in this area, terminating to the north-west with the Grade II listed pumping station and to the south-east to the Grade II listed bridge and Anchor Quay former brewery site. There are a number of views from and across the site, including views of the Roman Catholic Cathedral, St Giles Church, St Lawrence Church and the clock tower of City Hall. St Margarets tower is also a tall feature within the area and all of these are also listed buildings. The wider city views and numerous church spires are legible throughout the conservation area including across the development site and add interest and legibility to the townscape.
58. The application has been accompanied by a Design and Access Statement (D&A) and heritage statements which include a detailed analysis of the site and the surrounding area and explains how this has guided the design of the scheme. The analysis includes consideration of the context/surroundings and the nature, pattern and form of development associated with this part of the city. The prevailing materials in this area are red brick and pantiles with pitched and hipped roof forms fronting the river.
59. Various comments have been received on the application. With the original submission Historic England indicated that they were broadly content with the proposed development and consider that the mixture of traditional and contemporary forms are suited to this part of the conservation area. The detailing is also acceptable, although attention to detail and particularly the quality of the masonry will be essential if the design is to be successful. They suggested that the tallest part of the Westwick Street elevation was potentially out of scale with the surroundings and required further consideration. They also noted that the new buildings will not fully define the waterfront with a building line due to the way the internal courtyard between the buildings opens onto the riverside. In contrast on this point the Broads Authority appreciated that the buildings step back from the river corridor and avoided canalisation of this stretch of the river.
60. The council's design and conservation officer has also commented in terms of building height and river frontage. It is noted that the 5 storeys scale fronting Westwick Street takes reference from Watermans Yard development to the south

and is contextual. However; the previously proposed roof mounted plant area effectively increased the height of the building to 6 storeys. This would over-sail the prevailing building height in the conservation area and would be an incongruous and unwelcome addition. The design with cut out terrace was also considered to create a weak corner on the Westwick Street frontage. In terms of the river edge extra modelling was requested to show the building in context as the 3 storey element might create a boxy form as viewed from the nearby bridges, it was also suggested to redesign this element and potentially set it further back from the river. Other issues were raised by local residents including the effect of a taller building on nearby historic gateways through the City Wall and into the centre.

61. The revised scheme has sought to address the main concerns. Changes include incorporating design features to reduce the apparent massing of the building, to better define the separation from the river by reduced car parking and enhanced planting/screening to the central area and removal of the roof top plant. The agent has also provided additional modelling of the development within the context of the area to demonstrate potential impacts.
62. Overall Historic England welcomes the amendments and whilst they would still like earlier comments to be considered would not wish to object to the granting of consent. The amendments contain some positive changes, including the reduction of the roof top plant enclosure and addition of a masonry screen at the Westwick Street side. In terms of the degree of enclosure to the riverside of the development whilst this has not been addressed in amendment of the built form the additional attention to the definition of the courtyard parking area on this side could help improve the riverside experience. The Broads Authority has also welcomed the scheme.
63. In terms of other listed buildings located in close proximity to the site, although the development will be near to the pumping station and will contrast in design, the setting of this listed building is unlikely to be substantially compromised by the development. This building acts as an end stop to this part of the river and the scale of the new development will be viewed in this context and the immediate environs of the listed building will be largely unaffected. The proposed development in terms of both scale and appearance responds well to this historic building and reflects some of the design elements on the opposite side of the river at new mills.
64. The view of the cathedral which is currently afforded directly above the existing building will be lost but modelling of the scheme shows that the scheme will provide an appropriate setting to views past the development to the cathedral. The same conclusion is considered to apply to views along Westwick Street and from the river edge towards the City centre and the impact on listed and other buildings within the wider area. In terms of long views from Heigham Street / Barn Road areas back to the City centre the position and design of the building, whilst in some respects is relatively discrete in terms of influence of any view, is considered to deliver an appropriate design and approach to scale for development within this area.
65. The scheme has also been designed to frame views of St Giles church when viewed from the north and is considered to act as an appropriate element in terms of interest and legibility within the townscape. The council's design and conservation officer considers that the recent revisions have improved the design of the development and addressed issues of the overall massing of the building. The change to the area will cause a certain level of harm to the conservation area and

nearby listed buildings but in this context the impact of the development is considered acceptable and the degree of harm limited. In any event following revision the harm of the proposals is further reduced and is more than sufficient for the level of harm caused to be offset by the associated public benefit of the 'provision of new homes'.

66. In line with the comments of Historic England it is considered that the development would not result in harm to the significance of the conservation area and nearby listed buildings in terms of the NPPF and statutory duties to preserve or enhance the character and appearance of the conservation area and setting of listed buildings.
67. The site has a potentially interesting history, and this could be referenced to in some form of heritage interpretation in the public space which again is suggested as being sought by condition.

Main issue 3: Design

68. Key policies and NPPF paragraphs – JCS2, JCS18, DM3, NPPF paragraphs 9, 17, 56 and 60-66.
69. The proposed development would see the construction of two L-shaped buildings. Block 1 and Block 2. These two blocks would be separated by a central courtyard/car parking area leading to the riverside where an amenity area has been provided. This break in the development will frame an oblique view of St Giles church beyond to the south of the development site. Refuse and cycle storage have sensibly been pushed to the western side of the plot allowing easy access to Westwick Street. The proposed red-brick in Flemish bond is welcomed. A standing seam cladding could aid the verticality of the elevations and sample panel are suggested to be agreed by condition. A splayed reveal to window elevations has been reintroduced to provide better definition to the window reveals and interest to the elevations.
70. The sub-station and access positioned on the front of the site are subject to a 89 year lease. Access has been redesigned for this unit and a shared landscape space laid out to give a sense of the space here being incorporated into the scheme. Landscape spaces on the river frontage have been increased and space provided to allow for parking to be obscured from views from the river. This also assists in create some enclosure to the river edge as suggested by Historic England.
71. Some dis-jointed views of Roman Catholic Cathedral may be affected from the northern side of the river; however, the new view of St Giles Church through the development should help negate this loss. The loss of the existing partial views of the Roman Catholic Cathedral should not prevent the sensitive re-development of this 'negative site' which at present contributes nothing to the character and appearance of the conservation area. The creation of suitably scaled new buildings and a newly landscaped riverside amenity space should enhance the existing context.
72. The scale of the buildings is generally considered to deal well with the height found in the area, particularly on the river frontage where the development steps down from 5 to 3 and 4 storeys. In terms of the principle of a building of this height a tall element to the scheme is not out of keeping in the immediate area, as it will be read

in conjunction with the nearby residential elements facing Westwick Street to the east. The site is also relatively distant from the inner ring road and seen in the context of other large buildings on the approach to the City centre. Its development at the scale proposed is unlikely to lead to difficulties in designing other development which leads into the inner ring road and nearby gateways or affect the possible delivery of other development sites within the area.

73. The broad design approach is considered to be well founded and imaginative. The development will provide a new use for the site, re-establish a positive frontage to both the river and Westwick Street, re-create a route to the river frontage; make creative and effective use of a contemporary pallet of materials and provide the opportunity for landscape enhancements. The contemporary design approach to traditional forms is welcomed and subject to conditions the materiality will largely harmonise within the existing context.
74. The overall design of the development will create a pleasant unified scheme. The current proposals are considered to provide a good balance between site density and an appropriate layout. The landscaping to the river edge, central parking space and site frontages, detail to the front of the blocks and contemporary design should also positively address the street scenes and add design interest for the area. The approach taken builds in an active frontage to the street and river and provides a sense of a secure courtyard. It is considered that this approach is appropriate for the area, however achieving a good design will be down to good detailing and it is therefore recommended that any consent be subject to conditions on details of fascias, verges, windows, doors, bricks, roof finish and any cladding finish.
75. The scheme provides for a percentage of dwellings designed to lifetime homes standards. These are located at ground and upper floor positions distributed throughout the site. In terms of space standards the design of dwellings meets or exceeds housing design quality standards and follows other recognised design guidance in terms of private external amenity space allowances.

Main issue 4: Amenity

76. Key policies and NPPF paragraphs – DM2, DM11, NPPF paragraphs 9 and 17.
77. The scheme provides for 42 dwellings within an arrangement of two individual blocks. Buildings are pitched roof four storey on the south east side and three to five storey with three storey blocks to the end adjacent the river and flat or mansard type roof on Westwick Street. The shape of the site has led to the positioning of buildings within the north-west and south-east sections to form a surround to central parking area and frame to the amenity spaces, river edge and view through the site. This is seen to be an acceptable arrangement to provide on-site amenities for the benefit of residents.
78. The flats have areas of private space incorporated into their layout and also share communal external spaces within the development. There are other off-site amenities within easy access of the site. Generally the properties have been designed to meet appropriate space standards. The scheme layout will also enhance the link onto the river edge to the north of the site and enhance the trees and planting within the site. The provision of planting and design features within the site will also enhance the amenity and outlook for existing and future residents.

79. The arrangement of dwellings in each section seeks to minimise overlooking by ensuring that main living space rooms above first floor bedrooms look north/south or into the courtyard and that those on the boundary are to bedrooms/bathrooms or stair landings. Some of the flats could overlook other new flats, but in these instances changes have been made to layout of flat types to avoid significant overlooking issues between these properties. The buildings are stepped in height and take advantage of the site levels to improve light levels between buildings. This aids not only amenity but also winter light levels for thermal gain. The blocks of flats are positioned opposite existing residential properties but still at a distance and orientation to not significantly impinge on local amenities. The distances between existing and new buildings are considered to be acceptable and typical of an urban layout for all elements of the scheme.
80. Early assessment of shading and building distances has indicated that there will be no significant loss of light, loss of outlook or overlooking to adjacent properties. Layout has also removed main habitable room windows directly overlooking adjoining property to the east and west. Some upper floor windows can be obscured glazed and fixed opening designed to avoid creating difficulties for residents from overlooking, such as to landings and a condition is proposed to control finish of these windows.
81. The submitted noise report indicates that dwellings could be affected by road traffic and possibly workshop operations to the north-east. Suitable building design and use of glazing / ventilation systems indicate that the world health organisation sound levels for residences can be met. Some exceedance of these might be experienced in private balconies facing south but some exceedance of levels is considered acceptable having regard to the location and that there is the provision of additional communal open space within the development. Other potential noise sources exist from plant and machinery on the south-east border and from the sub-station on the front of the site. The report concludes that break out noise from these sources is not considered to be a significant issue and this has been confirmed by environmental protection officers.
82. The adjacent business on Westwick Street could potentially impact on new residences. There is also an established retail park to the south. However; regard has been had to retaining established commercial operations and potential for commercial noise and activity and in designing the scheme this existing relationship has been taken into consideration and upper floor private amenity spaces have been directed away from these properties. The submitted noise report advises on proposed building design to increase insulation levels and the development should not be greatly affected by business noise sources. In the circumstances it is unlikely that new development within the area would significantly impact on the lawful operation of nearby businesses.
83. Although no exact details have been provided, lighting should be positioned to the front entrances of dwellings together with lighting provided to illuminate the central car and cycle parking, footpaths and bin stores. Illumination of the communal spaces will help to further overcome security issues and are considered to be essential features to promote a safe and secure development. Conditions are suggested requiring submission of details of site lighting to ensure that there is no design or adverse amenity impacts or that light spill affects the ecology value of the river.

84. The proposals work well with reference to their relationship with adjacent properties and subject to conditions on joinery, glazing and landscaping it is not considered that the proposals would result in any unacceptable impact to adjacent properties in terms of outlook, overlooking or overshadowing or in terms of quality of the living environment for existing or future residents.

Main issue 5: Landscaping and open space

85. Key policies and NPPF paragraphs – DM3, DM8, NPPF paragraphs 9, 17 and 56.
86. Details have been worked up for indicative landscaping proposals across the site including the central space and river edge of the site. The proposal is intended to give communal benefits to future residents and the integration of the river edge into the layout of the site should help create connections and new legible spaces in the area. Of particular importance will be the detailing of communal spaces and how they are defined in relation to the wider area and for the creation of a pleasant access space within the development itself. The site also increases ground permeability which assists with drainage strategies and provides for some part green roof areas.
87. The development should be well landscaped to enhance its use and to promote biodiversity links. Design of the river frontage and river access has been investigated. Whilst there is no scope for direct access the scheme provides for the enhancement of the river setting which is supported by the Broads Authority. Revisions here also assist in screening the central parking area from the river edge which subject to planting detail is acceptable.
88. The comments from the Environment Agency have been noted regarding concern over the spread of non-native species, and it has been confirmed that the species indicated on the outline planting schemes are not listed on schedule 9 of the Wildlife and Countryside Act, the landscape officer therefore sees no reason to object to the inclusion of some non-native species within the contained raised planter areas to ensure that a balance between aesthetics, functionality and biodiversity is reached. Also noted that no riparian planting is proposed and the local area comprises of hard edge treatments and so consider the spreading or self-seeding of vegetation from this site is very unlikely, however it is suggested that a native mix of planting species is used for the areas directly adjacent to the river.
89. The setting out distance of buildings and road edge enables new trees to be positioned between buildings and Westwick Street on the south side of the site to help soften the street scene and avoid necessity for street tree provision within pavements which may not be capable of being designed to accept these. Other planting is proposed within the courtyard at key connection points through the site.
90. Further details will also be required on the planting scheme for the site as well as internal boundary treatments. The indicative layout of these spaces is considered to be acceptable and it is suggested that the specific details be conditioned as part of any consent. Conditions are also suggested to ensure biodiversity enhancements are provided as part of the scheme and an informative added in relation to wildlife protection. Details requiring a scheme for the provision and maintenance of landscaping and the central open space are also suggested by way of condition.

91. The proposal to introduce greenery into the pavers is supported, but we have highlighted that concrete grass grid systems do not always give the desired results, and so we would want to see specific details of the proposals to ensure its success. Design of hard surfaces for circulation, parking and pathways will be critical to the final design of the scheme and whilst initial examples of materials have been shown details of final hard landscaping are suggested to be agreed by condition. A condition related to historic interpretation which could be incorporated into any landscape scheme is mentioned above.

Main issue 6: Transport

92. Key policies and NPPF paragraphs – JCS6, DM28, DM30, DM31, NPPF paragraphs 17 and 39.
93. Analysis of trip generation from the development indicates that this will be relatively low at peak times with limited impacts on the wider road network. Changes to the roadway are limited to providing a single access point to the site instead of the current arrangements. Access arrangements have been assessed and overall the scheme design allows appropriate access for service and other vehicles without detriment to operations or safety in the immediate area. Suitable sized bin stores are located close to the roadway for ease of collection and limit the need for service vehicles to enter the site.
94. Design detail will be required to ensure that works within the highway, including realigned footpaths and the new access point, are to a suitable standard. Street light and cabinet positions might also require some realignment. Subject to conditions on surfacing and design work the access and servicing provisions are considered to be acceptable. Conditions are also suggested for the provision of bin facilities to ensure adequate design and secure access.
95. The site is located within a location suitable to promote travel by more sustainable forms of transport and in policy terms is within a location potentially suitable for car free or low car housing. With good links available to the local centre and public transport infrastructure it is therefore accessible by sustainable modes for all. Car ownership is likely to be lower than average due to close proximity of facilities. The car parking levels overall are below the Council maximum standard for the scale of development but allow some flexibility in parking. Two electric charging points are also incorporated into the scheme. The scheme incorporates measures to improve choice to cycle with a high level of secure and public cycle parking facilities.
96. The design of parking within the development area is provided within groups, close to and adjacent to homes and within view of the active rooms within these homes. The layout proposed for the internal courtyard demonstrates that adequate space for safe walkways and access through the area is also provided. On balance and in comparison to the removal of the previous commercial operation this level of car parking is considered to be acceptable and should adequately address parking issues within the area.
97. Cycle parking is available within bike stores for the flats built into communal space and have direct access to the highway. Details for provision of storage areas are suggested by way of conditions. It is envisaged that the very good level of accessibility for the site that travel will likely result in a modal shift towards more sustainable modes of travel. This approach is reinforced within policy DM28 and

DM31 which gives an indication of suitable levels of car parking for various locations.

Main issue 7: Affordable housing and viability

98. Key policies and NPPF paragraphs – JCS4, DM33, NPPF paragraph 50.
99. The promotion of this site has been discussed with Council Housing Officers to seek to secure a percentage of the affordable housing at appropriate rent levels for the locality and to ensure an appropriate split in tenure types being made available.
100. Layout of the scheme has been discussed to offer options for on-site provision within one of the unit wings. This would allow for some dedicated communal access space to allow easier management of any units provided. Initial viability appraisal for the site indicated that the scheme would not be viable to provide for either on site affordable housing provision in line with policy JCS4 as a minimum 33% of the houses and flats or as an off-site contribution.
101. Further discussion and analysis of information has taken place and an offer negotiated of 10% of a contribution for affordable housing provision off-site via a commuted sum. The agent/applicant have recognised the importance of both private and public sector housing delivery and agreed to a reduced profit margin below a normal 20% target. It is suggested that in addition to this as an indicative figure that review of viability where non-commencement/occupation of development occurs is also secured through S106 agreement.
102. The adopted Affordable Housing SPD states that where reduced affordable housing is accepted a S106 Obligation will be required and include an affordable housing viability review clause. This will require development viability to be reassessed in the event of development not being delivered within an agreed timescale. Given the complexities of this particular site an appropriate timescale would be commencement within 15months and occupation of within 18 months. This is comparable to approaches taken within the City centre for other development sites.

Compliance with other relevant development plan policies

103. A number of development plan policies include key targets for matters such as parking provision and energy efficiency. The table below indicates the outcome of the officer assessment in relation to these matters.

Requirement	Relevant policy	Compliance
Cycle storage	DM31	Yes subject to condition
Car parking provision	DM31	Yes subject to condition
Refuse Storage/servicing	DM31	Yes subject to condition
Energy efficiency	JCS 1 & 3 DM3	Yes subject to condition

Water efficiency	JCS 1 & 3	Yes subject to condition
Sustainable urban drainage	DM3/5	Yes subject to condition

Other matters

104. The following matters have been assessed and considered satisfactory and in accordance with relevant development plan policies, subject to appropriate conditions and mitigation:

Archaeology

105. Key policies and NPPF paragraphs – JCS1, DM9, NPPF section 12 paragraphs 128 and 141.

106. The desk based assessment submitted with the application provides explanation of the examination of evidence and details that the site is unlikely to have significant prehistoric or roman remains. There is considerable evidence within the wider search area of mediaeval activity and the report indicates that there is moderate potential for significant Saxon, mediaeval and remains of 16th century and later WWII buildings. The Historic Environment Service has therefore asked for an archaeological Written Scheme of Investigation prior to works commencing on site. The findings of such research could also assist with a scheme for heritage interpretation for the site. The site has a potentially interesting history, and this could be referenced to in some form of heritage interpretation in the public space which again is suggested as being sought by condition.

Biodiversity

107. Key policies and NPPF paragraphs – JCS1, DM6, NPPF paragraph 118.

108. EA comments on riparian species and compliance with the Wildlife and Countryside Act are discussed above. Potential impacts on bat activity from lighting on the river edge are also mentioned. Subject to suitable conditions on landscaping and lighting these issues should be satisfactorily resolved.

109. An ecological assessment and bat roost survey have been submitted with the application and in terms of ecology the site, being mostly simple flat roofed buildings in reasonable repair and other hard surface areas, appears to be of low ecological value. There are a number of buddleia on site and a tree adjoining the site providing some habitat but the maintained nature of the site has meant that the main interest would be nesting birds and potentially for foraging for bats. Potential impacts to protected species and other species of conservation interest from development of the site have been assessed as being minimal.

110. Mitigation would be suggested primarily as native species planting as being part of any new landscaping scheme and for the provision of bird and bat boxes. It is recommended that a number of bird boxes are incorporated into the development, and installed on some of the new homes such as some Swift boxes on the river elevation. It is suggested that any external lighting provided in conjunction with the development should be of a modern, low spill type to minimise light seepage into

the open areas at the edges of the site and that such detail is controlled by condition. Conditions are also suggested to ensure biodiversity enhancements are provided as part of the scheme and an informative added in relation to wildlife protection during site works.

Contamination

111. Key policies and NPPF paragraphs – DM11, NPPF paragraphs 120-122.
112. Phase 1 and phase 2 assessments of the site in terms of contaminants and remediation have been submitted with the application.
113. The reports are acceptable and make several recommendations. It is clear that whilst some elevated pollutant levels were found, the site is not grossly contaminated. The report suggests that pollution of controlled water is low and that this may be a result of wider area contamination not related to the development site. The Environment Agency have been consulted on the application and made observations on contamination and groundwater protection. They have no objection provided that conditions regarding ecology and the protection of the water environment are included.
114. The reports make some recommendations relating to potential remediation, including removal of the underground storage tanks and impacted soils etc. but the environmental protection officer has asked that a remedial method statement is developed to cover all points raised. Additional ground gas monitoring will also be required. Local impacts should be limited and development acceptable subject to conditions on contamination assessment (parts 1&2 already complied with in terms of our standard condition), to stop works and submit details of remediation if unknown contamination is found during works and to ask the developer to provide details of testing and/or suitable compliance for any imported top soil material.

Energy and water

115. Key policies and NPPF paragraphs – JCS3, DM1, NPPF paragraphs 94 and 96.
116. The initial scheme provided for air source heat pumps positioned on the roof of the western block. However; due to concerns about design and amenity impacts potentially arising from the position of these units the scheme has been revisited and a roof mounted PV scheme is now suggested to deliver 11% of the sites energy requirement from on-site renewable technology in line with policy JCS3. Water efficiency targets in line with current guidance are also mentioned within the submitted energy, water and construction statement.
117. Specification of a site waste management plan; planning of material quantities and delivery timings; and where possible, locally sourced materials used for construction should also improve the methodology for construction to assist in reducing construction and resource impacts.
118. The scheme is therefore considered to be acceptable and suitable conditions are suggested for the development to ensure energy systems are provided and maintained on site as necessary and that water conservation measures are incorporated into the scheme.

Flood risk

119. Key policies and NPPF paragraphs – JCS1, DM5, NPPF paragraphs 100 and 103.
120. Discussion on the sequential test is mentioned above in terms of accepting development in this location. This includes potential for benefits of regeneration and housing, need for housing and flood control. The design strategy for the site has considered in discussion with the EA ground levels and slab height above ordnance datum (AOD) and impacts from flood zones. It is also noted that the site at present is 100% impermeable.
121. The site lies within Flood Zone 2 and at the river edge within zone 3 defined by the ‘Planning Practice Guidance: Flood Risk and Coastal Change’ as having between a medium and a high probability of flooding where notwithstanding the mitigation measures proposed, the risk to life and property within the development from fluvial inundation would be unacceptable if the development were to be allowed. The proposal is for a “more” vulnerable development as defined in Table 2: Flood Risk Vulnerability Classification of the Planning Practice Guidance”. The design approach to building levels, increased permeability and surface water control before discharge from the site by way of attenuation tanks are seen to be an acceptable approach to surface water drainage design and flood defence for the site and area. A condition is suggested in terms of agreeing final design and management of these features.
122. The EA are satisfied that the flood risk assessment submitted with the application provides information necessary to make an informed decision. Finished floor levels have been set at 3.8m AOD which is 300mm above the 1 in 100 (1%) annual probability events, including an allowance for climate change. This is in line with the advisory requirements of Paragraphs 059 and 060 of the NPPF’s Planning Practice Guidance which advises that there should be no internal flooding in More Vulnerable developments from a design flood 100 (1%) and 200 (0.5%) year inclusive of climate change. An indicative emergency flood plan has also been submitted. To ensure that the development would be safe for its lifetime a condition is suggested to ensure that details of the flood evacuation plan are agreed and operations continue into the future.
123. This development may require a permit under the Environmental Permitting (England and Wales) Regulations 2010 from the Environment Agency for any proposed works or structures, in, under, over or within eight metres of the top of the bank of the River Wensum, designated a ‘main river’. This was formerly called a Flood Defence Consent. Some activities are also now excluded or exempt. A permit is separate to and in addition to any planning permission granted. The permit will control works in, over, under or adjacent to main rivers (including any culverting).

Trees

124. Key policies and NPPF paragraphs – DM7, NPPF paragraphs 109 and 118.
125. Tree impact is limited with only one tree slightly overhanging the site in its north-east corner. The tree is a semi-mature category B1 False Acacia tree and would be positioned outside of any area of building development. Assessment and recommendations have been made in terms of its potential for future impacts, which are considered to be limited, and for any necessary works to protect the tree during construction. Conditions are suggested for details of ground regrading, tree protection and retention of such protection during construction.

Equalities and diversity issues

126. There are no significant equality or diversity issues.

S106 Obligations

127. Whilst it is the applicant's intention at this time to provide a reduced off-site contribution for the provision of dwellings as affordable housing it is reasonable to secure through a S106 agreement review of viability at appropriate stages and then, as possible, a percentage of the dwellings in line with policy at agreed rent or tenure type or for additional off-site contribution.

Local finance considerations

128. Under Section 70(2) of the Town and Country Planning Act 1990 the council is required when determining planning applications to have regard to any local finance considerations, so far as material to the application. Local finance considerations are defined as a government grant or the Community Infrastructure Levy.

129. Whether or not a local finance consideration is material to a particular decision will depend on whether it could help to make the development acceptable in planning terms. It would not be appropriate to make a decision on the potential for the development to raise money for a local authority.

130. In this case local finance considerations are not considered to be material to the case.

Conclusion

131. Both the NPPF and DM9 require all development to have regard to the historic environment and maximise opportunities to preserve, enhance or better reveal the significance of designated assets. These policies are rooted in the requirements of the Planning (Listed Buildings and Conservation Areas) Act 1990 which imposes a duty on local authorities to have special regard and pay special attention to development affecting listed building and their settings and conservation areas. The comprehensive proposals for a high density and contemporary form of urban development have been carefully assessed in this context. On balance, there is considered to be limited harm and that any harm is considered to be outweighed by the benefits of the scheme in terms of: design quality; delivery of housing in a highly sustainable location; and the effective re-use of a vacant site. The scheme also provides for other benefits in enhancing the river edge in this area and for the potential delivery of affordable housing. The development is seen to be in accordance with the requirements of the National Planning Policy Framework and the Development Plan, and it has been concluded that there are no material considerations that indicate it should be determined otherwise.

Recommendation

To approve application no. 16/00456/F - BT Telephone Exchange Westwick House 70 Westwick Street Norwich NR2 4SY and grant planning permission subject to the completion of a satisfactory legal agreement to include provision of affordable housing contribution and review of viability should works not commence or dwellings be occupied and subject to the following conditions:

1. Commencement of development within 3 years from the date of approval;
2. Development to be in accord with drawings and details;
3. Details of facing and roofing materials; brick bond and mortar; joinery; verges; vent systems; external lighting; heritage interpretation; and photographic record of existing building;
4. Side windows to upper floor communal access spaces, hallways, bath/shower rooms, kitchens to be obscure glazed and fixed openings;
5. Details of any remaining archaeological work and written scheme of investigation
6. Details of vehicle charging points; cycle storage; site management for parking/access; and bin stores provision;
7. Details of highways works;
8. Construction management plan; parking; wheel washing;
9. Details of landscaping including: planting; tree pits; biodiversity enhancements, bird and bat boxes; site treatment works; boundary treatments, including any proposals to guard the edge of the river, separation of private amenity areas, gates, walls and fences; edge treatment to roof terraces and gardens; landscape features such as planters, seats, raised walls etc. complete with heights or levels to indicate the overall appearance; parking, access road and path link surfaces; and landscape management and implementation programme and maintenance;
10. Water framework directive compliance (EA)
11. Details of necessary AMS for additional site works, ground regrading, protection of existing trees;
12. Compliance with AIA, AMS and additional information at condition 11 and Tree Protection Scheme implemented prior to commencement;
13. Retention of tree protection;
14. Details of provision and maintenance of LZC technologies / renewable energy sources;
15. Details of water efficiency measures;
16. Details of the surface water drainage system and future maintenance of;
17. Details of emergency flood warning and evacuation plan and implementation of surface water flood strategy;
18. Site contamination investigation and assessment (excluding parts 1 and 2 in terms of our standard condition as already complied with);
19. Details of contamination verification plan;
20. Cessation of works if unknown contaminants found and submit details of remediation;
21. Details of testing and/or suitable compliance of all imported material prior to occupation;
22. Compliance with the recommendations of submitted noise report;

Article 35 (2) statement

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments at the pre-application and application stage the application has been approved subject to suitable land management, adoption, appropriate conditions and for the reasons outlined within the committee report for the application.

Informatives

- Considerate constructors

- Impact on wildlife
- Highways contacts, street naming and numbering, design note, works within the highway etc.
- Properties at this development will not be entitled to on street parking permits;
- Environment Agency guidance;
- Anglian Water guidance.



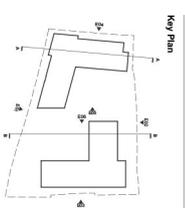
A PR 01.08.16 Planning Application to the Local Planning Authority
 Planning and Design & Construction Officers Recommendations
 Issued for Planning - 26.02.16

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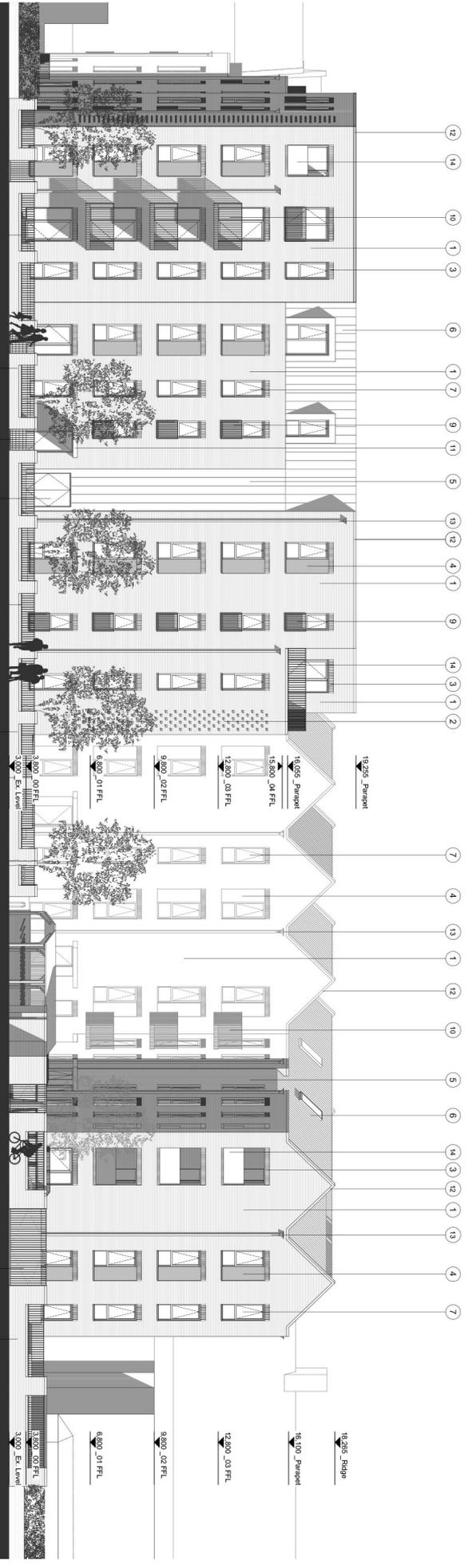
Client: Tederal Securised Property GP Ltd
 Project: 70 Westwick Street

Title: Planning - Proposed Ground Floor Plan
 Scale: 1:100
 Date: 08.05.2015
 Drawn: FR
 Checked: A

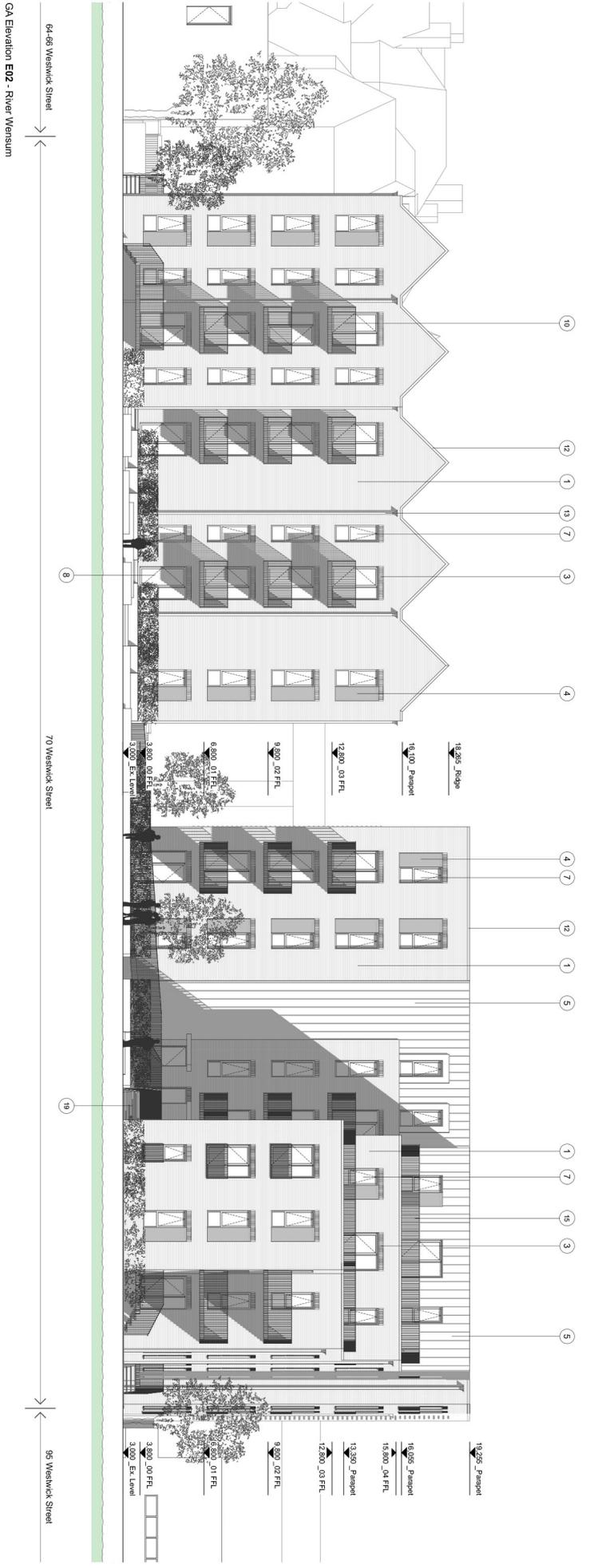
14.01.15-200



Key Plan



95 Westwick Street
GA Elevation E01 - Westwick Street
70 Westwick Street
Westwick Street



64-56 Westwick Street
EA Elevation E02 - River Wensum
70 Westwick Street
70 Westwick Street
95 Westwick Street

Materials Key

1. Red brickwork in finish bond with lime mortar
2. Propelling brickwork
3. Brickwork soldier course
4. Polyester powder coated metal panels (two tone colour with texture) with polyester window frames. No visible floor fixings
5. Profined metal wall cladding
6. Profined metal roof
7. Polyester powder coated aluminium/ultra composite windows
8. Polyester powder coated metal framed doors with PFC pads and vision panels
9. Polyester powder coated metal panel and rail balustrading around polyester powder coated metal deck (all on top position window frames)
10. Polyester powder coated metal post and rail balustrading around polyester powder coated metal facing projecting balcony
11. Polyester powder coated metal facing projecting entrance canopy
12. Polyester powder coated metal with no visible floor fixings, prefinished corners required at each change of direction
13. Polyester powder coated metal rainwater hopper and downpipe
14. Fasteners: clear anodised stainless balustrade
15. Polyester powder coated metal post and rail balustrading
16. Start finish **OMITTED** (formed) paint
17. Low wall and filling blockwork boundary walls in finish bond lime mortar, bonded on brick
18. Polyester powder coated metal post and rail balustrading (100mm spacing)
19. External metal cladding with polyester powder coated metal post and rail balustrading
20. Polyester powder coated metal louvers
21. Perforated brickwork panel

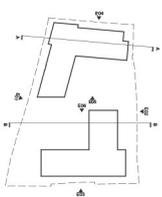
A 08 01.08.16 Planning and Design & Construction Officers recommendations
Issued for Planning - 26.02.16

Scale: 1/40
Date: 15.02.2016
Author: FR
Check: A

LSI ARCHITECTS
216, 218, 220, 222, 224, 226, 228, 230, 232, 234, 236, 238, 240, 242, 244, 246, 248, 250, 252, 254, 256, 258, 260, 262, 264, 266, 268, 270, 272, 274, 276, 278, 280, 282, 284, 286, 288, 290, 292, 294, 296, 298, 300, 302, 304, 306, 308, 310, 312, 314, 316, 318, 320, 322, 324, 326, 328, 330, 332, 334, 336, 338, 340, 342, 344, 346, 348, 350, 352, 354, 356, 358, 360, 362, 364, 366, 368, 370, 372, 374, 376, 378, 380, 382, 384, 386, 388, 390, 392, 394, 396, 398, 400, 402, 404, 406, 408, 410, 412, 414, 416, 418, 420, 422, 424, 426, 428, 430, 432, 434, 436, 438, 440, 442, 444, 446, 448, 450, 452, 454, 456, 458, 460, 462, 464, 466, 468, 470, 472, 474, 476, 478, 480, 482, 484, 486, 488, 490, 492, 494, 496, 498, 500, 502, 504, 506, 508, 510, 512, 514, 516, 518, 520, 522, 524, 526, 528, 530, 532, 534, 536, 538, 540, 542, 544, 546, 548, 550, 552, 554, 556, 558, 560, 562, 564, 566, 568, 570, 572, 574, 576, 578, 580, 582, 584, 586, 588, 590, 592, 594, 596, 598, 600, 602, 604, 606, 608, 610, 612, 614, 616, 618, 620, 622, 624, 626, 628, 630, 632, 634, 636, 638, 640, 642, 644, 646, 648, 650, 652, 654, 656, 658, 660, 662, 664, 666, 668, 670, 672, 674, 676, 678, 680, 682, 684, 686, 688, 690, 692, 694, 696, 698, 700, 702, 704, 706, 708, 710, 712, 714, 716, 718, 720, 722, 724, 726, 728, 730, 732, 734, 736, 738, 740, 742, 744, 746, 748, 750, 752, 754, 756, 758, 760, 762, 764, 766, 768, 770, 772, 774, 776, 778, 780, 782, 784, 786, 788, 790, 792, 794, 796, 798, 800, 802, 804, 806, 808, 810, 812, 814, 816, 818, 820, 822, 824, 826, 828, 830, 832, 834, 836, 838, 840, 842, 844, 846, 848, 850, 852, 854, 856, 858, 860, 862, 864, 866, 868, 870, 872, 874, 876, 878, 880, 882, 884, 886, 888, 890, 892, 894, 896, 898, 900, 902, 904, 906, 908, 910, 912, 914, 916, 918, 920, 922, 924, 926, 928, 930, 932, 934, 936, 938, 940, 942, 944, 946, 948, 950, 952, 954, 956, 958, 960, 962, 964, 966, 968, 970, 972, 974, 976, 978, 980, 982, 984, 986, 988, 990, 992, 994, 996, 998, 1000

Client: Tederal Securised Property GP Ltd
Project: 70 Westwick Street
Title: Planning - Proposed Elevations E01 and E02
Scale: 1/40
Date: 15.02.2016
Author: FR
Check: A

Key Plan

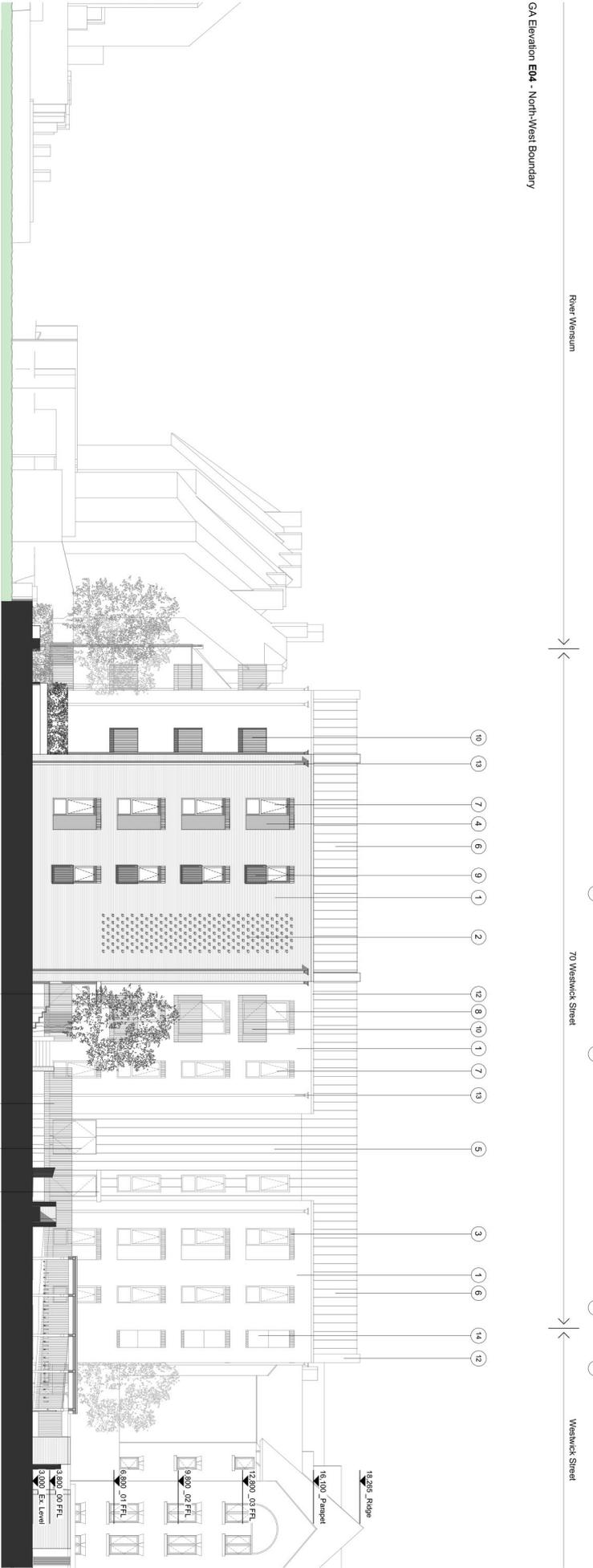


Materials Key

1. Red facing brickwork in finish bond with lime mortar
2. Projecting brickwork
3. Brickwork soldier course
4. Polyester powder coated metal panels (two tone color with texture) with polyester window frames. No visible floor fixings
5. Powder metal wall cladding
6. Profiled metal roof
7. Polyester powder coated aluminium/ultrather composite windows
8. Polyester powder coated metal framed doors with PVC seals and vision panels fully glazed to overhead
9. Polyester powder coated metal post and rail balustrading around perimeter balcony
10. Polyester powder coated metal post and rail balustrading around polyester powder coated metal facing projecting balcony.
11. Polyester powder coated aluminium facing projecting entrance canopy
12. Polyester powder coated metal panels with no visible floor fixings, projecting cornice required at each change of direction
13. Polyester powder coated metal awninger hopper and downpipe
14. Framesless clear tempered glass balustrade
15. Polyester powder coated metal post and rail balustrading
16. Steel frames **OMITTED** (to be confirmed)
17. Low wall and fixing to mark boundary walls in finish bond lime mortar, polyester powder coated metal post and rail balustrading
18. Polyester powder coated metal access gate with open panel (100mm spacing)
19. External metal cladding with Polyester powder coated metal post and rail balustrading
20. Polyester powder coated metal louvers
21. Perforated brickwork panel



GA Elevation E04 - North-West Boundary



GA Elevation E06 - North-West Yard

A PRG 01.03.16 Planning and Design & Construction
 Ockers recommendations
 Issued for Planning - 26.02.16



Client: Tederal Securified Property GP Ltd
 Project: 70 Westwick Street
 Title: Planning - Proposed Elevations E04 and E06
 Scale: 1/41
 Date: 15.02.2016
 Drawn: FR
 Check: A
 14.01.15-2022
 1:100



Reviewed by SFL

Report to Planning applications committee
13 October 2016

Report of Head of planning services

Subject 16/00759/F - 137 Unthank Road Norwich NR2 2PE

Reason for referral Objection

Item

4(c)

Ward:	Nelson
Case officer	Ms Charlotte Hounsell - charlottehounsell@norwich.gov.uk

Development proposal		
Demolition of shop. Construction of 1 No. ground floor retail (Class A1) unit, 1 No. ground floor hot food takeaway and restaurant (Class A3 and Class A5) and 1 No. first floor dwelling.		
Representations		
Object	Comment	Support
15 1 petition	1	0

Main issues	Key considerations
1 Principle of development	Number of non-retail units Number of A5 units Vacancy of the building Demolition
2 Design	Issue of demolition Loss of original features Materials Out of scale development
3 Residential amenity	Increase in HMO accommodation Noise and odour pollution Litter
4 Parking, highways and servicing.	Increased congestion Parking problems Proximity to pedestrian crossing Waste collection and storage.
Expiry date	26 July 2016
Recommendation	Approve



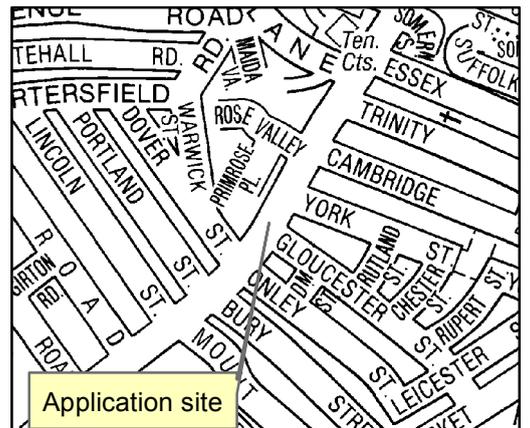
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Planning Application No 16/00759/F
 Site Address 137 Unthank Road

Scale 1:500



NORWICH
 City Council
 PLANNING SERVICES



The site and surroundings

1. The subject site is located on the North East side of Unthank Road, South West of the City Centre. The subject building was previously used as a hardware store and ironmongers. The site contains the main building to the front of the site, a single storey flat roof extension, a small detached outbuilding and a large workshop outbuilding. The site is located between (but outside of) the Heigham Grove and Newmarket Road Conservation Areas and is located within the parade of shops (of mixed uses) within the Unthank Road Local Retail Centre. The surrounding area also includes features retail and commercial uses as well as a high proportion of residential accommodation. Off-road parking is present on the forecourt area at the front of the site in close proximity to the pedestrian crossing. The existing building has been vacant for at least 8 years and is in a state of disrepair.

Constraints

2. The site is located within a Local Retail Centre.
3. The site is located within a Critical Drainage Area.

Relevant planning history

- 4.

Ref	Proposal	Decision	Date
08/01062/U	Change of use from ironmongers/DIY store (A1) to residential letting and estate agency (A2).	REF	23/12/2008
13/00900/F	Demolition of existing shop, outbuildings and erection of 1 No. shop with 2 No. dwellings above.	WITHDN	22/08/2013
14/00388/F	Demolition of the existing shop, outbuildings, retention of the former workshop building and erection of a new building containing a shop (A1) and cafe (A3) on the ground floor and 1 no. dwelling above. The proposal also includes an external sales area to the rear and front forecourt.	APPR	06/06/2014
16/00306/F	Demolition of shop. Retention of outbuildings and erection of 1 No. restaurant / cafe (class A3) and 1 No. hot food takeaway (class A5) with 1 No. four bedroom dwelling above.	WITHDN	29/04/2016

The proposal

5. The proposal is for the demolition of the existing retail unit and replacement with 1 No. retail unit, 1 No. hot food takeaway/restaurant. The large warehouse outbuilding to the rear is to remain as existing. The residential accommodation comprises an external access to the rear of the site, with outdoor amenity space at the rear which contains cycle and bin storage. The commercial units include an outdoor seating area and cycle parking to the forecourt area.
6. The application has been revised. The originally submitted proposal also included the conversion of the main ironmongers' outbuilding to an additional hot food takeaway/restaurant unit. This has been removed from the proposal due to concerns relating to the over-intense use of the site and adverse effects on residential amenity. In addition, parking provision has been removed from the proposal due to highway safety concerns.

Summary information

Proposal	Key facts
Scale	
Total no. of dwellings	1 x 4 bedroom dwelling
No. of affordable dwellings	None
Total floorspace	356m² total floorspace 128m² market housing 228m² Retail and hot food takeaway use
No. of storeys	2 storeys
Max. dimensions	Approximately 4.50m x 7.70m, 5.30m at the eaves and 7.20m at maximum height
Appearance	
Materials	Materials to be secured by condition
Energy and resource efficiency measures	To be secured by condition
Operation	
Opening hours	08:00-23:00
Ancillary plant and equipment	External ventilation for the A3/A5 use – details to be secured by condition

Transport matters	
Vehicular access	None proposed
No of car parking spaces	None proposed – car free housing permitted in this area. No parking permits would be provided.
No of cycle parking spaces	3 x cycle parking spaces – details to be secured by condition.
Servicing arrangements	Commercial and residential bin stores provided at the rear of the site and collection to be at the front of the site. Details of bin store to be secured by condition.

Representations

7. Advertised on site and in the press. Adjacent and neighbouring properties have been notified in writing. 13 letters of representation and one petition have been received citing the issues as summarised in the table below. All representations are available to view in full at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Issues raised	Response
Loss of retail unit and demolition of building	See main issue 1
Number of existing hot food takeaways which will be detrimental to the character of the area	See main issue 1
Loss of original features	See main issue 2
No details of materials	See main issue 2
Out of scale development	See main issue 2
Detrimental to residential amenity; noise and odour pollution, litter	See main issue 3
Parking and congestion problems	See main issue 4
Increased bin storage and collection	See main issue 4
Proposals will result in increase in HMO accommodation to the detriment of the character of the area.	No HMO's are proposed as part of this application.

Consultation responses

8. Consultation responses are summarised below the full responses are available to view at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Highways (local)

9. No objection in principle apart from the provision of parking spaces at the front. Vehicle access is difficult as the only section of dropped kerb is across the pedestrian crossing and the kerbed section is not an official vehicle access. The business premises would be entitled to business permits but the residential property would not. It would be preferable if the development would be car free with no vehicles allowed on the hardstanding. An operational car parking space could be allowed. This would free up the forecourt to be used for a seating area and give more space for cycle parking and perhaps landscaping. The footway is very narrow at this point and we would like to widen the footway taking land from the forecourt for this purpose by section 38 agreement. This would require resurfacing and protective bollards.

Norfolk police (architectural liaison)

10. The design of doors and windows should have certification to police preferred standards. Conversion of the outbuilding to a hot food takeaway with an unrestricted rear access could result in security issues for the occupiers. Recommend that appropriate fencing and access control systems are used. The gates should be positioned as close to the front as possible. As the site is located outside of the Late Night Activity Zone, the units should not be permitted to open past 00:00hrs.

Assessment of planning considerations

Relevant development plan policies

11. **Joint Core Strategy for Broadland, Norwich and South Norfolk adopted March 2011 amendments adopted Jan. 2014 (JCS)**
 - JCS1 Addressing climate change and protecting environmental assets
 - JCS2 Promoting good design
 - JCS3 Energy and water
 - JCS4 Housing delivery
 - JCS6 Access and transportation
 - JCS19 The hierarchy of centres
12. **Norwich Development Management Policies Local Plan adopted Dec. 2014 (DM Plan)**
 - DM1 Achieving and delivering sustainable development
 - DM2 Ensuring satisfactory living and working conditions
 - DM3 Delivering high quality design
 - DM5 Planning effectively for flood resilience
 - DM6 Protecting and enhancing the natural environment

- DM8 Planning effectively for open space and recreation
- DM11 Protecting against environmental hazards
- DM12 Ensuring well-planned housing development
- DM18 Promoting and supporting centres
- DM21 Protecting and supporting district and local centres
- DM24 Managing the impacts of hot food takeaways
- DM28 Encouraging sustainable travel
- DM29 Managing car parking demand in the city centre
- DM30 Access and highway safety
- DM31 Car parking and servicing
- DM32 Encouraging car free and low car housing

Other material considerations

13. Relevant sections of the National Planning Policy Framework March 2012 (NPPF):

- NPPF0 Achieving sustainable development
- NPPF2 Ensuring the vitality of town centres
- NPPF4 Promoting sustainable transport
- NPPF6 Delivering a wide choice of high quality homes
- NPPF7 Requiring good design
- NPPF8 Promoting healthy communities
- NPPF10 Meeting the challenge of climate change, flooding and coastal change
- NPPF11 Conserving and enhancing the natural environment

Case Assessment

14. Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. Relevant development plan policies are detailed above. Material considerations include policies in the National Planning Framework (NPPF), the Councils standing duties, other policy documents and guidance detailed above and any other matters referred to specifically in the assessment below. The following paragraphs provide an assessment of the main planning issues in this case against relevant policies and material considerations.

Main issue 1: Principle of development

15. Residential key policies and NPPF paragraphs – DM12, NPPF paragraphs 49 and 14.
16. Non-residential key policies and NPPF paragraphs – JCS19, DM20, DM21, DM24 NPPF paragraph 23.

Principle of Demolition

17. The subject site is not an identified heritage asset (local or statutory) nor is it located within a conservation area. Therefore planning permission is not required for its demolition (only prior approval). In addition, the building could also be demolished under extant permission (14/00388/F), subject to discharging

conditions. This permission also demonstrated that, due to the current condition of the building, it would not be viable to use the existing building. Therefore, whilst it is regrettable to lose this historic feature of the street scene, there is not significant justification to refuse the proposal based on the loss of the original building. Therefore the principle of demolition of the building is acceptable subject to sufficient detail and design.

Principle of Use

18. No. 137 Unthank Road falls within the Unthank Road Local Retail Centre which is currently made up of 44 units. As of 29 September 2016, 20 of units were A1 retail use and 22 were non-retail. In accordance with policy DM21, the proportion of retail units (A1) within a local retail centre should not fall below 50% and the current percentage of A1 unit is 47.6%. There are currently four existing A5 units and two existing A3 units which together account for 14.3% of the units within the centre.
19. Due to the addition of a further unit to the site, the percentage of A1 units within the local centre will be further reduced to 46.5%. However, as the proposal includes the provision of a retail unit, there is no loss of retail units overall in the centre. The inclusion of the A3/A5 unit would then account for 16.3% of A3/A5 units within the centre overall. This is not considered to be a significant percentage of units. In addition it should be noted that the proportion of retail and restaurant floorspace is similar to the approved extant permission. As such the proposed mix of class A uses is considered appropriate for the centre in accordance with part a) of policy DM21.
20. Concerns were raised that there would be a significant number of A5 units along the parade which would detrimental the vitality and diversity of services within the centre (part b) of the policy). Including the new A3/A5 unit, the use would account for 16.3% of the units within the centre, which is considered to be a relatively small proportion. In addition, the existing unit has been vacant for at least 8 years and therefore it could be argued that an empty unit affects the vitality of the centre and that this proposal seeks to improve it.
21. Part c) of the policy states that the proposal should not be detrimental to amenity, traffic or the environment. These issues are addressed within main issues 3 and 4 of the report. .
22. Under part d) of the policy, the proposal should provide a community benefit or address a deficiency in the area. It could be argued that provision of active businesses will be beneficial to the area in comparison to the existing vacant unit. As such the principle of the proposed uses is considered to be in accordance with policy DM21.

Main issue 2: Design

23. Key policies and NPPF paragraphs – JCS2, DM3, NPPF paragraphs 9, 17, 56 and 60-66.
24. The proposed building is of a similar height to the existing building and does not exceed the roof heights of the neighbouring units. It is noted that the proposed

building does extend further into the rear of the site than the existing building and is therefore of a greater massing than the adjacent units.

25. Whilst the existing building is currently not protected by any heritage designations, it is an attractive building prominent in the street scene. Therefore the details of the design should be suitable so that the proposal fits within the context of the surrounding area. The proposed materials include red brick, slate roof, and uPVC windows and doors to the rear of the site. The shop front is to be timber and the windows at the first floor on the front elevation are to be timber sliding sash design. The materials and details of the front façade are well related to the surrounding area and further details of the shop front are required by condition. The front forecourt area is proposed to be landscaped to improve its appearance and provide an outdoor seating area for the proposed units, as well as increasing the width of the footway. This will enhance the appearance of the frontage of the site in comparison to the existing situation.

Main issue 3: Amenity

26. Key policies and NPPF paragraphs – DM2, DM11, DM12, NPPF paragraphs 9 and 17.
27. The proposed residential unit exceeds the national minimum internal space standards and benefits from outdoor external private amenity space. In addition, the position of windows is not considered to result in significant overlooking of the neighbouring properties. It is noted that the proposed building is of a greater depth than the existing, however this is not considered to result in any significant loss of light to neighbouring habitable rooms.
28. Concerns were raised regarding the potential for noise and odour pollution. The details of the extract ventilation proposed at the rear of the building are to be secured by condition. This will include the provision of evidence that the ventilation system is adequate for the A3/A5 use as well as evidence that proposed anti-vibration mounting and other noise dampening measures will be included to reduce the impact.
29. The proposed use has the potential to result in increased noise and disturbance as a result of customers entering and leaving the premises, sometimes late at night and waiting outside the premises. However planning permission has already been granted for an A3 use at the site, with potential for some take away trade. The site is within a local centre where A5 uses are to be expected and the surrounding area includes a mix of uses. A condition will also be included restricting opening hours to between 08:00-23:00hrs on any day and deliveries and waste collections between 07:00 and 19:00 hrs, except Sundays and Bank Holidays. As such the proposed units are not considered to detract unduly from the amenity of occupiers in the surrounding area.

Main issue 4: Transport

30. Key policies and NPPF paragraphs – JCS6, DM28, DM30, DM31, DM32 NPPF paragraphs 17 and 39.

31. Concerns have been raised that the proposed development would result in increased congestion from customers trying to use the originally proposed parking area on the forecourt. This proposed parking area has been removed from the front forecourt area due to concerns that the number of potential vehicles crossing the pavement (which is currently not an approved vehicle crossover) would be dangerous due to the proximity to the pedestrian crossing. Policy DM31 states that one disabled parking space should be provided for a site of this size (for A1/A3/A5 use), however the provision of parking onsite is arguably detrimental to pedestrian movement and therefore in this instance no off street provision is considered acceptable.
32. In addition, the revised proposal does not include parking for the residential unit. In accordance with DM31, new dwellings in this location along high quality public transport corridors are permitted to be car free. The residential unit will have secure cycle storage to the rear of the site and will not receive parking permits for on street parking. The proposed commercial units will receive business permits and also provide cycle parking, with the aim of promoting more sustainable transport methods. Adequate bin stores have also been proposed.
33. The applicant has agreed to negotiate with Norwich City Council regarding the provision of part of their land closest to the highway being used to widen the footway adjacent to the pedestrian crossing.

Compliance with other relevant development plan policies

34. A number of development plan policies include key targets for matters such as parking provision and energy efficiency. The table below indicates the outcome of the officer assessment in relation to these matters.

Requirement	Relevant policy	Compliance
Cycle storage	DM31	Yes subject to condition
Car parking provision	DM31	Yes – car free housing Business permits only to be provided
Refuse Storage/servicing	DM31	Yes subject to condition
Energy efficiency	JCS 1 & 3 DM3	Yes subject to condition
Water efficiency	JCS 1 & 3	Yes subject to condition
Sustainable urban drainage	DM3/5	Yes subject to condition for hard landscaping materials
Biodiversity / protected species	JCS1, DM6	An ecology report has been submitted as part of this application stating that there was very little evidence suggesting that the building was being used by bats or birds. Therefore the demolition of the building is

		unlikely to result in harm to biodiversity in accordance with DM6.
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Equalities and diversity issues

35. There are no significant equality or diversity issues.

Local finance considerations

36. Under Section 70(2) of the Town and Country Planning Act 1990 the council is required when determining planning applications to have regard to any local finance considerations, so far as material to the application. Local finance considerations are defined as a government grant or the Community Infrastructure Levy.
37. Whether or not a local finance consideration is material to a particular decision will depend on whether it could help to make the development acceptable in planning terms. It would not be appropriate to make a decision on the potential for the development to raise money for a local authority.
38. In this case local finance considerations are not considered to be material to the case.

Conclusion

39. The proposed demolition of the existing building is acceptable in principle. Whilst the percentage of A1 units is reduced within the retail centre, there is no actual loss of retail units. The provision of a further A3/A5 unit would not account for a significant proportion of the units within the retail centre and therefore, on balance, this proposal is likely to improve the vitality and diversity of the centre as opposed to retaining a vacant unit. The design of the building is acceptable. The proposals are also acceptable in terms of amenity and transport.
40. The development is in accordance with the requirements of the National Planning Policy Framework and the Development Plan, and it has been concluded that there are no material considerations that indicate it should be determined otherwise.

Recommendation

To approve application no. 16/00759/F - 137 Unthank Road Norwich NR2 2PE and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Details of all materials including specifically the shop front and front façade
4. Details of soft and hard landscaping including rear amenity area, front forecourt, boundary treatments, bollards etc.
5. Details of bin and cycle storage and parking
6. Prior to any occupation as an A3/A5 use details of extract equipment / silencers/anti-vibration mounting/insulation that can demonstrate operation at acceptable noise levels must be submitted.

7. The A3/A5 unit shall not be open between the hours of 23:00 and 08:00hrs on any day
8. No trade deliveries or waste collections to take place between the hours of 19:00 and 07:00hrs on any day and no deliveries or waste collection to take place on Sundays and Bank Holidays.
9. To be built to energy efficiency/water efficiency standards
10. Permitted development rights for fencing removed in order to protect the open nature of the forecourt area
11. All windows within the side elevations shall be obscure-glazed and non Opening unless the parts of the windows which can be opened are more than 1.7 metres above the floor of the room in which the windows are installed;

Informatives

It should be noted that separate advertisement consent should be applied for prior to erection of advertisements at the site.

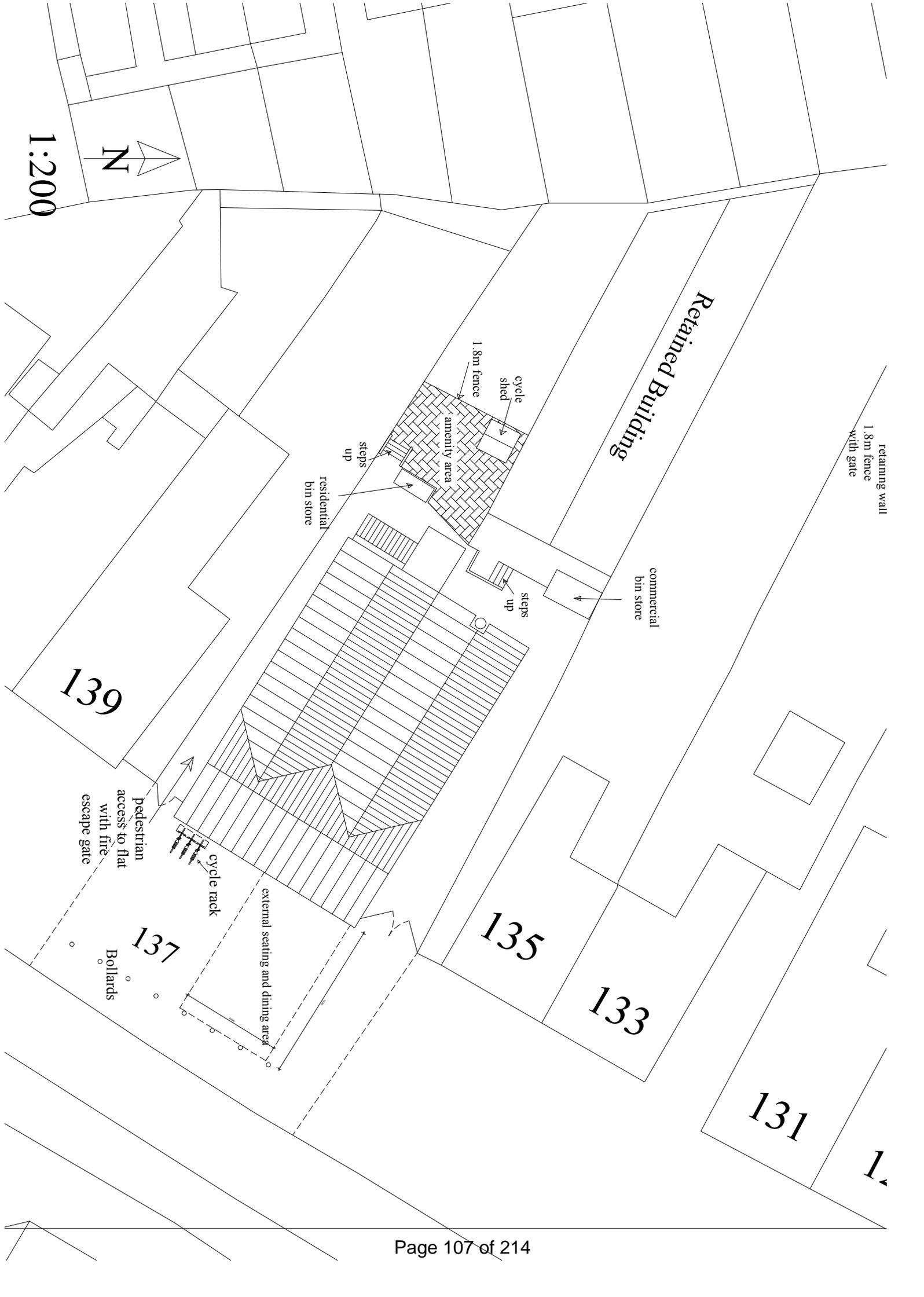
If the residential dwelling is occupied at any time by more than six unrelated residents then a separate application should be submitted for a change of use to a House of Multiple Occupation (HMO).

The applicant/agent should undertake negotiations with the Highways Department of Norwich City Council regarding a S38 agreement for the adoption of part of the land outlined in this application for highway purposes.

Article 35(2)

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined in the officer report.

1:200



139

Retained Building

1.8m fence
amenity area
cycle shed
steps up
residential bin store

commercial bin store
steps up

retaining wall
1.8m fence
with gate

pedestrian access to flat with fire escape gate

cycle rack

external seating and dining area

Bollards

137

135

133

131

1

Report to Planning applications committee
 13 October 2016

Report of Head of planning services

Subject 16/01117/F – Land west of 3 Beaumont Place

Reason for referral Objections and application affecting City Council land.

Item

4(d)

Ward:	Wensum
Case officer	Robert Webb - robertwebb@norwich.gov.uk

Development proposal		
2 no. dwellinghouses (one 3 bedroom and one 2 bedroom) and associated car parking.		
Representations		
Object	Comment	Support
9	0	0

Main issues	Key considerations
1 Principle of development	Principle of redevelopment for housing
2 Design	Impact on character of the area, scale, form, massing and appearance.
3 Transport	Accessibility of site, impact on car parking, traffic, highway safety, cycle parking, servicing.
4 Amenity	Impact on neighbouring occupiers
5 Trees and Landscaping	Consideration of landscaping, impact on trees
6 Flood risk	Flood risk to the development and impact of the proposal on flood risk
Expiry date	20 October 2016
Recommendation	Approval subject to conditions.



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Planning Application No 16/01117/F

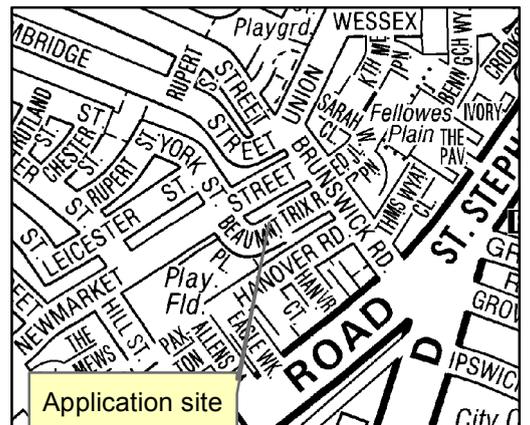
Site Address Land West of 3 Beaumont Place

Scale 1:1,000



NORWICH
City Council

PLANNING SERVICES



The site and surroundings

1. The site is located to the south of the city centre close to Newmarket Road and is a surface level car park which accommodates 14 spaces and is owned and managed by Norwich City Council. The site is surrounded by residential dwellings with Beaumont Place to the south east and Eagle Park is within very close proximity to the south.

Constraints

2. The site is within a Critical Drainage Area.

Relevant planning history

3. There is no relevant planning history held by the City Council.

The proposal

1. The proposal is one of a number of sites identified by Norwich City Council as having the potential to accommodate new affordable housing to be developed by a registered provider, Orwell Housing Association. The Council are seeking overall to deliver 66 affordable units across the city as part of the current programme, and these would be designed to meet Homes and Communities Agency (HCA) design and quality standards. The dwellings would be available at social or affordable rent whilst meeting high environmental standards. All homes would be advertised using the City Council's choice based letting scheme.
2. This application seeks to develop the site to provide two affordable semi-detached dwellings facing onto Beaumont Place, consisting of one 2 bedroom and one 3 bedroom property with associated parking, cycle storage and private garden space. Solar panels would be placed on the south and west facing roof.

Summary information

Proposal	Key facts
Scale	
Total no. of dwellings	2
No. of affordable dwellings	2
Total floorspace	154.7 square metres (2 bed dwelling – 72.4m ² , 3 bed dwelling 82.3m ²).
No. of storeys	2
Density	47 dwellings per hectare
Appearance	

Materials	Walls – red stock brickwork. Roof – red/orange concrete pantiles. Windows – white Upvc double glazed units. Doors – GPR/timber front entrance doors.
Energy and resource efficiency measures	Solar pv panels, low energy lighting, gas condensing combination boiler with flue gas heat recovery system.
Transport matters	
Vehicular access	From Beaumont Place
No of car parking spaces	2 (1 per property)
No of cycle parking spaces	1 cycle shed per property
Servicing arrangements	Bin storage within gardens.

Representations

3. Advertised on site and in the press. Adjacent and neighbouring properties have been notified in writing. 9 letters of representation have been received citing the issues as summarised in the table below. All representations are available to view in full at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Issues raised	Response
Concern at the loss of the car park and increased parking pressure	See main issue 4.
Loss of turning space for larger vehicles and potential impact on highway safety	See main issue 3.
Impact on no. 3 Beaumont Place in terms of loss of light and impact on existing rear access	See main issue 4.
Impact on other surrounding properties in terms of loss of light and loss of privacy.	See main issue 4.
Concern at the loss of the planted area on Beaumont Place.	See main issue 5.

Consultation responses

4. Consultation responses are summarised below. The full responses are available to view at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Environmental protection

5. I have viewed the desk study provided for this application and agree with the recommendation that further intrusive works are required. If approval is given, I suggest that conditions are applied.

Highways (local)

6. No objection on highway/transportation grounds. The development and site layout is acceptable, parking, refuse and cycle storage are acceptable.

Tree protection officer

7. No response received.

NCC Housing

8. Car park surveys have been carried out on this site and the surrounding roads to assess usage. On average, the car park is 43% occupied. On 30/9/16 there were 3 garages available to rent within 300 metres of the site. Our surveys have also shown that some of the cars using this car park do not have parking permits, and have been using this as a free parking area rather than using the on street permit controlled parking spaces.

Assessment of planning considerations

Relevant development plan policies

9. **Joint Core Strategy for Broadland, Norwich and South Norfolk adopted March 2011 amendments adopted Jan. 2014 (JCS)**
 - JCS2 Promoting good design
 - JCS3 Energy and water
 - JCS4 Housing delivery
 - JCS7 Supporting communities
 - JCS9 Strategy for growth in the Norwich policy area
10. **Norwich Development Management Policies Local Plan adopted Dec. 2014 (DM Plan)**
 - DM1 Achieving and delivering sustainable development
 - DM2 Ensuring satisfactory living and working conditions
 - DM3 Delivering high quality design
 - DM4 Providing for renewable and low carbon energy
 - DM5 Planning effectively for flood resilience
 - DM11 Protecting against environmental hazards
 - DM12 Ensuring well-planned housing development
 - DM28 Encouraging sustainable travel

- DM30 Access and highway safety
- DM31 Car parking and servicing

Other material considerations

11. Relevant sections of the National Planning Policy Framework March 2012 (NPPF):

- NPPF4 Promoting sustainable transport
- NPPF6 Delivering a wide choice of high quality homes
- NPPF7 Requiring good design
- NPPF10 Meeting the challenge of climate change, flooding and coastal change
- NPPF11 Conserving and enhancing the natural environment

Case Assessment

12. Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. Relevant development plan policies are detailed above. Material considerations include policies in the National Planning Framework (NPPF), the Councils standing duties, other policy documents and guidance detailed above and any other matters referred to specifically in the assessment below. The following sections provide an assessment of the main planning issues in this case in relation to the relevant policies and material considerations.

Main issue 1: Principle of development

13. Joint Core Strategy (JCS) Policy 4, supports housing delivery within the plan area, which this site falls. National policy, as set out in the Core Principles of the NPPF encourages new housing development to make the fullest possible use of public transport, walking and cycling, and focus significant development in locations which are or can be made sustainable. JCS policy 4 also encourages provision of affordable housing including of social rent and affordable rent tenure types as these are recognised and being particularly important in meeting housing need in the city.
14. Policy DM12 of the Norwich Development Management Policies Plan supports new residential development within the city boundary except in specific circumstances, none of which exceptions apply to this application site.
15. The NPPF encourages 'the effective use of land by reusing land that has been previously developed'. This site constitutes previously developed land and is also in a sustainable location for new housing, with good links to the City Centre. The proposed housing is therefore considered to be acceptable in principle and in this case would have the planning benefits of providing new affordable housing, subject to assessment against any other relevant policies or material considerations as outlined in the NPPF and the Development Plan. This is further set out below.

Main issue 2: Design

16. Key policies and NPPF paragraphs – JCS2, DM3, NPPF paragraphs 9, 17, 56 and 60-66.

17. The site is located in an area which is characterised by two storey residential development, some of which dates from the Victorian period and some of which is more modern. The proposal would introduce two new dwellings which would be of a similar form and scale to the adjacent Victorian properties, albeit they are designed for modern living on the inside. The design is sympathetic to the character of the street and provides a good standard of external amenity space.
18. The national space standards figure states a minimum recommended internal floor area of 81 square metres for a two bed, 4 person property and 95.5m² for a 3 bed 5 person property. The dwellings proposed would have an internal floor area of 72.4 m² and 82.3m² respectively which does not meet the recommended standard. However it is noted that if the properties were occupied by 3 / 4 persons then they would comply with the standard. Whilst this is regrettable, it is not considered in itself to be a reason to warrant refusal of the application, given the proposal is otherwise well designed and would lead to the delivery of affordable housing in a sustainable location.

Main issue 3: Transport

19. Key policies and NPPF paragraphs – DM2, DM11, NPPF chapter 4.
20. The site is located in an accessible location within walking distance of St. Stephen's Street which is a main transport corridor and also the city centre itself. Each of the dwellings would be provided with a parking space and cycle shed in accordance with the Council's parking standards set out in the Local Plan. The proposal therefore complies with relevant policies in terms of transport. The impact of the loss of parking is addressed in the following section.
21. No objection is raised by the Highway Officer on the grounds of highway safety. Beaumont Place is a cul-de-sac and whilst the proposal would prevent the opportunity to use the site for turning purposes, there is an access on the opposite side of the road just a few metres further up which would still afford the opportunity for vehicles, including larger delivery vehicles and minibuses to reverse into and turn.

Main issue 4: Amenity

22. Key policies and NPPF paragraphs – DM2, DM11, NPPF paragraphs 9 and 17.
23. Many of the objections state concern at the loss of the car park. A survey submitted with the application indicated that the car park is rarely full and had an average occupancy of 43% based on the week the survey took place. Peak use was during the night time when occupancy rose to 57%. The site is within a controlled parking zone and therefore existing and proposed residents have the opportunity to purchase a parking permit to park on the surrounding street network.
24. The Highway Officer raises no objection to the loss of the car park. It is acknowledged that some harm would be caused to residential amenity by the loss of the existing spaces. However this must be weighed against the significant benefits of delivering affordable housing in a sustainable location within the city. This will be a matter for Members to judge, but the advice from Officers is that in planning policy terms addressing housing need is considered to be of greater importance than providing off-road parking spaces, particularly in a location which has good links to public

transport and the city centre and where there is the opportunity to use other modes of transport such as buses and cycles to travel. It is therefore recommended that the application should not be refused on the grounds of loss of parking.

25. Further objections have been received regarding the impact of the development on privacy and natural light to existing properties. This matter has been fully assessed and it is concluded that due to the careful design and siting of the proposal no material harm would occur in terms of overshadowing. Any overlooking of gardens that might arise would be typical for a normal terraced street and in terms of directly facing windows the relationship between existing and proposed would be similar to the existing properties further down the street in terms of separation distances. This is considered acceptable in planning terms.
26. An objection has been received from the owner of no. 3 Beaumont Place who is concerned that the proposal would compromise the rear access to his property, due to the proposed width of the access between the existing and proposed house. This width would be approximately 1.2m which complies with the minimum standard required by Building Regulations and is therefore considered acceptable.

Main issue 5: Trees and landscaping

27. Key policies and NPPF paragraphs – DM3, DM7, DM8, NPPF paragraphs 9, 17, 56, 109 and 118.
28. The site is largely covered in hardstanding with 2 category C Hawthorn trees and a small area of plants and shrubs along the site frontage. The planted area and 1 of the trees would be removed as part of the proposal, with the other tree being retained. The proposal would however introduce new areas of planting and also private amenity space which would present further opportunities for planting. The details of the landscaping would be agreed by condition. It is therefore considered that the loss of the existing area would be mitigated by replacement planting and overall the proposal complies with relevant policies in relation to trees and landscaping.

Main issue 6: Flood risk

29. Key policies and NPPF paragraphs – JCS1, DM5, NPPF paragraphs 100 and 103.
30. The site is in a Critical Drainage Catchment as defined by the Norwich Surface Water Management Plan. Developers are required to show that the proposed development would not increase the vulnerability of the site, or the wider catchment, to flooding from surface water run-off from existing or predicted water flows. In addition, where practicable, the proposal should have a positive impact on the risk of surface water flooding in the area.
31. The site is within flood zone 1, and therefore at a low probability of flooding from rivers. A sustainable approach to water management is proposed. The development would maximise the use of soft landscaping and incorporate permeable paving for hard services whilst using a main sewer connection for water run-off. It is stated that the proposal would result in a significant reduction of impermeable surfacing. The proposal complies with the relevant policies in relation to flood risk.

Other matters

32. The proposal is acceptable in terms of its impact on biodiversity, land contamination and the energy efficiency measures proposed.

Equalities and diversity issues

33. There are no significant equality or diversity issues.

Local finance considerations

34. Under Section 70(2) of the Town and Country Planning Act 1990 the council is required when determining planning applications to have regard to any local finance considerations, so far as material to the application. Local finance considerations are defined as a government grant or the Community Infrastructure Levy.

35. Whether or not a local finance consideration is material to a particular decision will depend on whether it could help to make the development acceptable in planning terms. It would not be appropriate to make a decision on the potential for the development to raise money for a local authority.

36. In this case local finance considerations are not considered to be material to the case.

Conclusion

37. The proposal in Beaumont Place would provide two new affordable homes in a sustainable location and is considered to be acceptable in planning terms. There would be some harm caused in terms of the loss of the existing car park, and clearly this is a contentious matter for existing residents. However this must be balanced against the benefits of the proposal in terms of providing much needed affordable housing and this benefit is considered to outweigh the loss of the site for parking.

38. The development is in accordance with the requirements of the National Planning Policy Framework and the policies of the Development Plan, and there are no material considerations that indicate it should be determined otherwise.

Recommendation

To approve application 16/01117/F and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Details of facing and roofing materials; windows; joinery; boundary treatments, walls and fences; external lighting;
4. Details of hard and soft landscaping and planting
5. Water efficiency
6. Contamination risk assessment and report to be submitted
7. Unknown contamination to be addressed
8. Control on imported materials
9. Tree protection measures to be implemented

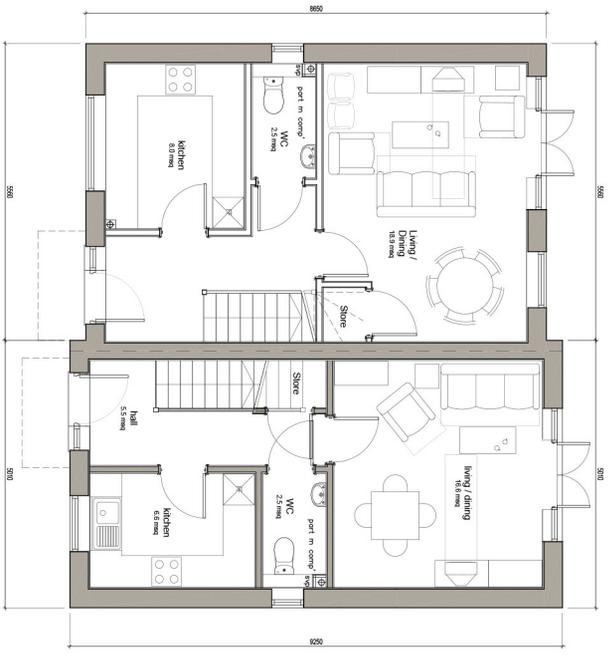
The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the

applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined in the officer report.



Plot 1 First Floor Plan

Plot 2 First Floor Plan



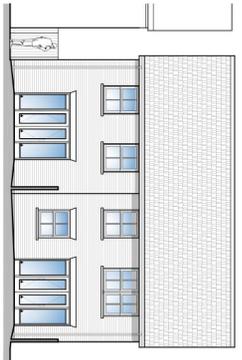
Plot 1 Ground Floor Plan

Plot 2 Ground Floor Plan

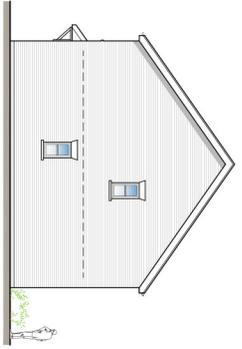


proposed southeast elevation 1:100

proposed southwest elevation 1:100



proposed northwest elevation 1:100



proposed northeast elevation 1:100

peterwellsarchitects

office from: kettingingham, woodbridge, Suffolk, IP13 7BA - 01728 745356 - info@peterwellsarchitects.co.uk

Project:	NCC GARAGE SITE BEAUMONT PLACE, NR2 2HH
Design Title:	Proposed Plans & Elevations
Client:	Orwell Housing
Date:	April 2016
Scale:	1:50, 1:100 @ A1
Design No.:	PW/52_P103
Revision:	D

Drawn by: JRM
Checked by: JRM
Designed by: JRM
Approved by: JRM
Authorised by: JRM
Date: 12/04/16

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Report to Planning applications committee
 13 October 2016

Report of Head of planning services

Subject 16/01098/F - Garages adjacent 56 Sotherton Road,
 Norwich

**Reason
 for referral** Objections and application affecting City Council land.

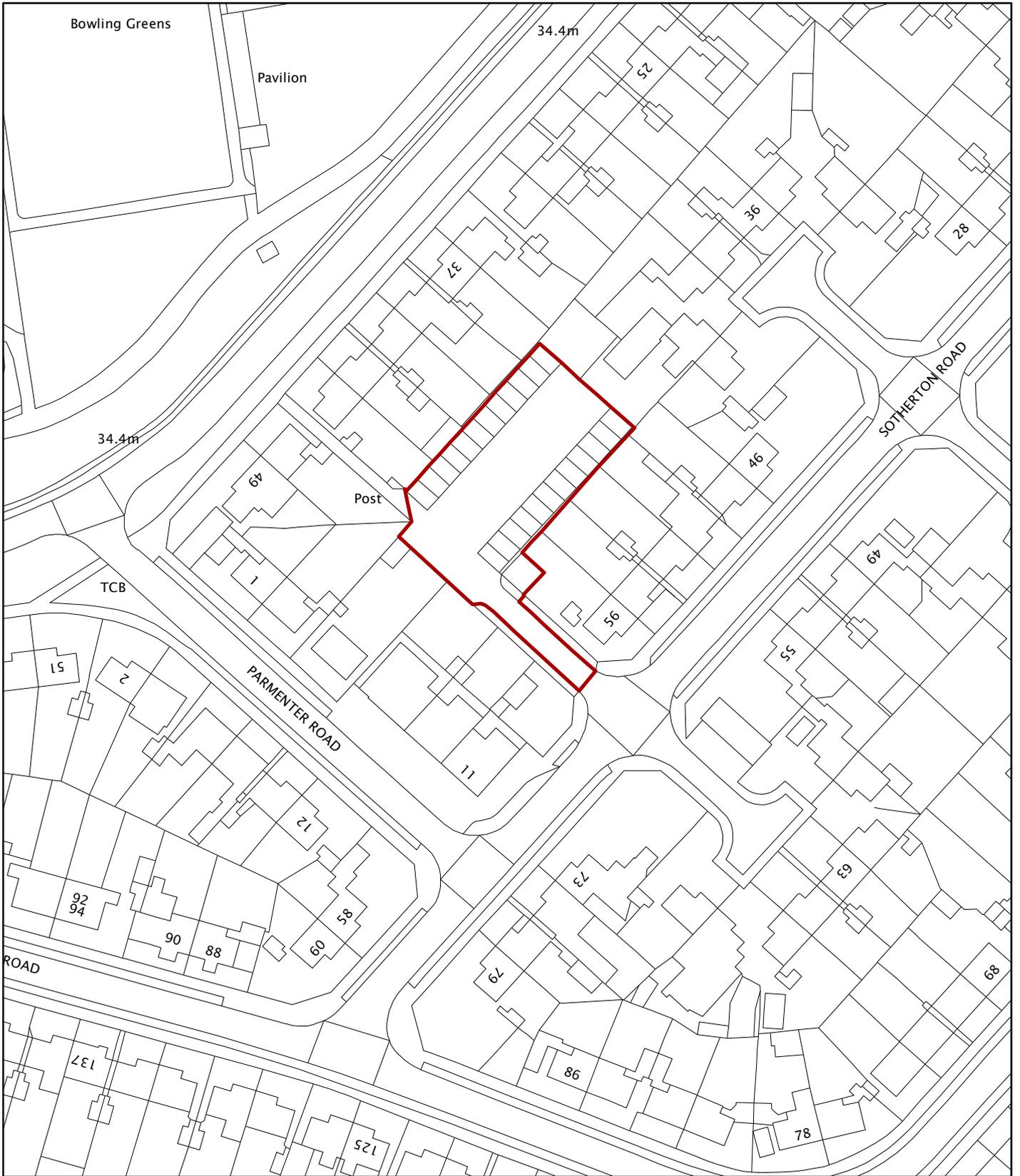
Item

4(e)

Ward:	Eaton
Case officer	Robert Webb

Development proposal		
Demolition of 14 No. garages and erection of 2 No. two bed dwellings.		
Representations		
Object	Comment	Support
4+petition signed by 29	0	0

Main issues	Key considerations
1 Principle of development	Principle of redevelopment for housing
2 Design/ Heritage	Impact on character of the area, scale, form, massing and appearance.
3 Transport	Accessibility of site, impact on car parking, traffic, highway safety, cycle parking, servicing.
4 Amenity	Impact on neighbouring occupiers, loss of parking
5 Trees and Landscaping	Consideration of landscaping, impact on trees
Expiry date	20 October 2016
Recommendation	Approval subject to conditions.



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Planning Application No 16/01098/F

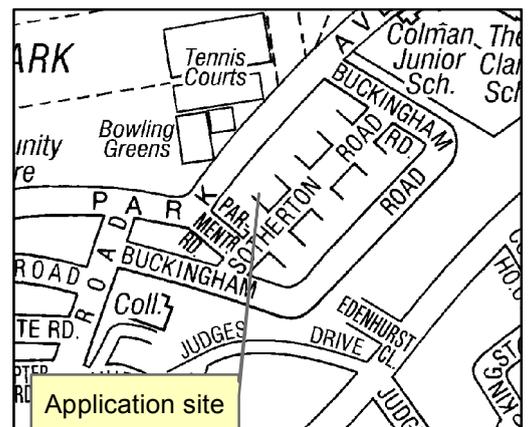
Site Address Garages adjacent 56 Sotherton Road

Scale 1:1,000



NORWICH
City Council

PLANNING SERVICES



The site and surroundings

1. The site is owned by Norwich City Council and currently comprises two garage blocks with a total of 26 garages which are available for public rent. The blocks are between Sotherton Road and South Park Avenue and the site is surrounded by two-storey residential development. Eaton Park is a short distance to the north-west.

Constraints

2. The site is adjacent to a conservation area and a number of locally listed buildings.

Relevant planning history

3. No relevant planning history.

The proposal

4. The proposal is one of a number of sites identified by Norwich City Council as having the potential to accommodate new affordable housing to be developed by a registered provider, Orwell Housing Association. The Council are seeking overall to deliver 66 affordable units across the city as part of the current programme, and these would be designed to meet Homes and Communities Agency (HCA) design and quality standards. The dwellings would be available at social or affordable rent whilst meeting high environmental standards. All homes would be advertised using the City Council's choice based letting scheme.
5. In this case permission is sought for the demolition of an existing garage block which contains 14 garages and the erection of two semi-detached properties with associated gardens and parking. The land is currently owned and managed by Norwich City Council. Each of the proposed houses would have 1 parking space, with a further two spaces that would be allocated by the City Council according to local need. The development would maintain access to an electricity sub-station.

Summary information

Proposal	Key facts
Scale	
Total no. of dwellings	2 x 2 bedroom dwellings
No. of affordable dwellings	2
Total floorspace	2 x 72.4 square metres (gross internal area)
No. of storeys	2
Density	17 dwellings per hectare

Appearance	
Materials	Walls – Red/Orange concrete pantiles, white uPVC windows, red stock facing brickwork, grp/timber entrance door.
Energy and resource efficiency measures	Solar pv panels, locally sourced materials, thermal bridging detailing, low energy light bulbs.
Transport matters	
Vehicular access	Access from Sotherton Road
No of car parking spaces	2 + 2 disabled spaces
No of cycle parking spaces	1 cycle shed per dwelling
Servicing arrangements	Bins storage to rear of properties, bin presentation area close to vehicle access.

Representations

6. Advertised on site and in the press. Adjacent and neighbouring properties have been notified in writing. 4 letters of representation have been received citing the issues as summarised in the table below. In addition a petition opposing the proposal signed by 29 people has been received. All representations are available to view in full at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Issues raised	Response
Concern regarding the loss of parking facilities	See main issue 4
Loss of privacy	See main issue 4
Impact on ability to manage rear access to neighbours property	See main issue 4
The proposal would affect the ability to access private garage	See main issue 4
It may not be possible for vehicles to turn into the garages that are to be retained given that the doors open outwards and the turning area will be reduced by the proposal.	See main issue 4

Consultation responses

7. Consultation responses are summarised below the full responses are available to view at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Highways (local)

8. No objection on highway/transportation grounds. The development and site layout is acceptable, parking, refuse and cycle storage are acceptable.

NCC Environmental Protection

9. I have viewed the desk study provided for this application and agree with the recommendation that further intrusive works are required. If approval is given, I suggest that conditions are applied. The UXO risk may also require further consideration by a specialist due to the fairly close proximity of known WWII bomb drops.

NCC Housing

10. With regard to existing garage tenants, the home ownership team wrote to all tenants asking them to give an expression of interest for a garage if they wanted to continue using a garage at this site. Of the 26 tenants only 12 returned an expression of interest. The team wrote to all 26 tenants a second time to confirm these but no further expressions of interest were received. There are 12 garages being maintained so all existing tenants can be accommodated.

Assessment of planning considerations

Relevant development plan policies

11. **Joint Core Strategy for Broadland, Norwich and South Norfolk adopted March 2011 amendments adopted Jan. 2014 (JCS)**
 - JCS2 Promoting good design
 - JCS3 Energy and water
 - JCS4 Housing delivery
 - JCS7 Supporting communities
 - JCS9 Strategy for growth in the Norwich policy area
12. **Norwich Development Management Policies Local Plan adopted Dec. 2014 (DM Plan)**
 - DM1 Achieving and delivering sustainable development
 - DM2 Ensuring satisfactory living and working conditions
 - DM3 Delivering high quality design
 - DM4 Providing for renewable and low carbon energy
 - DM5 Planning effectively for flood resilience
 - DM11 Protecting against environmental hazards
 - DM12 Ensuring well-planned housing development
 - DM28 Encouraging sustainable travel
 - DM30 Access and highway safety
 - DM31 Car parking and servicing

- DM32 Encouraging car free and low car housing
- DM33 Planning obligations and development viability

Other material considerations

13. Relevant sections of the National Planning Policy Framework March 2012 (NPPF):

- NPPF4 Promoting sustainable transport
- NPPF6 Delivering a wide choice of high quality homes
- NPPF7 Requiring good design
- NPPF10 Meeting the challenge of climate change, flooding and coastal change
- NPPF11 Conserving and enhancing the natural environment

Case Assessment

14. Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. Relevant development plan policies are detailed above. Material considerations include policies in the National Planning Framework (NPPF), the Councils standing duties, other policy documents and guidance detailed above and any other matters referred to specifically in the assessment below. The following paragraphs provide an assessment of the main planning issues in this case against relevant policies and material considerations.

Main issue 1: Principle of development

15. Joint Core Strategy (JCS) Policy 4, supports housing delivery within the plan area, which this site falls. National policy, as set out in the Core Principles of the NPPF encourages new housing development to make the fullest possible use of public transport, walking and cycling, and focus significant development in locations which are or can be made sustainable. JCS policy 4 also encourages provision of affordable housing including of social rent and affordable rent tenure types as these are recognised and being particularly important in meeting housing need in the city.
16. Policy DM12 of the Norwich Development Management Policies Plan supports new residential development within the city boundary except in specific circumstances, and none of the exceptions apply to this application site.
17. The NPPF encourages 'the effective use of land by reusing land that has been previously developed'. This site constitutes previously developed land. The site is in a sustainable location for new housing with good public transport links to the City Centre. The proposals for housing are therefore considered to be acceptable in principle and in this case would have the planning benefits of providing new affordable housing subject to assessment against any other relevant policies or material considerations as outlined in the NPPF and the Development Plan. This is further set out below.

Main issue 2: Design

18. Key policies and NPPF paragraphs – JCS2, DM1, DM3, DM12 and NPPF paragraphs 9, 17, 56, 57, 60-66.
19. One of the constraints of the site is the need to maintain access to an electricity sub-station, which means that only part of this site would be redeveloped. As a result of this only two dwellings are proposed, and the design is a conventional pair of brick and tile semi-detached houses with a pitched roof. This would be in keeping with the general character of the wider estate and is considered acceptable.
20. Each property would have a large rear garden of approximately 125 square metres, a car parking space and a cycle parking space. The proposal complies with the relevant local plan policies in terms of design and parking requirements.
21. The dwellings proposed would have an internal floor area of 72.4 square metres and the dwellings are intended as 2 bedroom 4 person houses. The floorspace is therefore below the national space standards figure of 81 square metres for this type of property. It is recognised however that if the dwellings were occupied by 3 persons, then the minimum space standard of 72m² would be met. Notwithstanding this, whilst the failure to meet the minimum space standards based on a four person occupancy is regrettable, it is not considered in itself to warrant refusal of the application because the proposal is otherwise well-designed and would lead to the delivery of affordable housing in a sustainable location. The design, layout and materials proposed are considered to be acceptable.

Main issue 3: Transport

22. Key policies and NPPF paragraphs - JCS6, DM28, DM31. NPPF chapter 4.
23. The local policy requires a maximum of 1.33 parking spaces per dwelling and covered and secure cycle parking. The scheme would provide a parking space for each of the two dwellings and each house would have a cycle shed. The proposal is therefore policy compliant in this regard. No objection is raised by the Highway Officer

Main issue 4: Amenity

24. Key policies and NPPF paragraphs – DM2, DM11, NPPF paragraphs 9 and 17.
25. In terms of overlooking, the windows at first floor level of the side elevations would serve bathrooms and a condition is recommended requiring the one on the south-east elevation to be obscure glazed and fixed shut. Some overlooking to existing gardens in Sotherton Road could arise from the bedroom windows at the rear, however these would be oblique views. In addition there may be some overlooking possible from the bedrooms at the front towards the rear gardens of the properties in Parmenter Road, but these would be partly screened by the existing trees and the gardens are approximately 15 metres away which is a reasonable distance. On balance the level of overlooking would not cause material harm.
26. With regard to loss of parking, information provided by Norwich City Council indicates that in June 2016, 96% of the garages were occupied. The proposal would result in 14 of these being demolished. However in June 2016 there were 12 other garages within 800m walk of the application site which were available. It would therefore appear that alternative garage provision does exist in the locality. In addition two

disabled spaces would be provided to serve an identified need for existing residents and these would be managed by Norwich City Council.

27. In planning terms the harm caused by the loss of the parking facilities must be weighed against the significant benefit of providing two new affordable dwellings to address an identified housing need. In terms of the planning balance having regard to national and local planning policy, these benefits are considered to outweigh the loss of amenity, particularly given the alternative parking provision available.
28. There is no concern about material harm from overshadowing or an overbearing form of development given the scale of the proposal and the sufficient distance that would exist between the new and existing dwellings.
29. Concern has been raised that it may not be possible for vehicles to turn into the garages that are to be retained given that the garage doors open outwards and the turning area would be reduced by the proposal. The driveway immediately outside of the garages would be 6 metres wide so it should still be possible, however should a problem arise the Council could replace the doors with an 'up and over' style of door. This would not, therefore, be a reason to withhold planning permission.

Main issue 5: Trees and Landscaping

30. Key policies and NPPF paragraphs – DM3, DM8, NPPF paragraphs 9, 17 and 56.
31. There would be a mixture of hard paving and soft landscaping including new hedging to help enhance the current appearance of the site which is dominated by hardstanding and garages. Existing trees within neighbouring properties would be safeguarded through the development process. The proposal complies with relevant policies in this regard.
32. A resident has also raised concern that the development would obstruct access to their private garage at no. 5 Parmenter Road, in the respect that they can currently turn into the garage site and "sweep around" to drive into their garage. It is accepted that it would be necessary to turn and reverse into the garage, however it would then be possible to drive out in a forward gear as opposed to reversing out. This is considered acceptable and not a reason to withhold planning consent.

Other matters

33. The proposal is acceptable in terms of its impact on flood risk, biodiversity, land contamination and the energy efficiency measures proposed.

Equalities and diversity issues

34. There are no significant equality or diversity issues.

Local finance considerations

35. Under Section 70(2) of the Town and Country Planning Act 1990 the council is required when determining planning applications to have regard to any local finance considerations, so far as material to the application. Local finance considerations are defined as a government grant or the Community Infrastructure Levy.

36. Whether or not a local finance consideration is material to a particular decision will depend on whether it could help to make the development acceptable in planning terms. It would not be appropriate to make a decision on the potential for the development to raise money for a local authority.
37. In this case local finance considerations are not considered to be material to the case.

Conclusion

38. The proposal for development in Sotherton Road would provide two new affordable homes in a sustainable location and is considered to be acceptable in planning terms. There would be some harm caused to residential amenity in terms of the loss of the existing garage block. However there is alternative provision within the area and this loss must be balanced against the benefit of the proposal in terms of providing much needed affordable housing. This benefit is considered to outweigh the loss of the site for parking, particularly given the presence of alternative garage provision within the locality.
39. The proposal is in accordance with the requirements of the National Planning Policy Framework and the policies of the Development Plan, and there are no material considerations that indicate it should be determined otherwise.

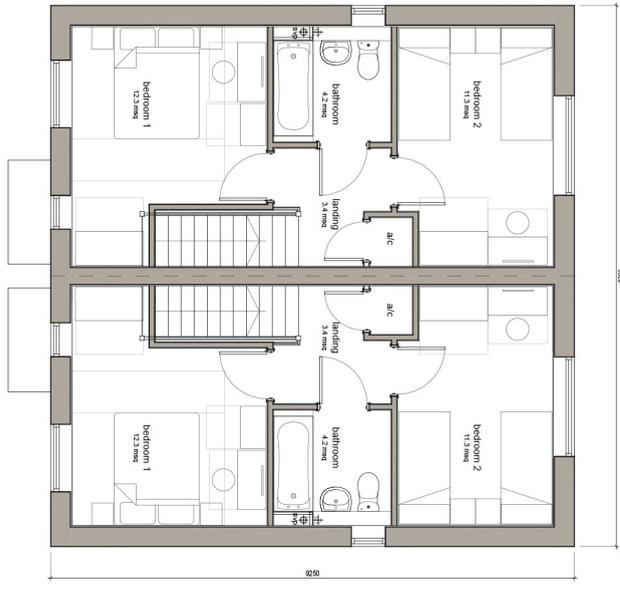
Recommendation

To approve application 16/01118/F and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Bathroom window on first floor south-eastern elevation to be obscure glazed and non-opening.
4. Details of facing and roofing materials; windows; joinery; boundary treatments, walls and fences; external lighting;
5. Details of hard and soft landscaping and planting
6. Water efficiency
7. Contamination risk assessment and report to be submitted
8. Unknown contamination to be addressed
9. Control on imported materials
10. Tree protection measures to be implemented

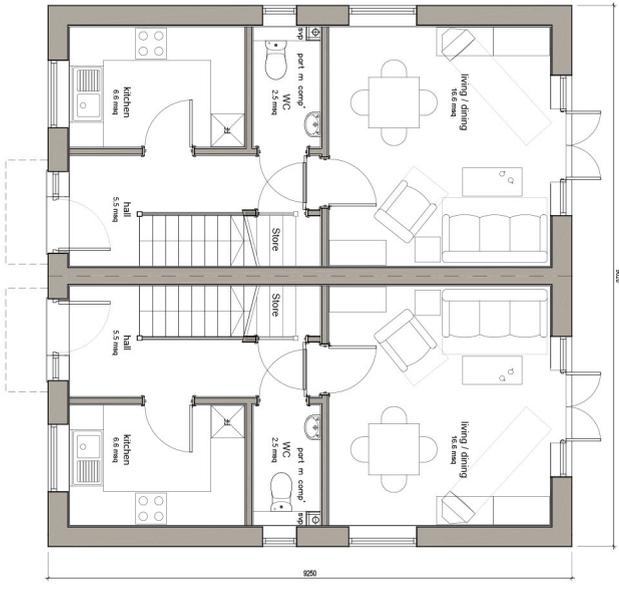
Article 35(2) statement

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined in the officer report.



Plot 1 First Floor Plan

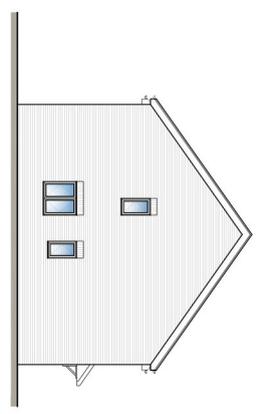
Plot 2 First Floor Plan



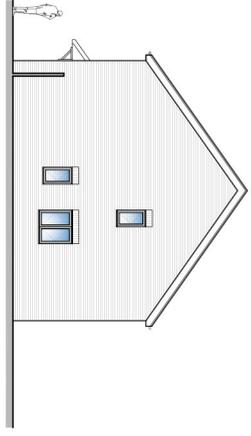
Plot 1 Ground Floor Plan

Plot 2 Ground Floor Plan

SCALE 1:50



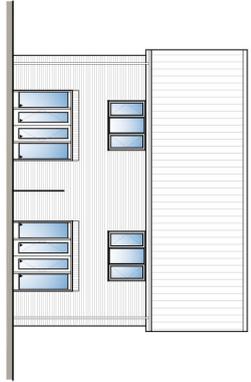
proposed north-west elevation 1:100



proposed south-east elevation 1:100



proposed south-west elevation 1:100



proposed north-east elevation 1:100

Roof Pitch: 35.0 deg
 Red/orange concrete panelling
 Fascia's, soffits & bargeboards uPVC colour: white
 Windows uPVC colour: white
 Entrance doors, gyo/venet colour: black
 Red Stock facing Brickswork
 Zno: 1173 x 2885mm P120/500 Veridian Clearline PV panels per unit

peterwellsarchitects

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Project: SOTHERTON ROAD, NORWICH, NR4 7DA

Client: ORWELL HOUSING

Draw Title: PROPOSED FLOOR PLANS AND ELEVS

Date: MAY 2016

Scale: 1:50/100 @ A1

Draw No.: P1775-PL04

Rev: B

Project: ORWELL HOUSING

Draw Title: PROPOSED FLOOR PLANS AND ELEVS

Date: MAY 2016

Scale: 1:50/100 @ A1

Draw No.: P1775-PL04

Rev: B

Report to Planning applications committee
 13 October 2016

Report of Head of planning services

Subject 16/01106/F - Land Used For Car Parking Adjacent To 69 Armes Street Norwich

Reason for referral Objections and application affecting City Council land.

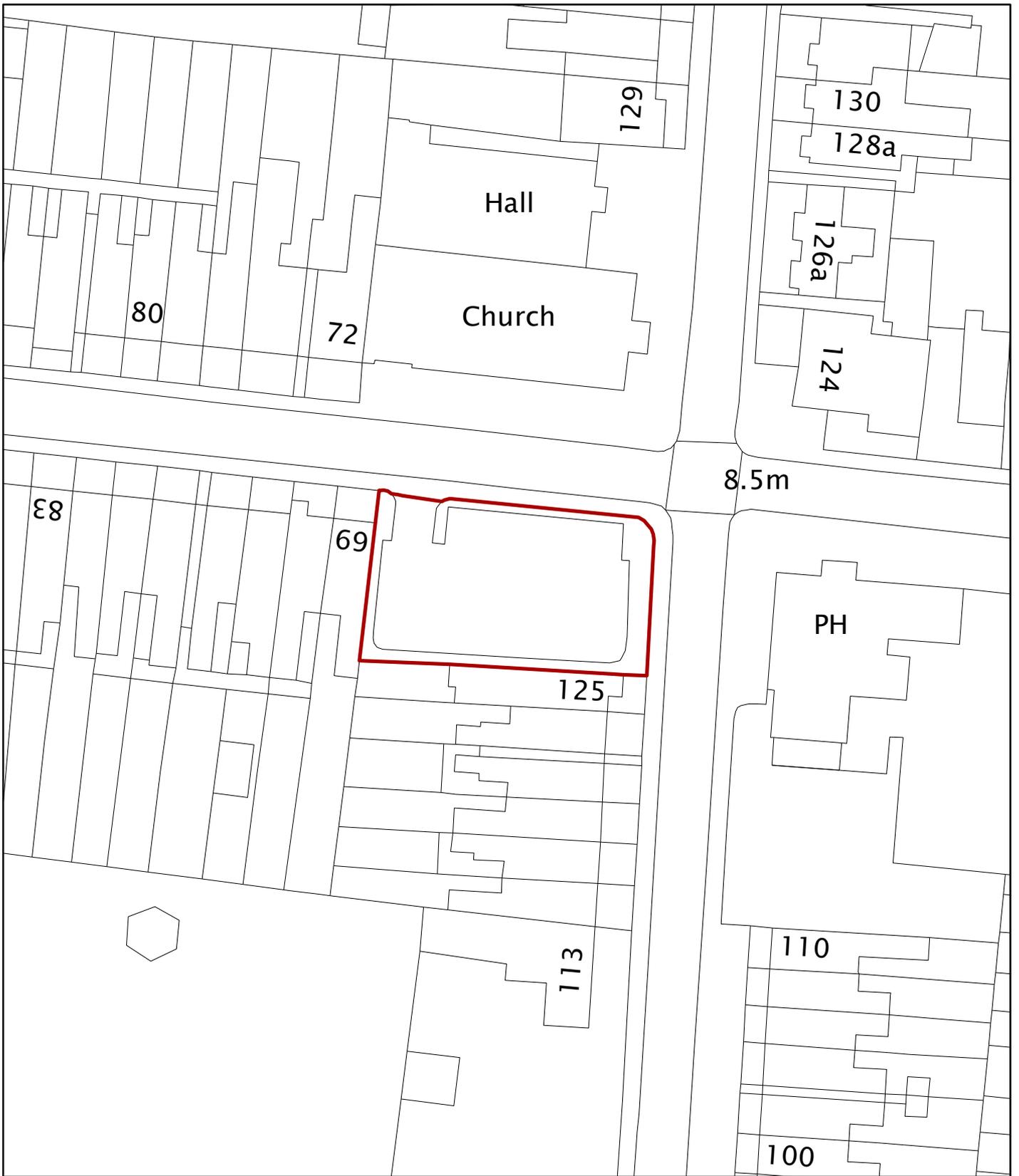
Item

4(f)

Ward:	Wensum
Case officer	Robert Webb - robertwebb@norwich.gov.uk

Development proposal		
Erection of 4 No. one bed flats with associated parking.		
Representations		
Object	Comment	Support
25+petition signed by 23	0	0

Main issues	Key considerations
1 Principle of development	Principle of redevelopment for housing
2 Design/ Heritage	Impact on character of the area, impact on the locally listed heritage asset, scale, form, massing and appearance.
3 Transport	Accessibility of site, impact on car parking, traffic, highway safety, cycle parking, servicing.
4 Amenity	Impact on neighbouring occupiers, loss of parking
Expiry date	20 October 2016
Recommendation	Approval subject to conditions.



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Planning Application No 16/01106/F

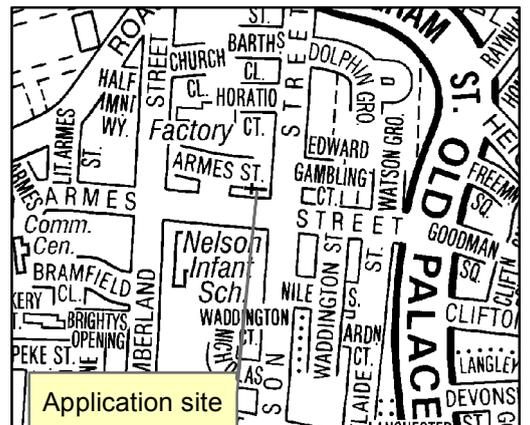
Site Address Car Park adjacent to 69 Armes Street

Scale 1:500



NORWICH
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PLANNING SERVICES



The site and surroundings

1. The site is currently a surface car park within a residential area. To the north is The Vineyard Community Centre (a locally listed building), to the east is the Nelson Public House, to the south and west there are terraced houses. The existing car park provides spaces for 15 cars and is owned and managed by Norwich City Council.

Constraints

2. There are mature trees at the front of the site either side of the vehicular access.

Relevant planning history

3. There is no relevant planning history held by the City Council.

The proposal

4. The proposal is one of a number of sites identified by Norwich City Council as having the potential to accommodate new affordable housing to be developed by a registered provider, Orwell Housing Association. The Council are seeking overall to deliver 66 affordable units across the city as part of the current programme, and these would be designed to meet Homes and Communities Agency (HCA) design and quality standards. The dwellings would be available at social or affordable rent whilst meeting high environmental standards. All homes would be advertised using the City Council's choice based letting scheme.
5. The proposal is to develop the site to provide 4 no. new affordable 2 bedroom flats within a two storey building with private amenity space for the ground floor flats, a parking space for each property, a cycle store and a bin store. Solar panels would be placed on the south and west facing roof. It should be noted that the proposals have been revised in order to provide an improved design which better addresses the corner location, as well as providing improved amenity for future occupiers and less prominent car parking and refuse storage areas.

Summary information

Proposal	Key facts
Scale	
Total no. of dwellings	4
No. of affordable dwellings	4
Total floorspace	186.45 square metres (46 sq m per dwelling)
No. of storeys	2
Density	103 dwellings per hectare

Appearance	
Materials	Walls – red stock brickwork. Roof – red/orange concrete pantiles. Windows – white Upvc double glazed units. Doors – GPR/timber front entrance doors.
Energy and resource efficiency measures	Solar pv panels, low energy lighting, gas condensing combination boiler with flue gas heat recovery system.
Transport matters	
Vehicular access	From Armes Street
No of car parking spaces	4 (1 per property)
No of cycle parking spaces	4
Servicing arrangements	Bin store provided

Representations

6. Advertised on site and in the press. Adjacent and neighbouring properties have been notified in writing. 21 letters of representation have been received citing the issues as summarised in the table below and a petition signed by 23 people has also been received. All representations are available to view in full at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Issues raised	Response
Loss of existing parking spaces for residents and users of the Vineyard Centre, concerns about increased parking pressure on the area, particularly during school drop-off and pick up times.	Key issue 4.
Impact on highway safety as a result of increased parking pressure.	Key issue 3.
Concerns about inadequate number of parking spaces for the new properties.	Key issue 2.
Concern about the cumulative impact of this application and the other proposals in Armes Street and Northumberland Street.	Key issue 4.

Consultation responses

7. Consultation responses are summarised below the full responses are available to view at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Ward Member – Councillor Sandra Bogelein

8. On behalf of residents of Northumberland Street, Armes Street and Nelson Street I would like to raise a number of concerns with regards to the applications on Northumberland Street and Armes Street.
9. First of all I would like to raise concerns with the fact that these developments will have to be appraised independently. The problems that will arise from the developments of car parking sites are clearly amplified if all three developments are approved, which are in very close proximity to each other. I would urge members to consider this cumulative negative impact in their decision making.
10. Residents' main concern is the loss of a well-used car parking site. Residents have been advised by the council that it is expected that the cars will be "absorbed" in the local area. This will pose a problem for residents especially during school drop off and pick up times. Residents feel it is very short sighted to develop on car parking space when in a lot of other areas on street and on pavement parking places a huge problem for residents, waste collection and emergency vehicles. This creates just another problem area. Please also note that the council has recently received an application to develop the land on 120-130 Northumberland Street, which will add over 30 additional dwellings to the area. Residents are worried that this development and the loss of the car parking space will create immense parking problems in the area.
11. As a ward councillor I am particularly worried about access issues for people with disabilities and prams that rely on a free pavement. At the moment Armes Street and Northumberland Street offer accessible ways to the city and bus stops mainly because of the additional parking. Alternative streets such as Nelson Street and West End Street are often inaccessible due to on pavement parking. I am very worried that taking away car parking spaces will reduce accessibility.
12. Please also note that in 2012 residents were consulted regarding the question whether these sites should be developed and there were very strong objections from residents which is why these sites were not brought forward at the time.

NCC Environmental Protection

13. I have viewed the desk study provided for this application and agree with the recommendation that further intrusive works are required. If approval is given, I suggest that conditions are applied.
14. The UXO risk may require careful consideration by a specialist due to this information, and also the proximity of other known WWII bomb drops.

Highways (local)

15. No objection on highway/transportation grounds. The development and site layout is acceptable, parking, refuse and cycle storage are acceptable.
16. With regard to the cumulative impact of developments in this area, it is worth noting that the redevelopment of council land for housing is a corporate priority. Residents do not have rights to park on this land, and may of course park on the highway. This part of the city centre is not subject to Controlled Parking Zone restrictions, parking is unrestricted and available to all on a first come first served basis.
17. In the future we could consider a CPZ and this would help to reduce commuter parking pressure, and to a more limited extent manage parking pressures from residents themselves, but there is no timescale for such work at present, and would need popular support to do so. It is important to remember that residents may need to find parking space on roads across the wider neighbourhood, not just outside or near to their homes.
18. For these reasons, I am content that the redevelopment of these car parking areas for homes is justified in policy terms.

NCC Housing

19. With regards to the impact of the loss of the car park on the surrounding area, car park surveys have been carried out on this site and the surrounding roads to assess usage. On average, the car park is 71% occupied, but there has been sufficient space on surrounding roads to accommodate the number of cars using it (also taking account of the development proposed on the site adjacent to no. 99 Armes Street). On 30/09/16 there were 12 garages available to rent within 400metres of the site.

Assessment of planning considerations

Relevant development plan policies

20. **Joint Core Strategy for Broadland, Norwich and South Norfolk adopted March 2011 amendments adopted Jan. 2014 (JCS)**
 - JCS2 Promoting good design
 - JCS3 Energy and water
 - JCS4 Housing delivery
 - JCS7 Supporting communities
 - JCS9 Strategy for growth in the Norwich policy area
21. **Norwich Development Management Policies Local Plan adopted Dec. 2014 (DM Plan)**
 - DM1 Achieving and delivering sustainable development
 - DM2 Ensuring satisfactory living and working conditions
 - DM3 Delivering high quality design
 - DM4 Providing for renewable and low carbon energy
 - DM5 Planning effectively for flood resilience
 - DM11 Protecting against environmental hazards
 - DM12 Ensuring well-planned housing development
 - DM28 Encouraging sustainable travel

- DM30 Access and highway safety
- DM31 Car parking and servicing

Other material considerations

22. Relevant sections of the National Planning Policy Framework March 2012 (NPPF):

- NPPF4 Promoting sustainable transport
- NPPF6 Delivering a wide choice of high quality homes
- NPPF7 Requiring good design
- NPPF10 Meeting the challenge of climate change, flooding and coastal change
- NPPF11 Conserving and enhancing the natural environment

Case Assessment

23. Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. Relevant development plan policies are detailed above. Material considerations include policies in the National Planning Framework (NPPF), the Councils standing duties, other policy documents and guidance detailed above and any other matters referred to specifically in the assessment below. The following sections provide an assessment of the main planning issues in this case in relation to the relevant policies and material considerations.

Main issue 1: Principle of development

24. Joint Core Strategy (JCS) Policy 4, supports housing delivery within the plan area, which this site falls. National policy, as set out in the Core Principles of the NPPF encourages new housing development to make the fullest possible use of public transport, walking and cycling, and focus significant development in locations which are or can be made sustainable. JCS policy 4 also encourages provision of affordable housing including of social rent and affordable rent tenure types as these are recognised and being particularly important in meeting housing need in the city.

25. Policy DM12 of the Norwich Development Management Policies Plan supports new residential development within the city boundary except in specific circumstances, none of which exceptions apply to this application site.

26. The NPPF encourages 'the effective use of land by reusing land that has been previously developed'. The site constitutes previously developed land and is in a sustainable location for new housing with good links to the City Centre. The proposed housing is therefore considered to be acceptable in principle and in this case would have the planning benefits of providing new affordable housing, subject to assessment against any other relevant policies or material considerations as outlined in the NPPF and the Development Plan. This is further set out below.

Main issue 2: Design

27. Key policies and NPPF paragraphs – JCS2, DM3, NPPF paragraphs 9, 17, 56 and 60-66.

28. The site is located in an area which is characterised by two storey residential development, but occupies a prominent position due to its corner location. The design has been revised following feedback from officers who considered that the scheme should better address the corner location of the site.
29. The revised plans show a development of a similar scale to the adjacent terraced properties, and would provide an active frontage to both Armes Street and Nelson Street. This is important given the corner nature of the site and the design and the design and scale is acceptable and in keeping with the character of the area.
30. The units would each have an internal floor area of 46 square metres and are intended as 1 bedroom 2 person houses. The floorspace is therefore below the national space standards figure of 51.5 square metres for this type of property. It is recognised however that if the dwellings were occupied by 1 person, then the minimum space standard of 40m² would be met. Notwithstanding this, whilst the failure to meet the minimum space standards based on two person occupancy is regrettable, it is not considered in itself to be a reason to warrant refusal of the application, given that the proposal is otherwise well-designed and would lead to the delivery of affordable housing in a sustainable location.
31. The ground floor units would have some private amenity space outside the front of their properties. It is not feasible to provide all of the units with private amenity space. No material harm would be caused by overlooking or overshadowing to neighbouring properties.

Main issue 3: Transport

32. Key policies and NPPF paragraphs – DM2, DM11, NPPF chapter 4.
33. The site is located in an accessible location within walking distance of Dereham Road and Heigham Street which has bus links into the city centre. The proposal would provide 1 parking space per property which is in accordance with the Council's parking standards. Secure cycle parking would be provided for each property. No objection is raised by the Highway Officer with regard to highway safety. The proposal therefore complies with the above policies. The issue of the loss of the existing car park is dealt with in the following section.

Main issue 4: Amenity

34. Key policies and NPPF paragraphs – DM2, DM11, NPPF paragraphs 9 and 17.
35. The main issue is the loss of the car park which is owned and managed by Norwich City Council and provides 15 parking spaces available for use by residents and visitors. A number of objections have been received to this aspect of the proposal, with concerns about increased parking pressure on local roads and the potential knock-on effects of this pressure. Concern has also been raised at the cumulative impact of the further proposals to develop another car park on Armes Street (application ref. 16/01109/F) and a car park on Northumberland Street (application ref. 16/01122/F).
36. Surveys conducted by the City Council in May and July this year indicate that the car park is well used at all times of the day and night. Information and photographs

have been provided by local residents which also indicate this to be the case. The fact that the car park is often busy or full is not disputed.

37. The Highway Officer has stated that parking is not restricted on the surrounding road network, and therefore parking is available on a first-come, first-served basis. It is acknowledged that some inconvenience may occur in that residents may have to park further afield from their property.
38. It is acknowledged that some harm would be caused to residential amenity by the loss of the existing spaces, both as result of this proposal and cumulatively with the other proposals nearby on Armes Street and Northumberland Street. However this must be weighed against the significant benefits of delivering affordable housing in a sustainable location within the city.
39. This will be a matter for Members to judge, but the advice from Officers is that in planning policy terms addressing housing need is considered to be of greater importance than providing off-road parking spaces, particularly in a location which has good links to public transport and the city centre and where there is the opportunity to use other modes of transport such as buses and cycles to travel. It is therefore recommended that the application should not be refused on the grounds of loss of parking.
40. The proposal would not cause material harm in terms of overshadowing or loss of privacy to the adjacent properties.

Other Matters

41. The proposal is acceptable in terms of its impact on flood risk, biodiversity, land contamination and the energy efficiency measures proposed.

Equalities and diversity issues

42. There are no significant equality or diversity issues.

Local finance considerations

43. Under Section 70(2) of the Town and Country Planning Act 1990 the council is required when determining planning applications to have regard to any local finance considerations, so far as material to the application. Local finance considerations are defined as a government grant or the Community Infrastructure Levy.
44. Whether or not a local finance consideration is material to a particular decision will depend on whether it could help to make the development acceptable in planning terms. It would not be appropriate to make a decision on the potential for the development to raise money for a local authority.
45. In this case local finance considerations are not considered to be material to the case.

Conclusion

46. The proposal for the development of the car park adjacent to no. 69 Armes Street would provide four new affordable homes in a sustainable location and is considered to be acceptable in planning terms. There would be some harm caused in terms of

the loss of the existing car park, and clearly this is a very contentious matter for existing residents. Regard has been had to the cumulative impact of this development and other proposals in close proximity in Armes Street and Northumberland Street, currently under consideration by the Local Planning Authority. However these impacts must be balanced against the benefits of the proposal in terms of providing much needed affordable housing and this benefit is considered to outweigh the loss of the site for parking.

47. The development is in accordance with the requirements of the National Planning Policy Framework and the policies of the Development Plan, and there are no material considerations that indicate it should be determined otherwise.

Recommendation

To approve application 16/01109/F and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Details of facing and roofing materials; windows; joinery; boundary treatments, walls and fences; external lighting;
4. Details of hard and soft landscaping and planting
5. Water efficiency
6. Contamination risk assessment and report to be submitted
7. Unknown contamination to be addressed
8. Control on imported materials

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined in the officer report.

Report to Planning applications committee
 13 October 2016

Report of Head of planning services

Subject 16/01109/F - Land Used For Car Parking Adjacent To 99
 Armes Street Norwich

Reason for referral Objections and application affecting City Council land.

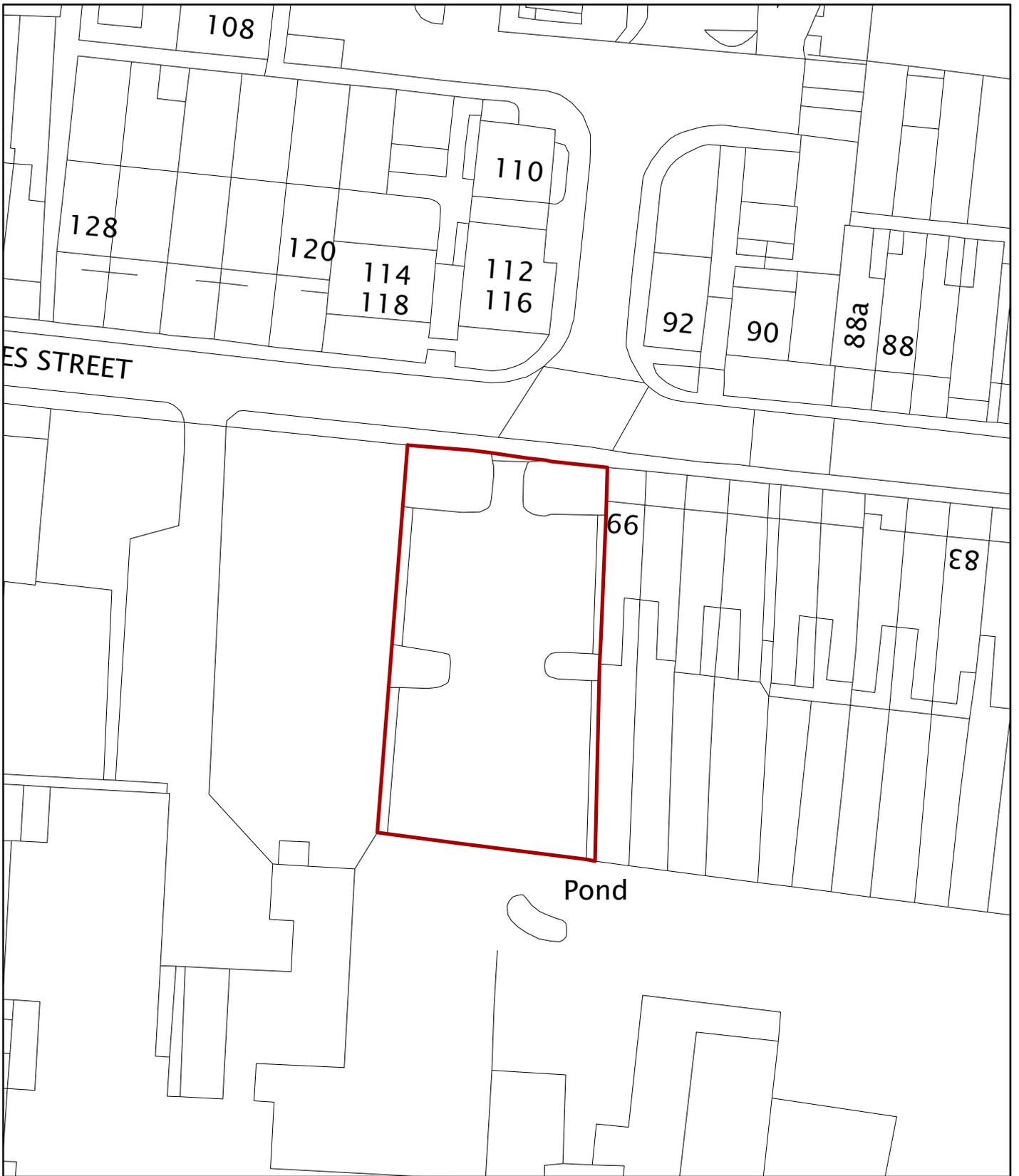
Item

4(g)

Ward:	Wensum
Case officer	Robert Webb - robertwebb@norwich.gov.uk

Development proposal		
3 No. two bed dwellinghouses and associated car parking.		
Representations		
Object	Comment	Support
21	0	0

Main issues	Key considerations
1 Principle of development	Principle of redevelopment for housing
2 Design	Impact on character of the area, scale, form, massing and appearance.
3 Transport	Accessibility of site, impact on car parking, traffic, highway safety, cycle parking, servicing.
4 Amenity	Impact on neighbouring occupiers, loss of parking
5 Trees	Consideration of impact on trees
Expiry date	20 October 2016
Recommendation	Approval subject to conditions.



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Planning Application No 16/01106/F

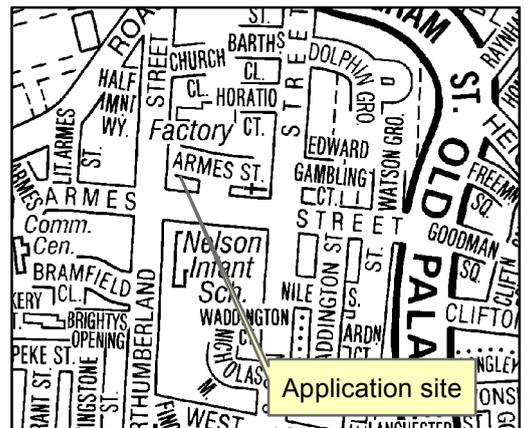
Site Address Land used for car parking adjacent to 99 Armes Street

Scale 1:500



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Application site

The site and surroundings

1. The site is currently a surface car park within a residential area. To the north is Armes Street and residential properties. To the east is a row of terraced houses and their gardens. To the south and west is Nelson Infant School. There are mature trees at the front of the site either side of the vehicular access.

Constraints

2. There are mature trees at the front of the site either side of the vehicular access.

Relevant planning history

3. There is no relevant planning history held by the City Council.

The proposal

4. The proposal is one of a number of sites identified by Norwich City Council as having the potential to accommodate new affordable housing to be developed by a registered provider, Orwell Housing Association. The Council are seeking overall to deliver 66 affordable units across the city as part of the current programme, and these would be designed to meet Homes and Communities Agency (HCA) design and quality standards. The dwellings would be available at social or affordable rent whilst meeting high environmental standards. All homes would be advertised using the City Council's choice based letting scheme.
5. The proposal is to develop the site to provide 3 no. new affordable 2 bedroom houses. They would take the form of a row of three terraced houses, set back from the street, with parking at the front and private gardens to the rear. Each property would have one dedicated parking space with a further visitor space serving all three dwellings. Solar panels would be placed on the rear face of the sloping roof.

Summary information

Proposal	Key facts
Scale	
Total no. of dwellings	3
No. of affordable dwellings	3
Total floorspace	217.2 square metres (72.4 sq m per dwelling)
No. of storeys	2
Density	43 dwellings per hectare
Appearance	
Materials	Walls – red stock brickwork. Roof – red/orange concrete pantiles. Windows – white Upvc double glazed units. Doors –

	GPR/timber front entrance doors.
Energy and resource efficiency measures	Solar pv panels, low energy lighting, gas condensing combination boiler with flue gas heat recovery system.
Transport matters	
Vehicular access	From Armes Street
No of car parking spaces	4 (1 per property plus 1 visitor space).
No of cycle parking spaces	Cycle shed provided for each property
Servicing arrangements	Bin storage to rear of properties, bin presentation area at front of site next to Armes Street.

Representations

6. Advertised on site and in the press. Adjacent and neighbouring properties have been notified in writing. 21 letters of representation have been received citing the issues as summarised in the table below. All representations are available to view in full at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Issues raised	Response
Loss of existing parking spaces for residents and concerns about increased parking pressure on the area, particularly during school drop-off and pick up times.	Key issue 4.
Impact on highway safety resulting from increased parking pressure.	Key issue 3.
Concerns about inadequate number of parking spaces for the new properties.	Key issue 3.
Concern about the cumulative impact of this application and the other proposals in Armes Street and Northumberland Street.	Key issue 4.

Consultation responses

7. Consultation responses are summarised below the full responses are available to view at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Ward Member – Councillor Sandra Bogelein

8. On behalf of residents of Northumberland Street, Armes Street and Nelson Street I would like to raise a number of concerns with regards to the applications on Northumberland Street and Armes Street.
9. First of all I would like to raise concerns with the fact that these developments will have to be appraised independently. The problems that will arise from the developments of car parking sites are clearly amplified if all three developments are approved, which are in very close proximity to each other. I would urge members to consider this cumulative negative impact in their decision making.
10. Residents' main concern is the loss of a well-used car parking site. Residents have been advised by the council that it is expected that the cars will be "absorbed" in the local area. This will pose a problem for residents especially during school drop off and pick up times. Residents feel it is very short sighted to develop on car parking space when in a lot of other areas on street and on pavement parking places a huge problem for residents, waste collection and emergency vehicles. This creates just another problem area. Please also note that the council has recently received an application to develop the land on 120-130 Northumberland Street, which will add over 30 additional dwellings to the area. Residents are worried that this development and the loss of the car parking space will create immense parking problems in the area.
11. As a ward councillor I am particularly worried about access issues for people with disabilities and prams that rely on a free pavement. At the moment Armes Street and Northumberland Street offer accessible ways to the city and bus stops mainly because of the additional parking. Alternative streets such as Nelson Street and West End Street are often inaccessible due to on pavement parking. I am very worried that taking away car parking spaces will reduce accessibility.
12. Please also note that in 2012 residents were consulted regarding the question whether these sites should be developed and there were very strong objections from residents which is why these sites were not brought forward at the time.

NCC Environmental Protection

13. I have viewed the desk study provided for this application and agree with the recommendation that further intrusive works are required. If approval is given, I suggest that the following conditions are applied.
14. Despite the report stating that no bomb drops are recorded on site, our GIS information shows that there was one in April 1942. The UXO risk may require careful consideration by a specialist due to this information, and also the proximity of other known WWII bomb drops.

Highways (local)

15. No objection on highway/transportation grounds. The development and site layout is acceptable, parking, refuse and cycle storage are acceptable.
16. With regard to the cumulative impact of developments in this area, it is worth noting that the redevelopment of council land for housing is a corporate priority. Residents do not have rights to park on this land, and may of course park on the highway. This

part of the city centre is not subject to Controlled Parking Zone restrictions, parking is unrestricted and available to all on a first come first served basis.

17. In the future the Council could consider a Controlled Parking Zone (CPZ) and this would help to reduce commuter parking pressure, and to a more limited extent manage parking pressures from residents themselves, but there is no timescale for such work at present, and would need popular support to do so. It is important to remember that residents may need to find parking space on roads across the wider neighbourhood, not just outside or near to their homes.
18. For these reasons, I am content that the redevelopment of these car parking areas for homes is justified in policy terms.

Tree protection officer

19. No response received.

NCC Housing

20. Car park surveys have been carried out on this site and the surrounding roads to assess usage. On average, the car park is 47% occupied, but there has been sufficient space on surrounding roads to accommodate the number of cars using it (also taking account of the development proposed on the car park adjacent to no. 69 Armes Street). On 30/09/16 there were 12 garages available to rent within 400 metres of the site.

Assessment of planning considerations

Relevant development plan policies

21. **Joint Core Strategy for Broadland, Norwich and South Norfolk adopted March 2011 amendments adopted Jan. 2014 (JCS)**
- JCS2 Promoting good design
 - JCS3 Energy and water
 - JCS4 Housing delivery
 - JCS7 Supporting communities
 - JCS9 Strategy for growth in the Norwich policy area
22. **Norwich Development Management Policies Local Plan adopted Dec. 2014 (DM Plan)**
- DM1 Achieving and delivering sustainable development
 - DM2 Ensuring satisfactory living and working conditions
 - DM3 Delivering high quality design
 - DM4 Providing for renewable and low carbon energy
 - DM5 Planning effectively for flood resilience
 - DM11 Protecting against environmental hazards
 - DM12 Ensuring well-planned housing development
 - DM28 Encouraging sustainable travel
 - DM30 Access and highway safety
 - DM31 Car parking and servicing

Other material considerations

23. Relevant sections of the National Planning Policy Framework March 2012 (NPPF):

- NPPF4 Promoting sustainable transport
- NPPF6 Delivering a wide choice of high quality homes
- NPPF7 Requiring good design
- NPPF10 Meeting the challenge of climate change, flooding and coastal change
- NPPF11 Conserving and enhancing the natural environment

Case Assessment

24. Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. Relevant development plan policies are detailed above. Material considerations include policies in the National Planning Framework (NPPF), the Councils standing duties, other policy documents and guidance detailed above and any other matters referred to specifically in the assessment below. The following sections provide an assessment of the main planning issues in this case in relation to the relevant policies and material considerations.

Main issue 1: Principle of development

25. Joint Core Strategy (JCS) Policy 4, supports housing delivery within the plan area, which this site falls. National policy, as set out in the Core Principles of the NPPF encourages new housing development to make the fullest possible use of public transport, walking and cycling, and focus significant development in locations which are or can be made sustainable. JCS policy 4 also encourages provision of affordable housing including of social rent and affordable rent tenure types as these are recognised and being particularly important in meeting housing need in the city.

26. Policy DM12 of the Norwich Development Management Policies Plan supports new residential development within the city boundary except in specific circumstances, none of the exceptions apply to this application site.

27. The NPPF encourages 'the effective use of land by reusing land that has been previously developed'. This site constitutes previously developed land. The site is in a sustainable location for new housing with good links to the City Centre. The proposed housing is therefore considered to be acceptable in principle and in this case would have the planning benefits of providing new affordable housing, subject to assessment against any other relevant policies or material considerations as outlined in the NPPF and the Development Plan. This is further set out below.

Main issue 2: Design

28. Key policies and NPPF paragraphs – JCS2, DM3, NPPF paragraphs 9, 17, 56 and 60-66.

29. The site is located in an area which is characterised by two storey residential development, some of which dating from the Victorian period and some being more

modern. The more open grounds of the adjacent school also provide part of the setting for the site.

30. The proposed dwellings would be set back from the properties immediately to the east, to ensure the retention of some mature lime trees at the front of the site. This layout would also allow space for parking to the front of the properties and is considered acceptable. Each property would have a private garden to the rear with a shed to store cycles.
31. The design would be fairly conventional for a modern two-storey terrace, with red brick, red roof tiles and a pitched roof. Solar pv panels would be positioned on the southern (rear) facing part of the roof.
32. The dwellings proposed would have an internal floor area of 72.4 square metres and the dwellings are intended as 2 bedroom 4 person houses. The floorspace is therefore below the national space standards figure of 81 square metres for this type of property. It is recognised however that if the dwellings were occupied by 3 persons, then the minimum space standard of 72m² would be met. Notwithstanding this, whilst the failure to meet the minimum space standards based on four person occupancy is regrettable, it is not considered in itself to warrant refusal of the application, given that the development is otherwise well-designed and would lead to the delivery of affordable housing in a sustainable location. The design, layout and materials proposed are considered to be acceptable.

Main issue 3: Transport

33. Key policies and NPPF paragraphs – DM2, DM11, NPPF chapter 4.
34. The site is located in an accessible location within walking distance of Dereham Road and Heigham Street which has bus links into the city centre. The proposal would provide 1 parking space per property and an additional visitor space which is in accordance with the Council's parking standards. Secure cycle parking would be provided for each property. No objection is raised by the Highway Officer with regard to highway safety. The proposal therefore complies with the above policies. The issue of the loss of the existing car park is dealt with in the following section.

Main issue 4: Amenity

35. Key policies and NPPF paragraphs – DM2, DM11, NPPF paragraphs 9 and 17.
36. The main issue is the loss of the car park which is owned and managed by Norwich City Council and provides 20 parking spaces available for use by residents and visitors. A number of objections have been received to this aspect of the proposal, with concerns about increased parking pressure on local roads and the potential knock-on effects of this pressure. Concern has also been raised at the cumulative impact of the further proposals to develop another car park on Armes Street (application ref. 16/01106/F) and a car park on Northumberland Street (application ref. 16/01122/F).
37. Surveys conducted by the City Council in May and July this year indicate that the car park is generally not well used during the day, with occupancy rates varying from 30-35% on weekdays. Parking rates increase during the evening and at night however, when they rise to 60% and 70% of the spaces being occupied.

Information and photographs provided by residents also suggests that the car parks are well used during the evenings.

38. The Highway Officer has stated that parking is not restricted on the surrounding road network, and therefore parking is available on a first-come, first-served basis. It is acknowledged that some inconvenience would may occur in that residents may have to park further afield from their property.
39. It is acknowledged that some harm would be caused to residential amenity by the loss of the existing spaces, both as a result of this proposal and cumulatively with the other proposals nearby on Armes Street and Northumberland Street. However this must be weighed against the significant benefits of delivering affordable housing in a sustainable location within the city.
40. This will be a matter for Members to judge, but the advice from Officers is that in planning policy terms addressing housing need is considered to be of greater importance than providing off-road parking spaces, particularly in a location which has good links to public transport and the city centre and where there is the opportunity to use other modes of transport such as buses and cycles to travel. It is therefore recommended that the application should not be refused on the grounds of loss of parking.
41. The proposal would not cause material harm in terms of overshadowing or loss of privacy to the adjacent properties.

Main issue 5: Trees and Landscaping

1. Key policies and NPPF paragraphs – DM3, DM7, DM8, NPPF paragraphs 9, 17, 56, 109 and 118.
2. A positive feature of the existing site is the presence of two lime trees at the entrance to the car park. The proposal would retain these trees, therefore safeguarding a valuable landscape feature of the street scene. There would be small landscaped areas at the front of each property. The proposal complies with relevant policies in relation to trees and landscaping.

Other matters

3. The proposal is acceptable in terms of its impact on flood risk, biodiversity, land contamination and the energy efficiency measures proposed.

Conclusion

4. The proposal for development of the car park adjacent to no. 99 Armes Street would provide three new affordable homes in a sustainable location and is considered to be acceptable in planning terms. There would be some harm caused in terms of the loss of the existing car park, and clearly this is a very contentious matter for existing residents. Regard has also been had to the cumulative impact of other nearby proposals affecting car parks elsewhere in Armes Street and Northumberland Street and currently being considered by the Local Planning Authority. However these impacts must be balanced against the benefits of the proposal in terms of providing much needed affordable housing and this benefit is considered to outweigh the loss of the sites for parking.

5. The development is in accordance with the requirements of the National Planning Policy Framework and the policies of the Development Plan, and there are no material considerations that indicate it should be determined otherwise.

Recommendation

To approve application 16/01109/F and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Details of facing and roofing materials; windows; joinery; boundary treatments, walls and fences; external lighting;
4. Details of hard and soft landscaping and planting
5. Water efficiency
6. Contamination risk assessment and report to be submitted
7. Unknown contamination to be addressed
8. Control on imported materials
9. Tree protection measures to be implemented

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined in the officer report.

Report to Planning applications committee
 13 October 2016

Report of Head of planning services

Subject 16/01121/F- Garages adjacent to 110 Quebec Road

Reason for referral Objections and application affecting City Council land.

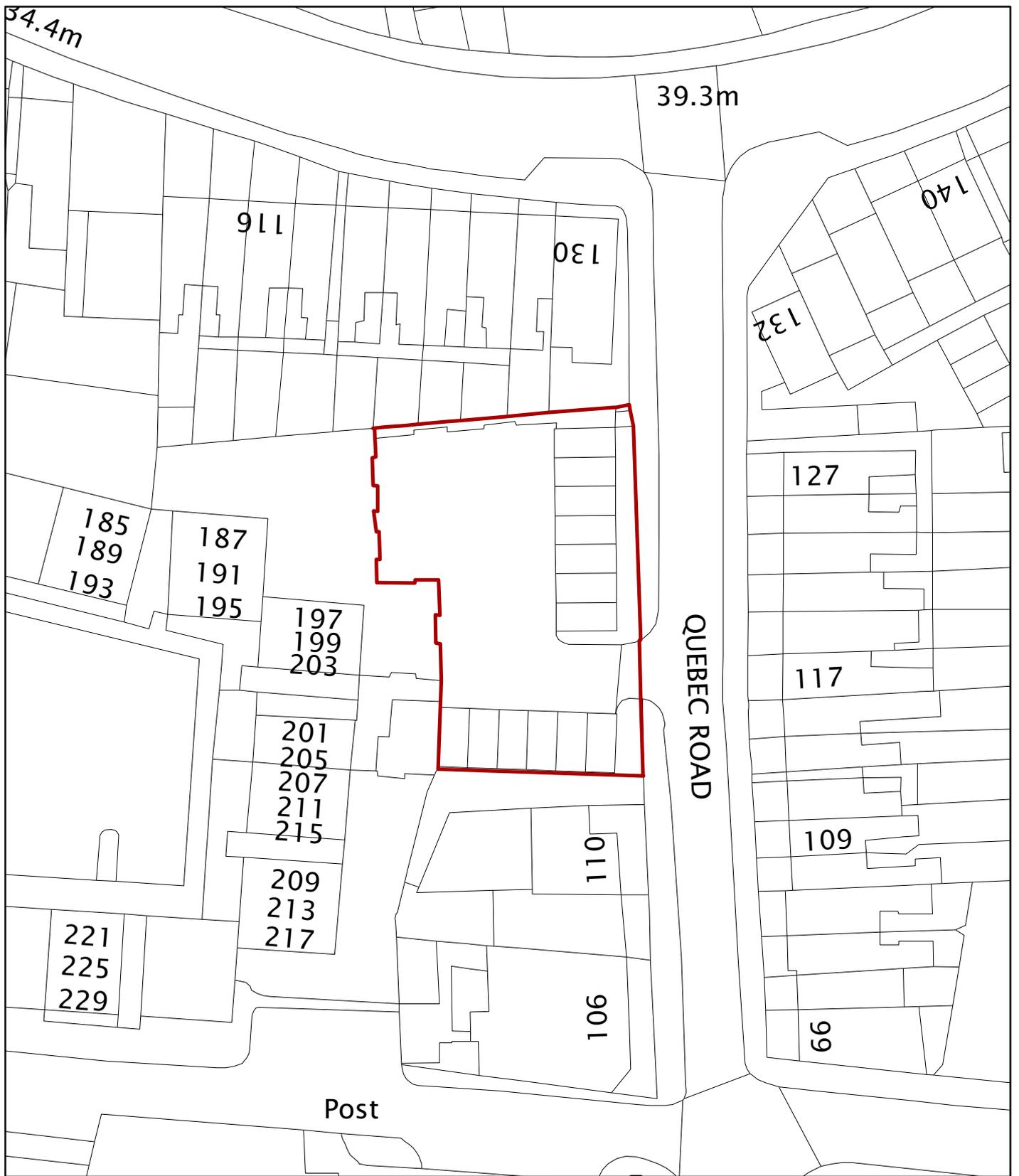
Item

4(h)

Ward:	Thorpe Hamlet
Case officer	Robert Webb - robertwebb@norwich.gov.uk

Development proposal		
Demolition of existing garages and the erection of 1 No. four bed dwelling and 2 No. two bed dwellings.		
Representations		
Object	Comment	Support
6	1	0

Main issues	Key considerations
1 Principle of development	Principle of redevelopment for housing
2 Design/ Heritage	Impact on character of the adjacent conservation area, scale, form, massing and appearance.
3 Trees and Landscaping	Consideration of landscaping, impact on trees
4 Transport	Accessibility of site, impact on car parking, traffic, highway safety, cycle parking, servicing.
5 Amenity	Impact on neighbouring occupiers, loss of parking
Expiry date	20 October 2016
Recommendation	Approval subject to conditions.



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Planning Application No 16/01121/F

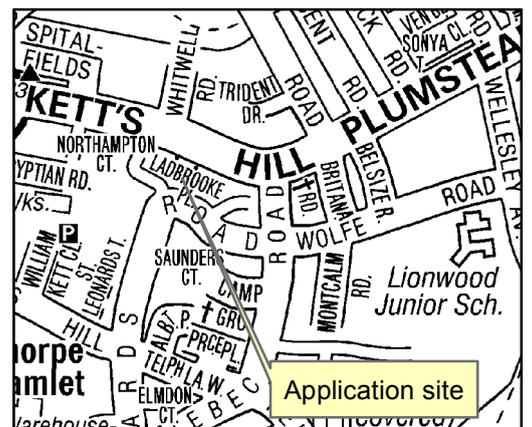
Site Address Garages adjacent to 110 Quebec Road

Scale 1:500



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The site and surroundings

1. The site is owned and managed by Norwich City Council and consists of two garage blocks providing a total of 13 garages and is also used as a communal bin store area for the flats within St. Leonard's Road. These flats are directly to the west of the site, with terraced housing to the north, south and east of the site.

Constraints

2. The site is adjacent to, but outside of, the Thorpe Hamlet Conservation Area.

Relevant planning history

3. There is no relevant planning history held by the City Council.

The proposal

4. The proposal is one of a number of sites identified by Norwich City Council as having the potential to accommodate new affordable housing to be developed by a registered provider, Orwell Housing Association. The Council are seeking overall to deliver 66 affordable units across the city as part of the current programme, and these would be designed to meet Homes and Communities Agency (HCA) design and quality standards. The dwellings would be available at social or affordable rent whilst meeting high environmental standards. All homes would be advertised using the City Council's choice based letting scheme.
5. The proposal is to develop the site to provide 3 no. new affordable houses, with 1 no. 4 bed dwelling and 2 no. 2 bed dwellings. They would take the form of a row of three terraced houses, fronting onto Quebec Road, with off road parking provided to the side. Each property would have dedicated parking space, 1 space for each of the 2 bed dwellings and 2 spaces for the 3 bed dwelling. Solar panels would be placed on the rear face of the sloping roof.

Summary information

Proposal	Key facts
Scale	
Total no. of dwellings	3
No. of affordable dwellings	3
Total floorspace	250.1 square metres (72.4 m ² per 2 bed dwelling, 105.3 m ² for the 4 bed dwelling)
No. of storeys	2
Density	45 dwellings per hectare
Appearance	

Materials	Walls – red stock brickwork. Roof – red/orange concrete pantiles. Windows – white Upvc double glazed units. Doors – GPR/timber front entrance doors.
Energy and resource efficiency measures	Solar pv panels, low energy lighting, gas condensing combination boiler with flue gas heat recovery system.
Transport matters	
Vehicular access	From Armes Street
No of car parking spaces	4 (1 per 2 bed property, 2 for the 4 bed property).
No of cycle parking spaces	Cycle shed provided for each property.
Servicing arrangements	Bin storage to rear of properties, bin presentation area at front of site next to Armes Street.

Representations

6. Advertised on site and in the press. Adjacent and neighbouring properties have been notified in writing. 7 letters of representation have been received citing the issues as summarised in the table below. All representations are available to view in full at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Issues raised	Response
Concern at loss of light to property	See main issue 4
Concern at loss of parking spaces	See main issue 4
Concern at loss of privacy	See main issue 4
Concern regarding highway safety	See main issue 3
Impact on trees	See main issue 5
Concern at the loss of the barrier/wall to 110 Quebec Road	See main issue 4
Concern that the site is currently used for communal refuse	See paragraph 32

Consultation responses

7. Consultation responses are summarised below the full responses are available to view at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Environmental protection

8. I have viewed the desk study provided for this application and agree with the recommendation that further intrusive works are required. If approval is given, I suggest that conditions are applied to secure this.

Highways (local)

9. No objection on highway/transportation grounds. The development and site layout is acceptable, parking, refuse and cycle storage are acceptable.

NCC Housing

10. There are eleven garage tenants that would be served with a Notice to Quit if the plans are approved. Garage vacancies were checked on 30/09/16. There are 8 garages available within 200 metres of the site and an additional 4 within 350 metres. Car park surveys have also been carried out on this site and the surrounding roads and car parks to assess usage. On average, the car park is 80% occupied, but there has been sufficient space on surrounding roads and car parks to accommodate the number of cars using it. On many of the counts, it has been noted that all the cars parked within the site did not have permits, so were using the parking spaces for free rather than using on street parking spaces which are permit controlled.

Assessment of planning considerations

Relevant development plan policies

11. **Joint Core Strategy for Broadland, Norwich and South Norfolk adopted March 2011 amendments adopted Jan. 2014 (JCS)**
 - JCS2 Promoting good design
 - JCS3 Energy and water
 - JCS4 Housing delivery
 - JCS7 Supporting communities
 - JCS9 Strategy for growth in the Norwich policy area
12. **Norwich Development Management Policies Local Plan adopted Dec. 2014 (DM Plan)**
 - DM1 Achieving and delivering sustainable development
 - DM2 Ensuring satisfactory living and working conditions
 - DM3 Delivering high quality design
 - DM4 Providing for renewable and low carbon energy
 - DM5 Planning effectively for flood resilience
 - DM11 Protecting against environmental hazards
 - DM12 Ensuring well-planned housing development
 - DM28 Encouraging sustainable travel
 - DM30 Access and highway safety

- DM31 Car parking and servicing

Other material considerations

13. Relevant sections of the National Planning Policy Framework March 2012 (NPPF):

- NPPF4 Promoting sustainable transport
- NPPF6 Delivering a wide choice of high quality homes
- NPPF7 Requiring good design
- NPPF10 Meeting the challenge of climate change, flooding and coastal change
- NPPF11 Conserving and enhancing the natural environment

Case Assessment

14. Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. Relevant development plan policies are detailed above. Material considerations include policies in the National Planning Framework (NPPF), the Councils standing duties, other policy documents and guidance detailed above and any other matters referred to specifically in the assessment below. The following sections provide an assessment of the main planning issues in this case in relation to the relevant policies and material considerations.

Main issue 1: Principle of development

15. Joint Core Strategy (JCS) Policy 4, supports housing delivery within the plan area, which this site falls. National policy, as set out in the Core Principles of the NPPF encourages new housing development to make the fullest possible use of public transport, walking and cycling, and focus significant development in locations which are or can be made sustainable. JCS policy 4 also encourages provision of affordable housing including of social rent and affordable rent tenure types as these are recognised and being particularly important in meeting housing need in the city.
16. Policy DM12 of the Norwich Development Management Policies Plan supports new residential development within the city boundary except in specific circumstances, none of the exceptions apply to this application site.
17. The NPPF encourages 'the effective use of land by reusing land that has been previously developed'. This site constitutes previously developed land. The site is in a sustainable location for new housing with good links to the City Centre. The proposed housing is therefore considered to be acceptable in principle and in this case would have the planning benefits of providing new affordable housing, subject to assessment against any other relevant policies or material considerations as outlined in the NPPF and the Development Plan. This is further set out below.

Main issue 2: Design

18. Key policies and NPPF paragraphs – JCS2, DM3, NPPF paragraphs 9, 17, 56 and 60-66.

19. The site is located in a residential area which is mainly characterised by Victorian terraced properties although there is also a development of three storey flats directly to the east.
20. The design proposes a modern two-storey terrace, with red brick, red roof tiles and a pitched roof. Solar pv panels would be positioned on the southern (rear) facing part of the roof. The form of development would be in keeping with the existing pattern of development on Quebec Road and the design detailing respects the position of the site close to the Conservation Area.
21. The two storey dwellings proposed would have an internal floor area of 72.4 square metres and the dwellings are intended as 2 bedroom 4 person houses. The floorspace is therefore below the national space standards figure of 81 square metres for this type of property. It is recognised however that if the dwellings were occupied by 3 persons, then the minimum space standard of 72m² would be met. Similarly the four bed house shows an internal floor area of 105.3m², which would be close to the minimum standard for 5 people, but somewhat short of the minimum area of 115m² recommended for 6 people. Whilst the failure to meet the minimum space standards is regrettable, it is not considered in itself to warrant refusal of the application given that the proposal is otherwise well-designed and would lead to the delivery of affordable housing in a sustainable location.

Main issue 3: Transport

22. Key policies and NPPF paragraphs - JCS6, DM28, DM31.
23. The site is located in an accessible location within walking distance of the city centre and Ketts Hill which has bus links into the city centre. The proposal would provide 4 parking spaces which meets the Council's parking standards. Secure cycle parking would be provided for each property. No objection is raised by the Highway Officer with regard to highway safety. The proposal therefore complies with the above policies. The issue of the loss of the existing car park is dealt with in the following section.

Main issue 4: Amenity

24. Key policies and NPPF paragraphs – DM2, DM11, NPPF paragraphs 9 and 17.
25. A number of objections have been received to the proposal, with concerns about increased parking pressure on local roads and the potential knock-on effects of this pressure.
26. Information provided with the application indicates that in June this year 85% of the garages were occupied. This amounts to 11 of the 13 garages. At that time there were 10 available garages within 800m of the site, which indicates that alternative provision is available in the locality. There is also a small parking area which appears to be well used, particularly during the evenings and weekends.
27. It is acknowledged that some harm would be caused to residential amenity by the loss of the existing spaces. However this must be weighed against the significant benefits of delivering affordable housing in a sustainable location within the city. This will be a matter for Members to judge, but the advice from Officers is that in planning policy terms addressing housing need is considered to be of greater

importance than providing off-road parking spaces, particularly in a location which has good links to public transport and the city centre and where there is the opportunity to use other modes of transport such as buses and cycles to travel. It is therefore recommended that the application should not be refused on the grounds of loss of parking.

28. Whilst some objections have been raised regarding the impact on properties in Ketts Hill, a sunpath analysis has been submitted which demonstrates that the proposal would not cause material harm in terms of overshadowing. The proposals would also not result in significant loss of daylight or sunlight to these properties given the level of separation between the existing and proposed properties. In addition the proposal has been designed to ensure no material harm from overlooking would occur. The rear wall of the garage block to the southern boundary with no. 110 Quebec Road would be retained in order to maintain privacy.

Main issue 5: Trees and Landscaping

29. Key policies and NPPF paragraphs – DM3, DM7, DM8, NPPF paragraphs 9, 17, 56, 109 and 118.
30. Two category C silver birch trees would be removed to facilitate the development. Given that the trees are of a low quality this is considered acceptable and the remaining trees on site would be retained. Replacement tree planting should be sought by condition and a further condition is sought requiring the approval of a suitable landscaping scheme. Subject to these conditions the proposal complies with relevant policies in relation to trees and landscaping

Other Matters

31. The proposal is acceptable in terms of its impact on flood risk, biodiversity, land contamination and the energy efficiency measures proposed.
32. During the course of dealing with the application it has become apparent that there is a need to provide a replacement bin store within the site to serve the nearby flats on St. Leonard's Road. At the time of writing this matter was being addressed by the applicant and it is anticipated that an amended site plan will be submitted which will be available as an update to the agenda report.

Equalities and diversity issues

33. There are no significant equality or diversity issues.

Local finance considerations

34. Under Section 70(2) of the Town and Country Planning Act 1990 the council is required when determining planning applications to have regard to any local finance considerations, so far as material to the application. Local finance considerations are defined as a government grant or the Community Infrastructure Levy.
35. Whether or not a local finance consideration is material to a particular decision will depend on whether it could help to make the development acceptable in planning terms. It would not be appropriate to make a decision on the potential for the development to raise money for a local authority.

36. In this case local finance considerations are not considered to be material to the case.

Conclusion

37. The proposal in Quebec Road would provide three new affordable homes in a sustainable location and is considered to be acceptable in planning terms. There would be some harm caused to residential amenity in terms of the loss of the existing garage blocks. However this must be balanced against the benefit of the proposal in terms of providing much needed affordable housing and this benefit is considered to outweigh the loss of the site for parking, particularly given the presence of alternative garage provision within the locality.

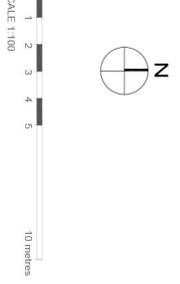
38. The proposal is in accordance with the requirements of the National Planning Policy Framework and the policies of the Development Plan, and there are no material considerations that indicate it should be determined otherwise.

Recommendation

To approve application 16/01121/F and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Details of facing and roofing materials; windows; joinery; boundary treatments, walls and fences; external lighting;
4. Details of hard and soft landscaping and planting
5. Water efficiency
6. Contamination risk assessment and report to be submitted
7. Unknown contamination to be addressed
8. Control on imported materials
9. Tree protection measures to be implemented and replacement tree planting to be agreed

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined in the officer report.



LEGEND

- Existing trees to be retained
- Trees to be removed
- Proposed planting
- Existing paved area
- Proposed paved area
- Existing driveway
- Proposed driveway
- Existing fence
- Proposed fence
- Existing wall
- Proposed wall
- Existing window
- Proposed window
- Existing door
- Proposed door
- Existing roof
- Proposed roof
- Existing floor
- Proposed floor
- Existing ceiling
- Proposed ceiling
- Existing staircase
- Proposed staircase
- Existing lift
- Proposed lift
- Existing ramp
- Proposed ramp
- Existing stairs
- Proposed stairs
- Existing balcony
- Proposed balcony
- Existing terrace
- Proposed terrace
- Existing garden
- Proposed garden
- Existing lawn
- Proposed lawn
- Existing path
- Proposed path
- Existing wall
- Proposed wall
- Existing window
- Proposed window
- Existing door
- Proposed door
- Existing roof
- Proposed roof
- Existing floor
- Proposed floor
- Existing ceiling
- Proposed ceiling
- Existing staircase
- Proposed staircase
- Existing lift
- Proposed lift
- Existing ramp
- Proposed ramp
- Existing stairs
- Proposed stairs
- Existing balcony
- Proposed balcony
- Existing terrace
- Proposed terrace
- Existing garden
- Proposed garden
- Existing lawn
- Proposed lawn
- Existing path
- Proposed path

26.06.16 **F** Approved site plan submitted to the Local Authority

22.07.16 **F** Approved site plan submitted to the Local Authority

02.08.16 **C** Approved site plan submitted to the Local Authority

02.08.16 **A** Approved site plan submitted to the Local Authority

02.08.16 **A** Approved site plan submitted to the Local Authority

26.06.16 **F** Approved site plan submitted to the Local Authority

22.07.16 **F** Approved site plan submitted to the Local Authority

02.08.16 **C** Approved site plan submitted to the Local Authority

02.08.16 **A** Approved site plan submitted to the Local Authority

02.08.16 **A** Approved site plan submitted to the Local Authority

Project: QUEBEC ROAD, NORWICH, NR1 4HY

Client: ORWELL HOUSING

Design: PROPOSED SITE PLAN

Date: MAY 2016

Scale: 1:100 @ A1

Drawn by: SCAF

Checked by: I:100 @ A1

Project No.: PWT7-R-03

Revision: F

Project: QUEBEC ROAD, NORWICH, NR1 4HY

Client: ORWELL HOUSING

Design: PROPOSED SITE PLAN

Date: MAY 2016

Scale: 1:100 @ A1

Drawn by: SCAF

Checked by: I:100 @ A1

Project No.: PWT7-R-03

Revision: F

Report to Planning applications committee
 13 October 2016

Report of Head of planning services

Subject 16/01115/F- Garages opposite 46 Goldwell Road

Reason for referral Objections and application affecting City Council land.

Item

4(i)

Ward:	Town Close
Case officer	Robert Webb - robertwebb@norwich.gov.uk

Development proposal		
Demolition of garages and construction of 6 No. flats.		
Representations		
Object	Comment	Support
37	0	0

Main issues	Key considerations
1 Principle of development	Principle of redevelopment for housing
2 Design	Impact on the character of the area, scale, form, massing and appearance.
3 Transport	Accessibility of site, impact on car parking, traffic, highway safety, cycle parking, servicing.
4 Amenity	Impact on neighbouring occupiers
Expiry date	20 October 2016
Recommendation	Approval subject to conditions.



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Planning Application No 16/01115/F

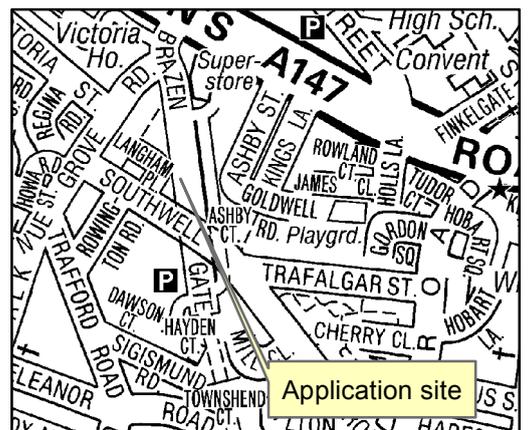
Site Address Garages opposite 46 Goldwell Road

Scale 1:1,000



NORWICH
City Council

PLANNING SERVICES



The site and surroundings

1. The site is owned and managed by Norwich City Council and consists of a surface car park with 18 spaces and 7 garages just to the south of the city centre. The site is at the end of a cul-de-sac and is surrounded by residential development to the east, west and south. To the north is a business unit and beyond that a multi-storey car park used by Aviva.

Constraints

2. The site is adjacent to, but outside of, the Thorpe Hamlet Conservation Area.

Relevant planning history

3. There is no relevant planning history held by the City Council.

The proposal

4. The proposal is one of a number of sites identified by Norwich City Council as having the potential to accommodate new affordable housing to be developed by a registered provider, Orwell Housing Association. The Council are seeking overall to deliver 66 affordable units across the city as part of the current programme, and these would be designed to meet Homes and Communities Agency (HCA) design and quality standards. The dwellings would be available at social or affordable rent whilst meeting high environmental standards. All homes would be advertised using the City Council's choice based letting scheme.
5. In this instance the proposal is to demolish an existing garage block and construct a two storey building in place of the garage block and existing car park which would contain six one bedroom flats. The flats would be a car-free development, although six parking spaces would be provided which would be allocated by Norwich City Council in response to local need in the area. Two of these would be disabled spaces. There would be a private communal amenity area to the rear which would house a cycle store. The proposals have been revised to improve their appearance from the street and relationship with the surrounding context.

Summary information

Proposal	Key facts
Scale	
Total no. of dwellings	6
No. of affordable dwellings	6
Total floorspace	166.6m ² (each flat 46m ² , with one being 48.6m ²)
No. of storeys	2
Density	96 dwellings per hectare

Appearance	
Materials	Walls – red stock brickwork and cream render. Roof – concrete slates. Windows – white Upvc double glazed units. Doors – GPR/timber front entrance doors.
Energy and resource efficiency measures	Solar pv panels, low energy lighting, gas condensing combination boiler with flue gas heat recovery system.
Transport matters	
Vehicular access	From Goldwell Road
No of car parking spaces	The proposed development is car free but includes 4 parking spaces and 2 disabled places which would be owned and allocated by Norwich City Council in response to local need.
No of cycle parking spaces	Cycle storage provided for each property within secure area.
Servicing arrangements	Communal bin storage within the ground floor or proposed building.

Representations

6. Advertised on site and in the press. Adjacent and neighbouring properties have been notified in writing. Representations have been received from 31 individuals citing the issues as summarised in the table below. In addition a petition has been received signed by 61 people. All representations are available to view in full at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Issues raised	Response
Concern at loss of parking spaces and increased parking pressure on Goldwell Road, Ashby Street and Kings Lane where parking is at a premium, including from the use of business parking permits.	See main issue 4
Concern at loss of garage spaces	See main issue 4
Concern regarding highway safety	See main issue 3
Concern the proposal would be over-dominant, cause overlooking and overshadowing	See main issue 4
The flats do not meet the minimum internal space standards	See main issue 2

Concern that the proposal would inhibit access by emergency vehicles.	See main issue 3
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Consultation responses

7. Consultation responses are summarised below the full responses are available to view at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Environmental protection

8. I have viewed the desk study provided for this application and agree with the recommendation that further intrusive works are required. If approval is given, I suggest that conditions are applied to secure this. The unexploded ordnance risk may also require further consideration by a specialist due to the close proximity of known WWII bomb drops.

Highways (local)

9. No objection on highway/transportation grounds. The development and site layout is acceptable, parking, refuse and cycle storage are acceptable.

NCC Housing

10. Following public consultation on the plans for this site, we were made aware that there are Blue Badge holders using the garages here. We spoke to the applicants and have now agreed that instead of them developing a housing scheme with six allocated parking spaces, that the site will be car-free, with six spaces, including two disabled spaces to be made available for local residents. There are six garage tenants that would be served with a Notice to Quit if the plans are approved. Garage vacancies were checked on 30/09/16. There were 6 garages available within 250 metres of the site and a further 6 available within 500 metres. There are additional garages within Goldwell Road but these are currently fully occupied. When a vacancy arises, priority will be given to any blue badge holders who have been served a Notice To Quit.

Assessment of planning considerations

Relevant development plan policies

11. **Joint Core Strategy for Broadland, Norwich and South Norfolk adopted March 2011 amendments adopted Jan. 2014 (JCS)**
- JCS2 Promoting good design
 - JCS3 Energy and water
 - JCS4 Housing delivery
 - JCS7 Supporting communities
 - JCS9 Strategy for growth in the Norwich policy area
12. **Norwich Development Management Policies Local Plan adopted Dec. 2014 (DM Plan)**
- DM1 Achieving and delivering sustainable development
 - DM2 Ensuring satisfactory living and working conditions

- DM3 Delivering high quality design
- DM4 Providing for renewable and low carbon energy
- DM5 Planning effectively for flood resilience
- DM11 Protecting against environmental hazards
- DM12 Ensuring well-planned housing development
- DM28 Encouraging sustainable travel
- DM30 Access and highway safety
- DM31 Car parking and servicing

Other material considerations

13. Relevant sections of the National Planning Policy Framework March 2012 (NPPF):

- NPPF4 Promoting sustainable transport
- NPPF6 Delivering a wide choice of high quality homes
- NPPF7 Requiring good design
- NPPF10 Meeting the challenge of climate change, flooding and coastal change
- NPPF11 Conserving and enhancing the natural environment

Case Assessment

14. Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. Relevant development plan policies are detailed above. Material considerations include policies in the National Planning Framework (NPPF), the Councils standing duties, other policy documents and guidance detailed above and any other matters referred to specifically in the assessment below. The following sections provide an assessment of the main planning issues in this case in relation to the relevant policies and material considerations.

Main issue 1: Principle of development

15. Joint Core Strategy (JCS) Policy 4, supports housing delivery within the plan area, which this site falls. National policy, as set out in the Core Principles of the NPPF encourages new housing development to make the fullest possible use of public transport, walking and cycling, and focus significant development in locations which are or can be made sustainable. JCS policy 4 also encourages provision of affordable housing including of social rent and affordable rent tenure types as these are recognised and being particularly important in meeting housing need in the city.
16. Policy DM12 of the Norwich Development Management Policies Plan supports new residential development within the city boundary except in specific circumstances, none of the exceptions apply to this application site.
17. The NPPF encourages 'the effective use of land by reusing land that has been previously developed'. This site constitutes previously developed land. The site is in a sustainable location for new housing within the city centre. The proposed housing is therefore considered to be acceptable in principle and in this case would have the planning benefit of providing new affordable housing, subject to assessment against

any other relevant policies or material considerations as outlined in the NPPF and the Development Plan. This is further set out below.

Main issue 2: Design

18. Key policies and NPPF paragraphs – JCS2, DM3, NPPF paragraphs 9, 17, 56 and 60-66.
19. The site is surrounded by two storey residential development and the proposal would be in keeping with the scale and form of development in the area. Amended plans have been sought during the application process to ensure the layout respects the position of surrounding buildings and provides for a better standard of amenity for existing and future residents. The proposal provides for the necessary bin and cycle storage and also provides a small outside amenity area, together with small areas of outdoor private amenity spaces for the ground floor flats. The design, scale and appearance is considered acceptable.
20. The flats proposed would have an internal floor area of 46 sq. m. (apart from one flat which has an internal floor area of 48.6 sq. m.) and are intended as 1 bedroom 2 person units. The floorspace is therefore below the national space standards figure of 50 sq. m. for this type of property. It is recognised however that if the dwellings were occupied by 1 person, then the minimum space standard of 39m² would be met. Whilst the failure to meet the minimum space standards is regrettable, it is not considered in itself to warrant refusal of the application given that the proposal is otherwise well-designed and would lead to the delivery of affordable housing in a sustainable location.

Main issue 3: Transport

21. Key policies and NPPF paragraphs – DM2, DM11, NPPF chapter 4.
22. The site is located in an accessible and sustainable location within walking distance of the city centre and bus station. Car-free development is therefore acceptable in this location. The proposal would provide 6 parking spaces which would be allocated by the City Council according to local need. Secure cycle parking would be provided for each property. No objection is raised by the Highway Officer with regard to highway safety. The proposal therefore complies with the above policies. The proposal would not compromise the ability of the emergency services to access Goldwell Road. The issue of the loss of the existing car park and garages is dealt with in the following section.

Main issue 4: Amenity

23. Key policies and NPPF paragraphs – DM2, DM11, NPPF paragraphs 9 and 17.
24. A number of objections have been received to the proposal, with particular concerns about the loss of parking/garage spaces, increased parking pressure on the surrounding roads and the potential knock-on effects of this pressure.
25. Information provided with the application indicates that in June this year 100% of the garages were occupied, meaning all 7 garages. At that time there were 8 available garages within 800m of the site, which indicates that alternative provision is available in the locality. Information received from the applicant indicates the

surface parking area is very well used, particularly during the evenings and weekends, and this point has been made by the residents.

26. It is acknowledged that some harm would be caused to residential amenity by the loss of the existing spaces. However this must be weighed against the significant benefits of delivering affordable housing in a sustainable location within the city. This will be a matter for Members to judge, but the advice from Officers is that in planning policy terms, addressing housing need is considered to be of greater importance than providing off-road parking spaces, particularly in a location which has excellent links to public transport and the city centre and where there is the opportunity to use other modes of transport such as buses and cycles to travel. It is therefore recommended that the application should not be refused on the grounds of loss of parking.
27. With regard to other concerns about loss of privacy and overshadowing, the amended proposal has been designed in such a way that would minimise these impacts, both through its siting and also through the careful positioning of windows and the use of obscure glazing where appropriate. No material harm would be caused in this regard.

Other matters

28. The proposal is acceptable in terms of its impact on flood risk, biodiversity, land contamination and the energy efficiency measures proposed.

Equalities and diversity issues

29. There are no significant equality or diversity issues.

Local finance considerations

30. Under Section 70(2) of the Town and Country Planning Act 1990 the council is required when determining planning applications to have regard to any local finance considerations, so far as material to the application. Local finance considerations are defined as a government grant or the Community Infrastructure Levy.
31. Whether or not a local finance consideration is material to a particular decision will depend on whether it could help to make the development acceptable in planning terms. It would not be appropriate to make a decision on the potential for the development to raise money for a local authority.
32. In this case local finance considerations are not considered to be material to the case.

Conclusion

33. The proposal for development of the site in Goldwell Road would provide six new affordable homes in a sustainable location and is considered to be acceptable in planning terms. It is acknowledged that there would be some harm caused to residential amenity for the surrounding properties in terms of the loss of the existing garage and surface parking, and clearly this is a contentious matter for local residents. However this must be balanced against the benefit of the proposal in terms of providing much needed affordable housing and when assessed against local and

national planning policies, this benefit is considered to outweigh the loss of the site for parking, particularly given the presence of alternative garage provision within the locality and the very sustainable and accessible location of the site.

34. The proposal is in accordance with the requirements of the National Planning Policy Framework and the policies of the Development Plan, and there are no material considerations that indicate it should be determined otherwise.

Recommendation

To approve application 16/01121/F and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Details of facing and roofing materials; windows; joinery; boundary treatments, walls and fences; external lighting;
4. Details of hard and soft landscaping and planting
5. Window serving stairwell on southern elevation to be obscure glazed and non-opening
6. Water efficiency
7. Contamination risk assessment and report to be submitted
8. Unknown contamination to be addressed
9. Control on imported materials

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations, following negotiations with the applicant and subsequent amendments the application has been approved subject to appropriate conditions and for the reasons outlined in the officer report.

LEGEND

-  Details taken from floorplans.
-  Details proposed new parking layout to be Specialist design & to the approval of the Local Authority.
-  Details 2000 x 1700mm timber garden shed for extra storage etc. to have a permanent lock to BS 3621:2004. Shed to be securely fixed to concrete base.
-  Details standard 100 brick perforated walls with cast in situ concrete base to be approved by the Local Authority.
-  Details 2 No. 140 litre wheelie bins, General domestic waste & weekly collection bin & collected by the Local Authority on a weekly collection day.
-  Details 1000mm high perforated cast in situ concrete base to be approved by the Local Authority.
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Proposed site plan 1:100

Schedule	Detached / Semi-d	Storey Height	Height / Area	Parking Arrangements
Flat 1	1b/2p	1	46.0 (1665)	No allocated parking space
Flat 2	1b/2p	1	46.0 (1665)	No allocated parking space
Flat 3	1b/2p	1	46.0 (1665)	No allocated parking space
Flat 4	1b/2p	1	46.0 (1665)	No allocated parking space
Flat 5	1b/2p	2	46.0 (1665)	No allocated parking space
Flat 6	1b/2p	2	46.0 (1665)	No allocated parking space
622 sqm (0.087 hectares) Site Area				

peterwellsarchitects

office from: Watershipham, Woodbridge, Suffolk, IP19 7BA - 01779 745556 - info@peterwellsarchitects.co.uk

Project: NCC Garage Site, Goldwell Road, RN1 3PX

Client: Orwell Housing

Date: April 2016

Scale: 1:100 @ A1

Drawn by: P.W./S.P./J.Z.

Checked by: F.

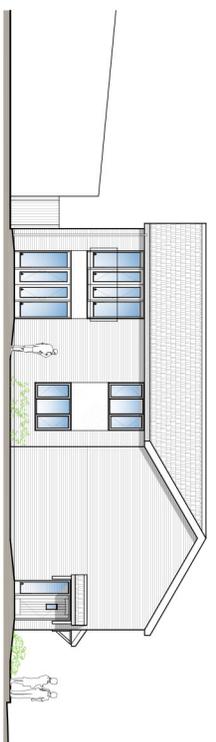
Roof Pitch 36.0 deg
 Concrete cladding
 Windows uPVC colour: white
 Entrance Doors: G Steel faced painted uPVC gullies & downpipes colour: black
 Staircase: G Steel faced painted uPVC
 Monochrome through colour Render, colour: cream
 Tno: 1173 x 2688mm P120/500 Warden Clearline PV panel per unit



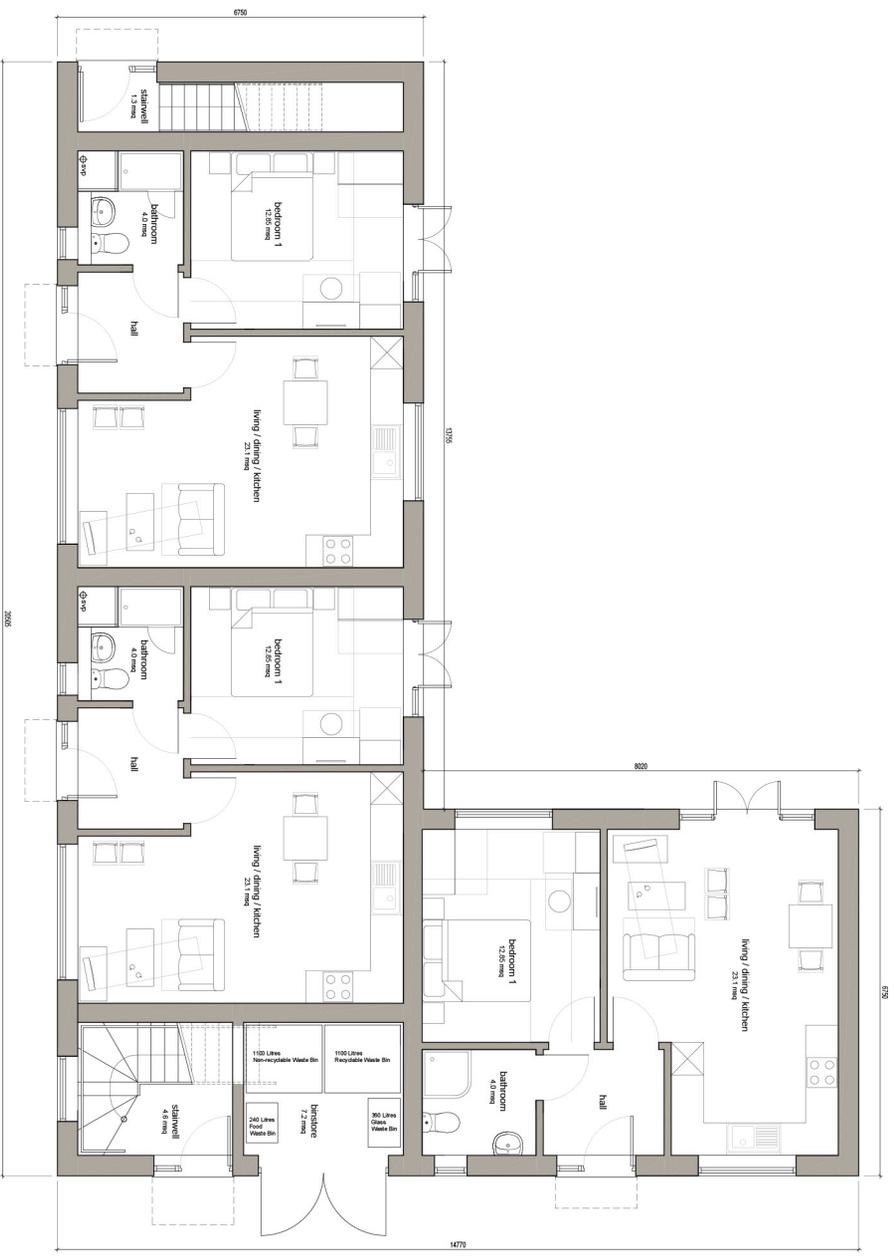
proposed south elevation 1:100

- ▲ Proposed Slab/Chimney
- ▲ Unimproved Slab/Chimney
- Observed Cladding

CSM
 CONSTRUCTION DESIGN AND MANAGEMENT REGULATIONS 2015
 These regulations apply to the construction of buildings over the ground. Refer to clause 5.4.1
 1. Approved by the Council of the City of London on 11th October 2015
 2. Approved by the Council of the City of London on 11th October 2015
 3. Approved by the Council of the City of London on 11th October 2015
 4. Approved by the Council of the City of London on 11th October 2015
 5. Approved by the Council of the City of London on 11th October 2015
 6. Approved by the Council of the City of London on 11th October 2015
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 8. Approved by the Council of the City of London on 11th October 2015
 9. Approved by the Council of the City of London on 11th October 2015
 10. Approved by the Council of the City of London on 11th October 2015



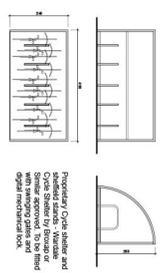
proposed west elevation 1:100



Plot 3 Ground Floor Plan
 1b2p Flat
 46.0m² GIA

Plot 1 Ground Floor Plan
 1b2p Flat
 46.0m² GIA

Plot 2 Ground Floor Plan
 1b2p Flat
 46.0m² GIA



proposed cycle store details - 1:100

peterwellsarchitects

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Project: NCC Garage Site, Goldwell Road, NR1 3PX

Client: Orwell Housing

Date: April 2016

Scale: 1:50, 1:100 @ A1

Disc No.: PW761.P1.03

Revision: F

Disc No.	Revision	Author	Checked
012616 F	Approved for planning submission	AW	PN
012616 E	Approved for planning submission	AW	PN
012616 D	Approved for planning submission	AW	PN
012616 C	Approved for planning submission	AW	PN
012616 B	Approved for planning submission	AW	PN
012616 A	Approved for planning submission	AW	PN



proposed north elevation 1:100

proposed east elevation 1:100



Plot 6 First Floor Plan
1b2p Flat
46 Dm² GIA

Plot 4 First Floor Plan
1b2p Flat
48 Dm² GIA

Plot 5 First Floor Plan
1b2p Flat
46 Dm² GIA

SCALE 1:50

0 1 2 3 4 5 metres

peterwellsarchitects

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12.04.16	E	Approved by project committee	add	PPG
07.07.16	C	Approved by design committee	add	PPG
02.08.16	B	Approved by client	add	PPG
15.08.16	A	Approved by client	add	PPG
15.08.16	A	Approved by client	add	PPG

Project:	NCC Garage Site, Godwell Road, NRI 3PX	Site Name:	Proposed First Floor Plan & Elevations	Drawn:	Planning
Client:	Oswell Housing	Scale:	1:50, 1:100 @ A1	Revision:	E
Date:	April 2016	Scale:	1:50, 1:100 @ A1	Revision:	E

Report to Planning applications committee
 13 October 2016

Report of Head of planning services

Subject Application no 16/00563/F - Kingdom Hall Of Jehovah's Witnesses Clarke Road Norwich NR3 1JL

Reason for referral Objections

Item

4(j)

Ward:	Sewell
Case officer	Mr Steve Fraser-Lim - stevefraser-lim@norwich.gov.uk

Development proposal		
Demolition of existing building and erection of 3 No. dwellings (revised plans).		
Representations		
Object	Comment	Support
8	0	0

Main issues	Key considerations
1 Principle of Development	Principle of loss of Hall and redevelopment for housing
2 Design and Heritage	Impact on character of surrounding area and site
3 Transport	Access and egress to the site / cycle / bin storage
4 Amenity	Internal and external amenity space for future occupiers and the impact of development on neighbouring properties
Expiry date	14 July 2016
Recommendation	Approve



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Planning Application No 16/00563/F

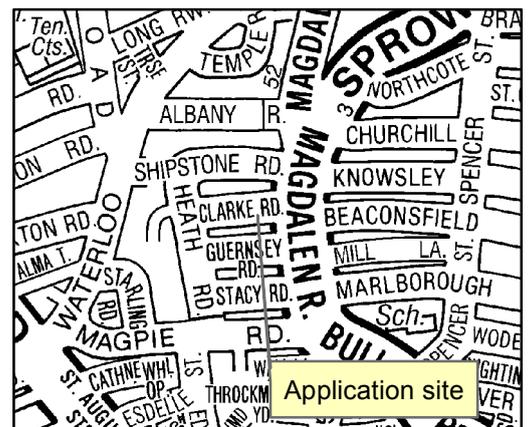
Site Address Kingdom Hall of Jehovah's Wtnesses
Clark Road

Scale 1:1,000



NORWICH
City Council

PLANNING SERVICES



The site and surroundings

1. The application seeks full permission for the demolition of the Kingdom Hall of Jehovah's Witnesses and the subsequent erection of three terraced dwellings. The Hall occupies most of the site, being built up against the southern boundary.
2. The proposed dwellings are sited 2.4m back from the road, with the principal elevations facing towards Clarke Road. They are 2 ½ stories high, with accommodation in the roof space. Private amenity areas are provided to the rear, and parking for one car per dwelling is provided via integral garages accessed from the principal elevation.
3. The immediate neighbours are largely residential, however a convenience store (Tesco Express), with an attached maisonette, is located to the east of the site.

Constraints

4. The site is located within an Area of Main Archaeological Interest and a Critical Drainage Area. The convenience store forms part of a Local Retail Centre. Clarke Road and the immediate area lies within a Controlled Parking Zone (CPZ).
5. Clarke Road slopes down to the west. The site borders the rear boundaries of dwellings along Guernsey Road to the rear (south), which is sat at a lower level.

Relevant planning history

6.

Ref	Proposal	Decision	Date
4/1997/0675	Erection of pitched roof on existing flat roof and internal alterations	APCON	17/11/1997

The proposal

7. The application seeks full planning permission for the demolition of Kingdom Hall and the erection of three terraced dwellings. The Hall was last used as a place of worship.
8. The dwellings would be 2½ storeys, with dormers to the rear serving master bedrooms in the roof space. A single integral garage is located on the ground floor in all the dwellings. A total of three bedrooms per dwelling are proposed. It is noted that the plans indicate an attic room per dwelling within the roof space, which could in principle be converted into additional living accommodation. However due to the shallow roof pitch this room would be restricted in use as a result of the roof height.
9. The plans have been amended following discussions with the agent. The amended plans represent a reduction in terms of scale and height from those originally submitted. The proposal has been reduced from 3 storeys to 2½ storeys, and rear balconies have been removed. The design has also been altered to include a dual

pitched roof rather than a curved roof. Representations were received following the advertisement of both the original plans and amended plans, and are split out accordingly below.

Summary information

Proposal	Key facts
Scale	
Total no. of dwellings	3
No. of affordable dwellings	0
Total floor space	379sqm
No. of storeys	2.5
Max. dimensions	Terrace block 15.5m wide and maximum 11m deep (staggered principal elevation). Height to eaves 5.1m and height to roof ridge 8.9m.
Density	97 dwellings per hectare
Appearance	
Materials	Ground floor red facing bricks, first floor buttermilk render and red pantiles to the roof. Fenestration to be white uPVC.
Transport matters	
Vehicular access	Single integral garage per dwelling. No permits to be issued.
No of car parking spaces	Total of 3 spaces within the garages, one per dwelling.
No of cycle parking spaces	4 per dwelling (2 adult and 2 children sized bikes)
Servicing arrangements	Individual bin storages to rear within private gardens

Representations

- Advertised on site and in the press. Adjacent and neighbouring properties have been notified in writing. 9 letters of representation have been received from 8 individuals, citing the issues as summarised in the table below. All representations are available to view in full at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Issues raised from Original Plans	Response
There is already too little parking in the area, with the spaces outside the Hall used in the evening by residents (which would be lost). Concerns regarding the safety of accessing the garages and possible conflict with pedestrians. Exacerbated by the vicinity of Tesco Express.	See main issue 3
History of subsidence on the road and concerns that the works would impact the stability of existing properties. Will a wall become a shared boundary wall?	This would be covered under Building Regulations, and potentially the Party Wall Act 1996.
Properties would be overbearing at 3 stories and create overshadowing and overlooking for both the neighbouring dwellings and their gardens. Exacerbated by an increase in the level of land to the east.	See main issue 4
Concerns regarding the removal of asbestos.	Applicant will be advised of appropriate precautions to take, which are covered under other regulations.
Design is not sympathetic to the area; it is out of scale and looks like a hospital/hotel. It is not an ambitious contemporary design or traditional in nature. Could be more like the development opposite.	See main issue 2
Very limited amenity space.	See main issue 4
Issues raised from Amended Plans	Response
Concerns remain regarding traffic – understood that new dwellings were required to provide more parking than shown.	See main issue 3
Concerns that the garages would be converted into habitable space, losing the only parking. Or not used for parking at all. Would permits also be issued? Does not automatically mean that there is space to park.	See main issue 3
Concerns regarding construction traffic.	Given the small scale of the development construction impacts considered are not considered significant that further mitigation measures are required.
Uncertainty regarding the location of the new	Block plan has now been provided.

<p>dwellings as no block plan has been provided. Drawings lack detail in terms of measurements.</p>	<p>Drawings are to scale; measurements are not a requirement.</p>
<p>Dormer windows are not found locally – inappropriate and would create more overlooking than the skylights found elsewhere.</p>	<p>See main issue 4</p>
<p>4th bedroom/attic room is ambiguous – it can clearly be converted into a 4th bedroom once built.</p>	<p>See main issue 4</p>
<p>Result in both overshadowing and overlooking for neighbouring properties. Although welcome the removal of the balconies.</p>	<p>See main issue 4</p>
<p>Design could be far more aesthetically pleasing. Still represents overdevelopment of the site.</p>	<p>See main issue 2</p>

Consultation responses

11. Consultation responses are summarised below the full responses are available to view at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Highways (local)

12. No objection. The properties would not be entitled to parking permits so integral garages are sensible. A Traffic Regulation Order amendment will be required to create double yellow lines across the frontage. The kerb will also need to be dropped and the pavement strengthened. Consideration also needs to be given to cycle parking and bin storage.

Norfolk Historic Environment Service

13. No archaeological implications.

Natural Areas Officer

14. No comments received

Norwich Society

15. (Original plans) The design is over complicated and gives no consideration to the context. The proposal is poor architecture and is out of scale.

Assessment of planning considerations

Relevant development plan policies

16. **Joint Core Strategy for Broadland, Norwich and South Norfolk adopted March 2011 amendments adopted Jan. 2014 (JCS)**
 - JCS1 Addressing climate change and protecting environmental assets
 - JCS2 Promoting good design
 - JCS3 Energy and water
 - JCS7 Supporting communities
 - JCS9 Strategy for growth in the Norwich policy area
 - JCS12 The remainder of the Norwich urban area including the fringe parishes

17. **Norwich Development Management Policies Local Plan adopted Dec. 2014 (DM Plan)**
 - DM1 Achieving and delivering sustainable development
 - DM2 Ensuring satisfactory living and working conditions
 - DM3 Delivering high quality design
 - DM4 Providing for renewable and low carbon energy
 - DM5 Planning effectively for flood resilience
 - DM6 Protecting and enhancing the natural environment
 - DM9 Safeguarding Norwich's heritage
 - DM12 Ensuring well-planned housing development
 - DM22 Planning for and safeguarding community facilities
 - DM28 Encouraging sustainable travel
 - DM30 Access and highway safety
 - DM31 Car parking and servicing
 - DM32 Encouraging car free and low car housing

Other material considerations

18. **Relevant sections of the National Planning Policy Framework March 2012 (NPPF):**
 - NPPF4 Promoting sustainable transport
 - NPPF6 Delivering a wide choice of high quality homes
 - NPPF7 Requiring good design
 - NPPF8 Promoting healthy communities
 - NPPF10 Meeting the challenge of climate change, flooding and coastal change
 - NPPF11 Conserving and enhancing the natural environment
 - NPPF12 Conserving and enhancing the historic environment

Case Assessment

19. Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. Relevant development plan policies are detailed above. Material considerations include policies in the National Planning Framework (NPPF), the Councils standing duties, other policy documents and guidance detailed above and any other matters referred to specifically in the assessment below. The following

paragraphs provide an assessment of the main planning issues in this case against relevant policies and material considerations.

Main issue 1: Principle of development

20. The principle policies relating to new housing development are Joint Core Strategy (JCS) Policy 4, which supports housing delivery within the plan area, which this site falls, and policy DM12 of the Norwich Local Plan Development Management Policies which deals with new housing development in the city. DM12 supports new housing development subject to the following criteria below, which would all be met in this case:
 - The site is not designated for other purposes;
 - No objection has been received from the Health and Safety Executive;
 - The site is not in the late night activity zone;
 - It does not involve the conversion of high quality office space; and
 - It is not in the primary or secondary retail area or in a district or local centre.
21. The site currently contains a disused place of worship. Whilst the principle of housing within this area is acceptable, for the proposed development to be considered acceptable the loss of this community facility must also be acceptable.
22. The agent has submitted a supporting statement which states that The Kingdom Hall of Jehovah's Witnesses was placed on the market for sale in 2015. The applicants then purchased the property in March 2016, after it had been on the market for approximately 12 months. The current owners have since advertised the building for hire on; social media, online classified advertisement site, and a board on the front of the building. This has not resulted in any bookings. The statement also highlights that an alternative community hall is available nearby (Silver Road Community Centre). It is located approximately 650m walking distance away. DM 22 serves to protect the loss of community facilities, such as community centres and places of worship. Their loss will only be permitted where adequate alternative provision exists within 800m walking distance, or reasonable efforts have been made to preserve the facility and the property has been marketed with no realistic interest received. With alternative provision close by the loss of the Hall is considered to comply with DM 22. Furthermore the supporting statement provides evidence that the current owners have been unsuccessful in their advertising.
23. The Hall is constructed from a mixture of metal sheeting, stained cladding, render, concrete roof tiles and buff bricks. The building is at odds with the prevailing character of the area, and its loss does not raise any historic or local character concerns.

Main issue 2: Design and Heritage

24. Key policies and NPPF paragraphs – JCS2, DM3, NPPF paragraphs 9, 17, 56 and 60-66. Heritage key policies and NPPF paragraphs – DM9, NPPF paragraphs 128-141.
25. The amended plans are for a block of 3 terraced houses, which responds to the neighbouring Victorian terraces in terms of form. The ridge line runs east to west, as do the neighbouring properties, and the terraced block would be sited at a

similar distance from the road. However the roof ridge would be higher and the width of the gables wider.

26. The elevations could perhaps be improved in terms of proportions and placement of fenestration. However the design does include a staggered frontage which is considered to add some interest and depth to the principal elevation, and the fenestration does respond to some extent to the neighbouring dwellings.
27. The use of dormer windows, whilst not prevalent in the immediate area, is considered to be acceptable. Sited to the rear the impact upon the wider character would be relatively small, and they are considered to be suitable for the style of dwellings proposed.
28. At 97 dwellings per hectare the density of the proposal is relatively high. DM 3 advises that density should be in keeping with the existing character of the area. Given that the adjacent 3 terraces to the west represent a density of 127 dwellings per hectare, this level is however considered to be acceptable.
29. The external finish is proposed to be a mixture of red facing bricks and cream render, with red pantiles. Minimal details have been given. The fenestration appears to replicate the top hung sash effect uPVC windows found elsewhere within the road. Whilst this is not a form of fenestration particularly encouraged, given its current use they are considered to be acceptable. All these materials are found within the immediate vicinity; however a condition would be added to request further details before their use.
30. The site is within an identified Area of Main Archaeological Interest. Although no report was submitted, the Norfolk Historic Environment Service has advised that there are no concerns with the proposed development in terms of any impact upon Archaeological remains. The site does not fall within a Conservation Area and there are no listed buildings in the immediate vicinity. The proposal is therefore considered to comply with both DM 9 and NPPF para 128-141.

Main issue 3: Transport

31. Key policies and NPPF paragraphs – JCS6, DM28, DM30, DM31, NPPF paragraphs 17 and 39.
32. The site is located within a Controlled Parking Zone (CPZ), with permit parking available for some existing residents. However, as advised by Highways the new dwellings would not be eligible for permits. Appendix 3 in the Norwich Local Plan provides guidelines on the parking and cycle requirements for new developments. This development can be car free, given that it is considered to be located within an accessible area by virtue of being within a CPZ. Furthermore there is currently access to a car club in the adjacent road (Shipstone Road). As such a garage is not considered to be essential, but is considered acceptable; a maximum of 1.33 spaces per dwelling is permitted.
33. Cycle storage is included within the rear gardens, meeting the minimum storage set out in Appendix 3 in the Norwich Local Plan.
34. With no objection from Highways the garages are considered to be acceptable.

Main issue 4: Amenity

35. Key policies and NPPF paragraphs – DM2, DM11, NPPF paragraphs 9 and 17.
36. As described above the proposed terrace block responds to the existing terraces in terms of siting and form. The dwellings would be taller than the adjacent neighbours, with the neighbour to the east 8.6m to the roof ridge and to the west 8.3m (compared to the proposed 8.9m). However as the proposal is largely in line with the neighbouring dwellings, the impact from overshadowing here would be restricted to the windows facing the site.
37. The dwelling to the west has one window in this elevation which appears to serve a landing, and is sited 2m from the current Hall. The proposal will result in a blank wall sited 1.6m away. Whilst this will lead to some additional overshadowing, given the existing arrangement and type of window this is considered to be acceptable on balance. The neighbouring maisonette to the east has one window facing the site, sited 5.8m from the current Hall. The proposal will result in a blank wall sited 4.5m away. Given the existing arrangement and orientation this is also considered to be acceptable.
38. To the rear the dwellings are terraced too, with two storey protruding rear sections extending towards the site. The majority of dwellings along both Clarke Road and Guernsey are of a similar design, and have first floor bedroom windows in the rear of two storey sections. As such there is a degree of overlooking between these dwellings; with first floor windows sited approx. 14m away from each other. The proposal would arguably replicate this relationship; with the dormer windows sited approx. 14.8m away from the existing first floor windows in the dwellings to the south. Whilst it is acknowledged that this would increase the level of overlooking for these properties, given the layout elsewhere in this vicinity it is considered to be acceptable. Due to the orientation the level of overshadowing does not cause significant concern. Although the bulk of the building will appear larger due to the orientation of the roof running east to west instead of north to south, the building will be placed further away from the southern boundary than the current Kingdom Hall.
39. Due to the size constraints of the site it is considered appropriate to remove permitted development rights to prevent the site becoming overdeveloped and creating a significant impact upon any neighbour's amenity. Furthermore this would prevent the addition of any windows within the side elevations which could cause undue overlooking.

Compliance with other relevant development plan policies

40. A number of development plan policies include key targets for matters such as parking provision and energy efficiency. The table below indicates the outcome of the officer assessment in relation to these matters.

Requirement	Relevant policy	Compliance
Cycle storage	DM31	Yes subject to condition
Car parking provision	DM31	Yes, one space per dwelling provided via the garages

Refuse Storage/servicing	DM31	Yes subject to condition
Water efficiency	JCS 1 & 3	Yes subject to condition
Sustainable urban drainage	DM3/5	Yes. The reduction in the size of the building on the site will in itself enable less run off if some of the remainder of the site is left permeable. Whilst the applicant has indicated that the surface water run-off will be disposed of via a soakaway no details have been given of any other measures such as a permeable driveway. However these details can be requested via a condition. With a suitable condition the impact upon the drainage is considered acceptable as it should improve the existing arrangement.

Equalities and diversity issues

41. There are no significant equality or diversity issues.

Local finance considerations

42. Under Section 70(2) of the Town and Country Planning Act 1990 the council is required when determining planning applications to have regard to any local finance considerations, so far as material to the application. Local finance considerations are defined as a government grant or the Community Infrastructure Levy.
43. Whether or not a local finance consideration is material to a particular decision will depend on whether it could help to make the development acceptable in planning terms. It would not be appropriate to make a decision on the potential for the development to raise money for a local authority.
44. In this case local finance considerations are not considered to be material to the case.

Conclusion

45. The loss of the Hall and redevelopment of the site for housing is considered to be acceptable for the reasons given above. The amended design of the terrace dwellings reflects the existing dwellings within the immediate vicinity and would not detract from the character and appearance of the area.
46. Whilst it is acknowledged that there will be some increase in the level of overlooking and overshadowing for some neighbours, the levels are considered to be acceptable in this relatively densely built area as they are comparable to existing relationships.

47. The level of parking provided accords with DM 31 and DM 32. With no objection from Highways the proposed garages are considered acceptable.
48. As such the development is in accordance with the requirements of the National Planning Policy Framework and the Development Plan, and it has been concluded that there are no material considerations that indicate it should be determined otherwise.

Recommendation

To approve application no. 16/00563/F - Kingdom Hall Of Jehovah's Witnesses Clarke Road Norwich NR3 1JL and grant planning permission subject to the following conditions:

1. Standard time limit;
2. In accordance with plans;
3. Landscaping Details
4. External Materials
5. Removal of Permitted Development rights
6. Water efficiency
7. Submission cycle/ bin storage details

Article 35(2) statement

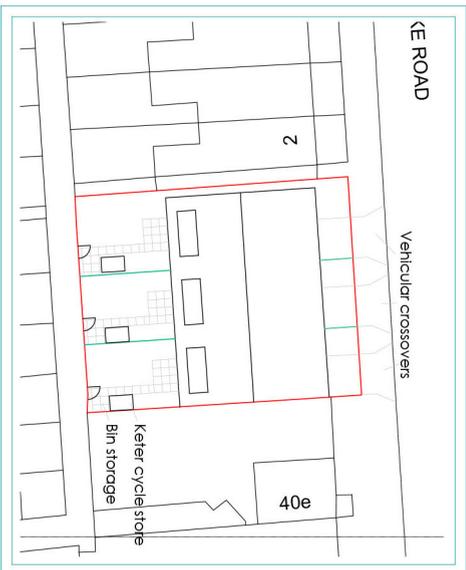
AT2 Approved following amendments



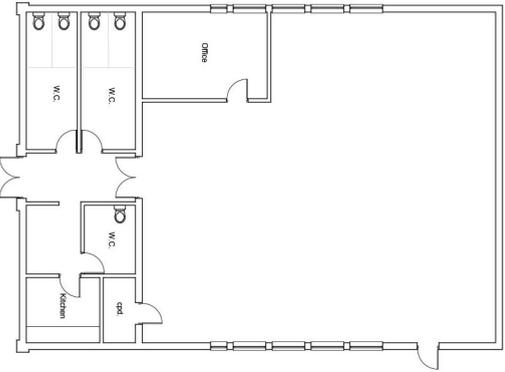
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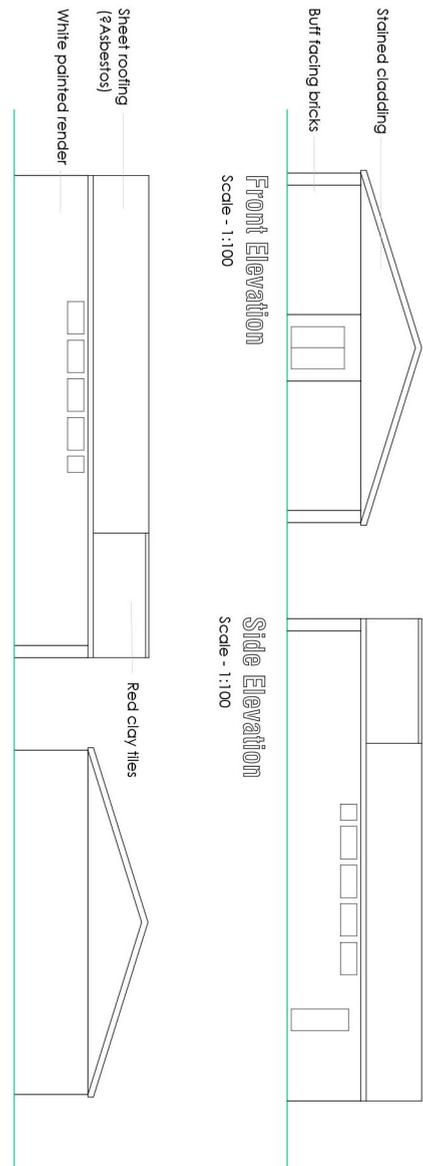
Existing Block Plan
Scale - 1:200



Proposed Block Plan
Scale - 1:200



Existing Ground Floor
Scale - 1:100



Front Elevation
Scale - 1:100

Side Elevation
Scale - 1:100

Side Elevation
Scale - 1:100

Rear Elevation
Scale - 1:100

NOTES:
1. DO NOT SCALE from this drawing.
Contractors must verify all dimensions on site before setting out.
Contracting work, ordering materials or making any trip drawings.
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The contractor confirmed in the drawing a representative and accurate representation of the proposed building and does not warrant or certify the structure of the building or neighbouring structure at the time of construction.
The contractor is to visit the site to make himself acquainted with the site conditions and to make all arrangements to ensure that a full and final quotation for the work will be submitted, taking into account all variations.
Failure to do so will be at the contractor's own risk and no amendments to the work.
The contractor is to carry out all works in full compliance with the Health and Safety Commission's Approved Code of Practice (ACOP) for the construction of buildings.
All work and working practices on the site shall be carried out in accordance with the above and to ensure that there are no accidents or fatalities.
The contractor is to be responsible for all arrangements to cover the prevention of accidents and injury.
All work is to be carried out in compliance with the local authority regulations, good standards, fire safety recommendations, specialist subcontract recommendations and manufacturer literature is to be kept on site. Provide all relevant guarantees in duplicate for presentation to the client.

Allow of necessary appearance and form work, and all materials and finishes to be used shall be approved by the local Building Control and other methods as required by the local authority.
Carefully examine the drawings and verify any discrepancies with the site conditions and to make all arrangements to ensure that a full and final quotation for the work will be submitted, taking into account all variations.
Check all dimensions on the ground and notify any discrepancies to the CA for reference prior to proceeding.

This drawing and details is for use solely in connection with the project identified below.
No responsibility for any error or omission in this Specification will be Recognised unless brought to the Attention Of The Client Or the Agent prior to signing the Contract.

Rev:	Date:	By:	Author:
B	26/09/12	C.C.	Cycle store added
A	28/08/12	C.C.	Block plan amended



Project Name: www.pricedecorarchitecture.co.uk
Proposed Development of:
Norman News
Clerke Road
Norwich

for S&R Capital Ltd.

Drawn By:	Job Number:
Prasanna Rajasekaran	216
Checked By:	Drawn No:
Collette Corring	002 B
Date:	As noted at AI
May 2015	002 B

All dimensions are in millimetres.
All dimensions to be checked on site.

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Contracting work, ordering materials or making any trip
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The information contained in this drawing is representative and does not warrant or certify the structure of the building or neighbouring structures of the frame of construction.
The contractor is to visit the site to make himself acquainted with the site conditions and to make all arrangements to ensure that a full and final quotation for the work will be submitted taking into account all variations.
Failure to do so will be at the contractor's own risk and no amendments to the work.

The contractor is to comply with all works in full compliance with the Health and Safety Commission's Approved Code of Practice (Working Construction for Health and Safety) and the Construction (Design and Management) Regulations 2015. All work and working practices on the site shall be carried out in accordance with the above and to ensure that there are no accidents, injuries or claims that arise.
The contractor is to include all performance requirements to cover the prevention of accidents and injury.
All work is to be carried out to comply with the local authority regulations, good standards, fire safety, recommendations, specialist subcontract recommendations manufacturers literature etc to be kept on site. Provide all relevant guarantees in duplicate for presentation to the client.

Allow of necessary appearance and form work, 1:1 and 1:100 scale. The contractor is to provide all necessary building control and permit notifications as required to the local authority in writing. The contractor is to provide all necessary building control and permit notifications as required to the local authority in writing. The contractor is to provide all necessary building control and permit notifications as required to the local authority in writing.
Carefully note the scale of the drawing and only engineering drawings are to be used for construction purposes. Check all dimensions on the drawing and confirm them to the client and notify any discrepancies to the CA for rectification prior to proceeding.

The starting and ending of the work shall be in connection with the project described below.
No Responsibility for any fire or omission in this Specification will be Recognized unless brought to the Attention of the Client on the spot prior to signing the Contract.
D 22.08.16 C.C. Recr deviation offered
E 14.07.16 C.C. Recr deviation offered
F 14.07.16 C.C. Recr deviation offered
A 23.06.16 C.C. Steel deviation added

Rev	Date	By	For	Approved:

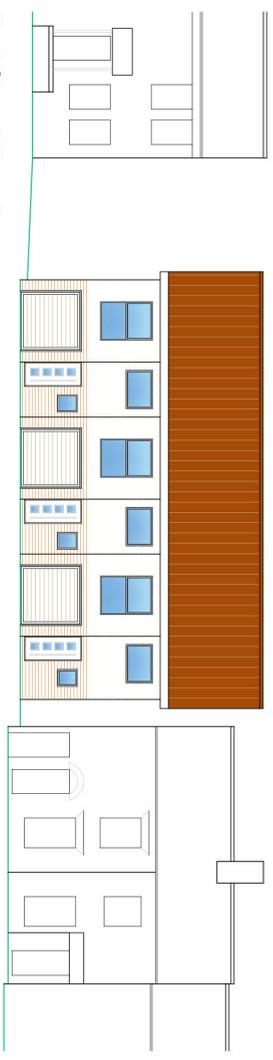


Project Name: www.practicalarchitecture.co.uk
Proposed Development at:
Norman News
Clarke Road
Norwich

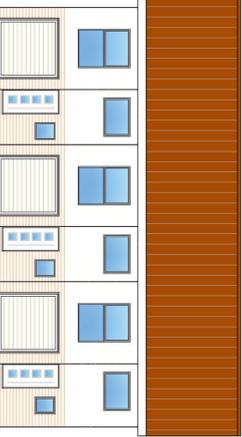
for S&R Capital Ltd.

Drawing Title: **Proposed Development at Norman News Clarke Road Norwich**
Drawing No: **216**
Drawn By: **Collette Coding**
Checked By: **Collette Coding**
Date: **April 2016**
As noted at A1 **001 D**

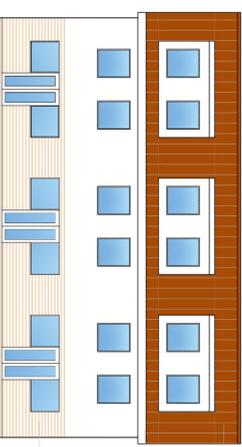
All dimensions are in millimetres.
All dimensions to be checked on site.



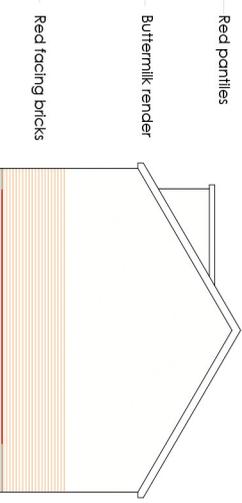
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Scale - 1:100



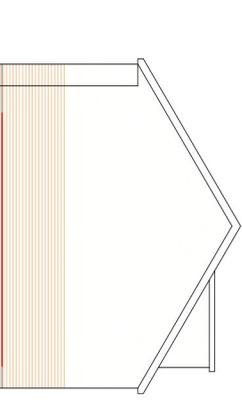
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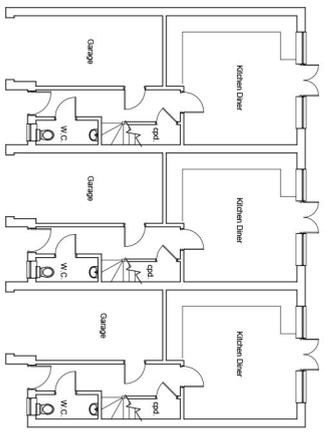
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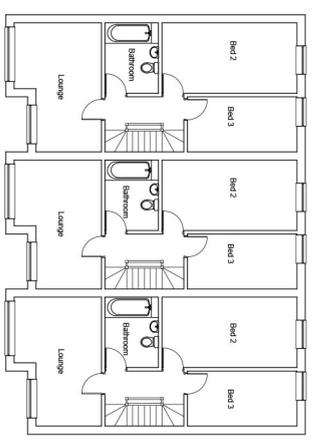
Side Elevation
Scale - 1:100



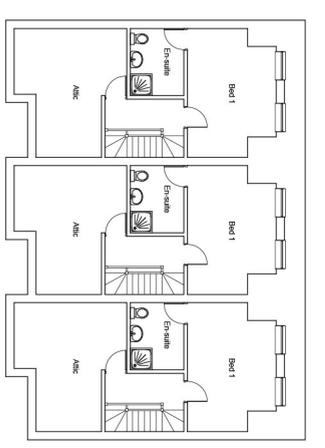
Side Elevation
Scale - 1:100



Ground Floor
Scale - 1:100



First Floor
Scale - 1:100



Second Floor
Scale - 1:100



Report to Planning applications committee
 13 October 2016

Report of Head of planning services

Subject 16/01156/F - 70 Grove Walk Norwich NR1 2QH

Reason for referral Objection

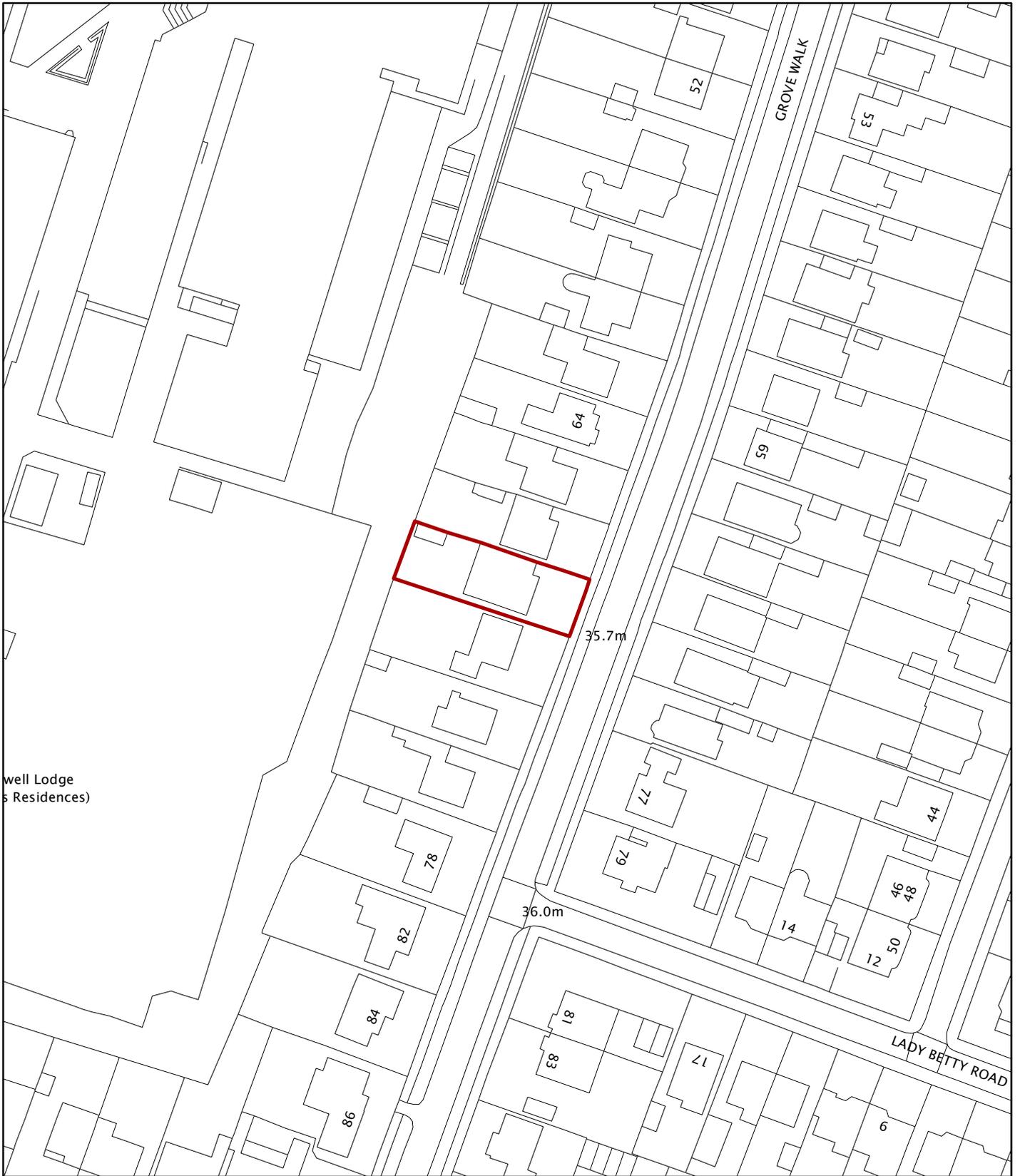
Item

4(k)

Ward:	Town Close
Case officer	Mr Samuel Walker - Samuelwalker@norwich.gov.uk

Development proposal		
Second storey side extension and extension of roof.		
Representations		
Object	Comment	Support
3		

Main issues	Key considerations
1 Design	Acceptability of design in relation to existing dwelling and form of the development.
2 Amenity	Impact on neighbours in terms of daylight, sunlight, overshadowing, outlook and over.
Expiry date	28 September 2016
Recommendation	Approve



well Lodge
(5 Residences)

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Planning Application No 16/01156/F

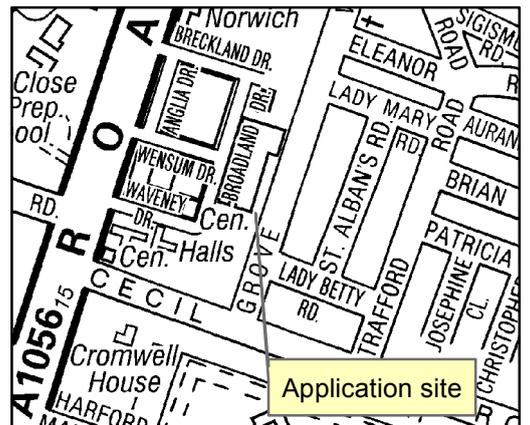
Site Address 70 Grove Walk

Scale 1:1,000



NORWICH
City Council

PLANNING SERVICES



The site and surroundings

1. The application site is located on the west side of Grove Walk. The property is a two storey detached dwelling of red brick construction.
2. This is a residential area within Norwich and is characterised by properties of this era, the design of the properties in this street all share similarities, whilst displaying individual characteristics. The properties are predominantly detached (some semi-detached) with clear separation between buildings.

Constraints

3. Critical drainage area.

Relevant planning history

- 4.

Ref	Proposal	Decision	Date
10/01003/F	Erection of a single storey side and rear extension.	APPR	16/07/2010

The proposal

5. A first floor side extension is proposed to be constructed above the existing garage to the same eaves height as the primary dwelling with a hipped roof subservient to the primary roof. The first floor extension is proposed to be clad with cementitious weatherboard finish. A hip to gable roof extension to the rear roof slope is also proposed.

Summary information

Proposal	Key facts
Scale	
Total floorspace	First floor extension 16m² approx. Second floor extension 24m² approx.
No. of storeys	2 and 3
Max. dimensions	First floor side extension: 5.41m eaves height 8.0m overall height Second floor rear extension: 8.8m approx. – ridge height to match existing roof.

Appearance	
Materials	Roof: Pantiles to match existing, velux rooflights Walls: Cementitious weatherboard finish Joinery: White uPVC to match existing

Representations

6. Advertised on site and in the press. Adjacent and neighbouring properties have been notified in writing. 5 letters of representation from three addresses have been received citing the issues as summarised in the table below. All representations are available to view in full at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Issues raised	Response
Design – (poor design, inappropriate selection of materials, not in keeping with the character of the subject property or the wider area, overdevelopment of the site.)	See main issue 1.
Amenity – (Including: daylight, sunlight, overlooking, intensification of residential use of the dwellinghouse, siting of services, disruption caused by building works.)	See main issue 2.

Consultation responses

7. Consultation responses are summarised below the full responses are available to view at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Tree Protection Officer

8. (Verbal) No trees are affected by the proposed development, the proposals are for an increase in height on the existing footprint.

The Norwich Society

9. *2 storey side extension* The extension is a good design but we are concerned over the aggregate effect on the street and the visual intrusion.

Assessment of planning considerations

Relevant development plan policies

10. **Joint Core Strategy for Broadland, Norwich and South Norfolk adopted March 2011 amendments adopted Jan. 2014 (JCS)**
- JCS2 Promoting good design

11. Norwich Development Management Policies Local Plan adopted Dec. 2014 (DM Plan)

- DM1 Achieving and delivering sustainable development
- DM2 Ensuring satisfactory living and working conditions
- DM3 Delivering high quality design

Other material considerations

12. Relevant sections of the National Planning Policy Framework March 2012 (NPPF):

- NPPF0 Achieving sustainable development
- NPPF7 Requiring good design

Case Assessment

13. Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. Relevant development plan policies are detailed above. Material considerations include policies in the National Planning Framework (NPPF), the Councils standing duties, other policy documents and guidance detailed above and any other matters referred to specifically in the assessment below. The following paragraphs provide an assessment of the main planning issues in this case against relevant policies and material considerations.
14. The principle of residential extensions is acceptable with the main issues to assess in this case being design and amenity.

Main issue 1: Design

15. Key policies and NPPF paragraphs – JCS2, DM3, NPPF paragraphs 9, 17, 56 and 60-66.
16. The proposed side extension is set back from the front elevation of the main dwelling, with a lower roof ridge height. As such it would appear as a subservient addition to the main dwelling. The extension would infill a visual gap between the adjoining dwelling and these gaps between detached dwellings are evident within the street. However whilst all properties in the street are detached, there is some variety in house types and the spacing between dwellings, with some dwellings sited close together. A small gap would still be retained between the adjacent property the north. As such the proposed extension would not have an undue terracing effect upon the character of the street.
17. The weatherboard finish of the proposed side extension has a more contemporary feel which would contrast with the traditional brick appearance of the main house. However a variety of material types are found within the street and the set-back design of the extension will reduce its prominence. Overall the detailed design and materials of the extension would ensure that it appears as a visually distinct but appropriate addition to the main building.
18. The proposed roof extension is to the rear roof slope and would not be visible from the public realm. It has been designed to provide new accommodation in the roof f without resulting in a large and bulky box dormer type extension. Roof extensions of

similar type and scale would also be permissible under householder permitted development rights. As such the proposed roof extension would not harm the appearance of the surrounding area.

Main issue 2: Amenity

19. Key policies and NPPF paragraphs – DM2, DM11, NPPF paragraphs 9 and 17.
20. The proposed side extension will decrease the separation between the subject property and 68 Grove Walk, this will result in a degree of loss of daylight and sunlight to the side windows on the first floor and to a greater extent on the ground floor of no.68. These windows are obscure glazed secondary windows to the rooms they serve. As such the loss of daylight and sunlight to these windows is not considered to be sufficient to justify refusal of this application.
21. The proposed side extension would result in removal of two existing windows in the side elevation of the parent dwelling. As such the proposals would improve any overlooking impacts in comparison to the existing situation. The proposed roof extension would allow some overlooking views into neighbouring gardens. However these views would be similar to existing from first floor windows, and could be achieved through permitted development roof extensions. As such this impact is not considered significant.
22. Concerns with regard to noise from extraction and ventilation are also noted but are unlikely to be significant for a residential property and would also be dealt with by Building Regulations. Traffic and parking issues during the construction phase would not represent a reason for refusal of the application and would not be so significant as to justify further mitigation measures.

Compliance with other relevant development plan policies

23. A number of development plan policies include key targets for matters such as parking provision and energy efficiency. The table below indicates the outcome of the officer assessment in relation to these matters.

Requirement	Relevant policy	Compliance
Sustainable urban drainage	DM3/5	The proposals would not increase areas of hard surfacing across the site and as such would not increase surface water flood risk, in accordance with policy DM5.

Equalities and diversity issues

24. There are significant/There are no significant equality or diversity issues.

Local finance considerations

25. Under Section 70(2) of the Town and Country Planning Act 1990 the council is required when determining planning applications to have regard to any local finance considerations, so far as material to the application. Local finance considerations are defined as a government grant or the Community Infrastructure Levy.

26. Whether or not a local finance consideration is material to a particular decision will depend on whether it could help to make the development acceptable in planning terms. It would not be appropriate to make a decision on the potential for the development to raise money for a local authority.
27. In this case local finance considerations are not considered to be material to the case.

Conclusion

28. The proposed extensions would not detract from the appearance of the surrounding area or the amenity of neighbouring occupiers. The development is therefore in accordance with the requirements of the National Planning Policy Framework and the Development Plan, and it has been concluded that there are no material considerations that indicate it should be determined otherwise.

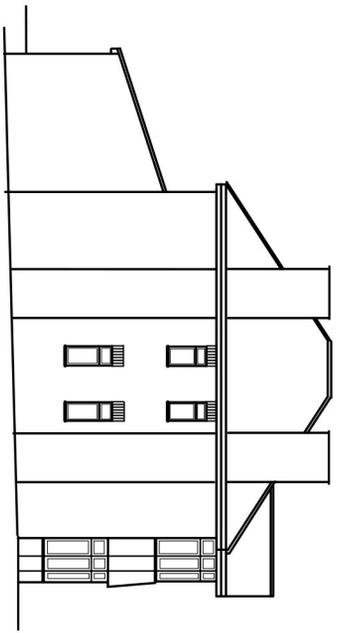
Recommendation

To approve application no. 16/01156/F - 70 Grove Walk Norwich NR1 2QH and grant planning permission subject to the following conditions:

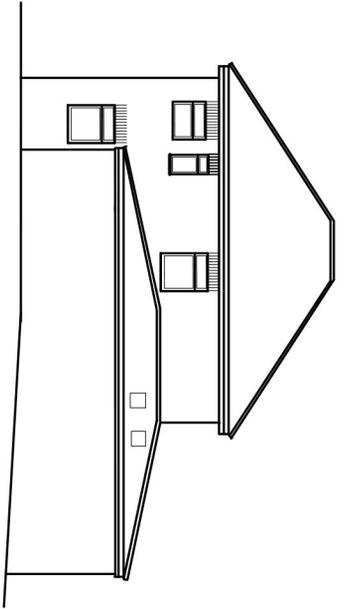
1. Standard time limit;
2. In accordance with plans;

Article 35(2) Statement

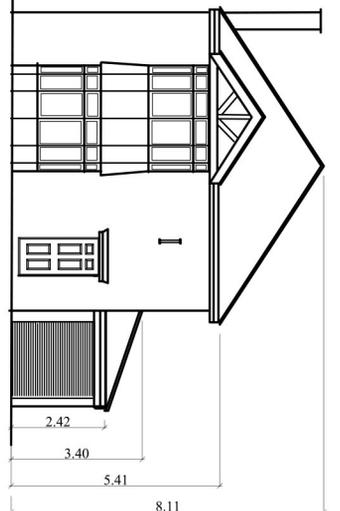
The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations and has approved the application subject to appropriate conditions and for the reasons outlined in the officer report.



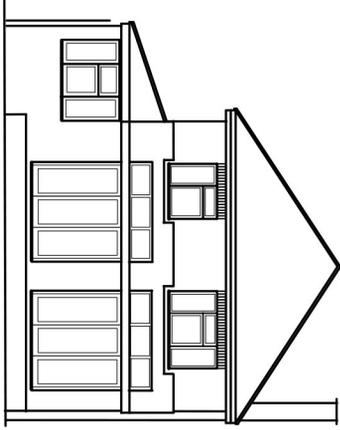
South Flank Elevation



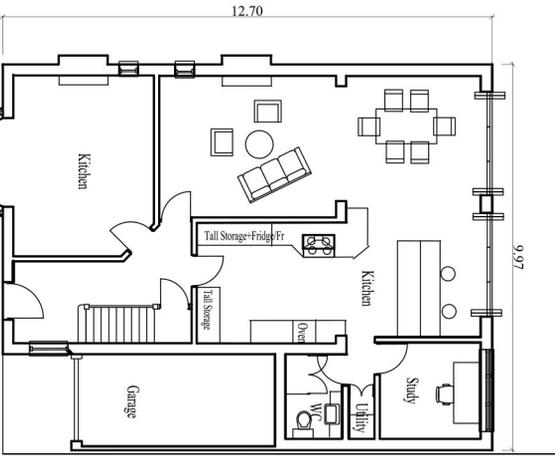
North Flank Elevation



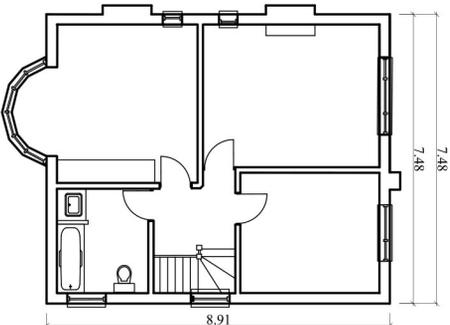
Front Elevation



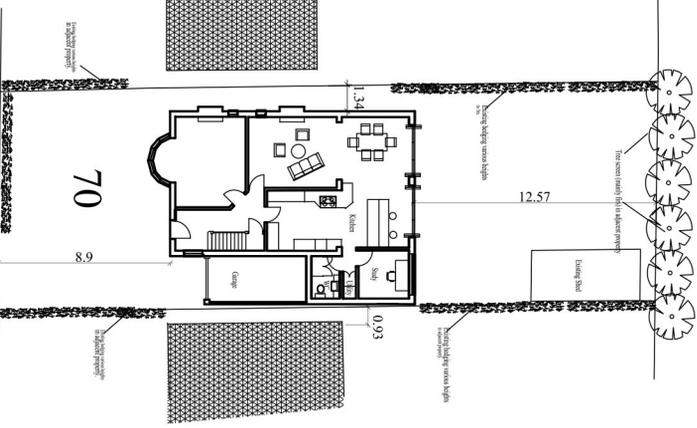
Rear Elevation



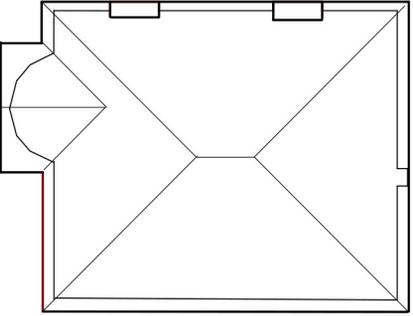
Ground Floor Plan



First Floor Plan



Grove Walk



Roof Plan

Position	Description	Date	By
A	Window in south flank removed. Chimneys reduced	20/07/16	JHB

Significant Spaces

Building Constraints
 Veneange Farm
 Veneange Road
 Salthouse NR13 6HD
 01493 50002

Project File:
 Second Storey Side Extension and Loft Extension
 70 Grove Walk, Norwich NR1 2QH

Drawings:
 Existing Plans and Elevations ;
 Site Plan

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Report to Planning applications committee
 13 October 2016

Report of Head of planning services

Subject 16/00761/F - 17 And 19 Neville Street Norwich NR2 2PR

Reason for referral Objection

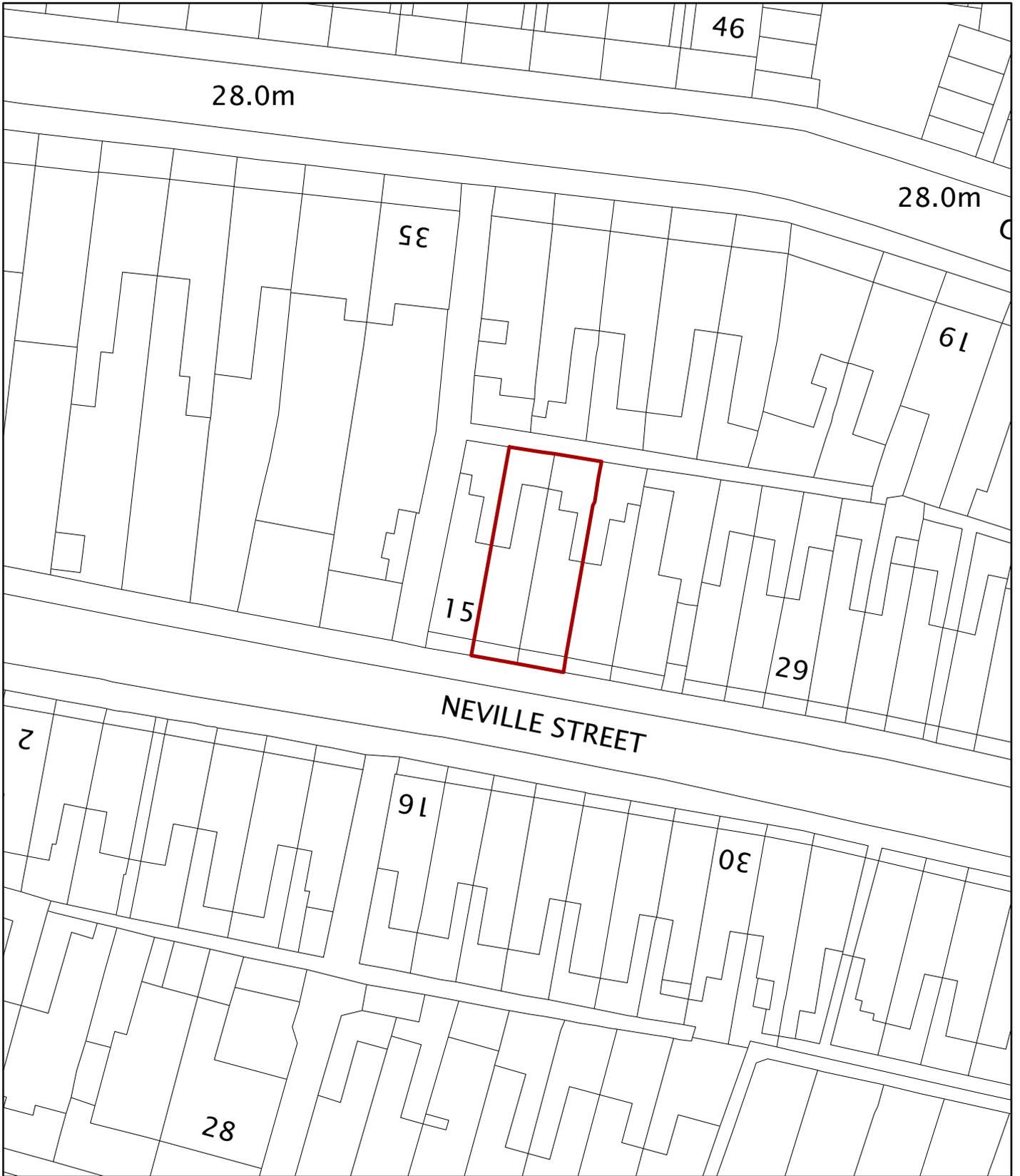
Item

4(I)

Ward:	Nelson
Case officer	Mr Stephen Polley - stephenpolley@norwich.gov.uk

Development proposal		
Replacement UPVC windows and external doors.		
Representations		
Object	Comment	Support
2	0	0

Main issues	Key considerations
1	The impact of the proposal on the character and appearance of the conservation area
Expiry date	27 July 2016
Recommendation	Approve



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Planning Application No 16/00761/F

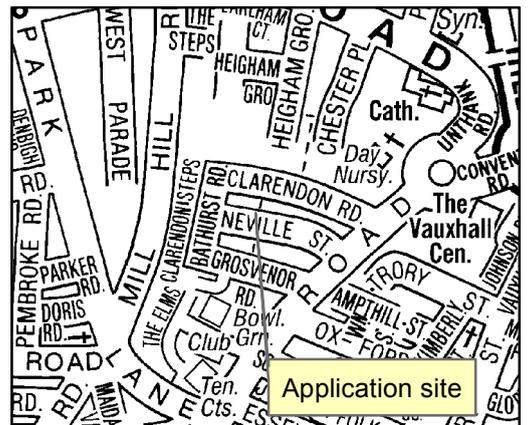
Site Address 17 and 19 Neville Street

Scale 1:500



NORWICH
City Council

PLANNING SERVICES



The site and surroundings

1. The site is located on the north side of Neville Street to the south-west of the city centre. The subject properties form part of the middle section of a small terrace of 2 storey dwellings constructed circa 1900.
2. The properties forming the terrace and similar terraces opposite have been finished in a mix of red brick, white render and all feature projecting bay windows originally finished with timber detailing. The windows on the subject property have previously been replaced with UPVC windows which do not match the original designs still present on nos. 16 and 18 opposite.

Constraints

3. The area is characterised by rows of two storey Victorian terraced houses and forms part of the Heigham Grove conservation area. Neville Street lies within subsection E of the conservation area appraisal which is described as “Medium size C19th terraces varied in character”. A specific issue mentioned within the appraisal is that of “alterations to windows and doors which introduce different styles which disrupt the visual harmony of traditional terraces within street views”.
4. The subject properties are both locally listed as are the majority of properties within the surrounding area. The subject properties and surrounding properties are also subject to an Article 4 direction which removes permitted development rights (including the replacement of windows and doors on front elevations where they front the highway). Such works require planning permission as a result.

Relevant planning history

5. There is no relevant planning history.

The proposal

6. The proposal is for the replacement of all windows and doors of both properties. These include the bay windows and first floor front facing windows to the front, 2 no. windows to the side elevations and 4 no. windows to the rear. All replacement windows are to be UPVC type. The existing front doors are to be replaced with composite doors with a design featuring 2 panels and a curved glazing section closely matching the properties opposite. The rear doors are less ornate featuring 2 panels and a rectangular glazing section.
7. It should be noted that the front facing and bay window designs have been revised during the course of the application to now feature a design which very closely matches the ornate original timber sash windows still present on the properties opposite. The windows to the sides and rear are to be of simpler sash design.

Representations

8. Advertised on site and in the press. Adjacent and neighbouring properties have been notified in writing. 2 letters of representation have been received citing the issues as summarised in the table below. All representations are available to view

in full at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Issues raised	Response
The proposed materials are out of keeping with the conservation area and article 4 direction.	See main issue 1
The proposed window design is out of keeping with the surrounding area	See main issue 1

Consultation responses

9. Consultation responses are summarised below the full responses are available to view at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Design and conservation

10. Consultation responses are summarised below the full responses are available to view at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.
11. Comments have been received from the Conservation and Design Officer who considers the additional details with regard to window design is acceptable.

Assessment of planning considerations

Relevant development plan policies

12. **Joint Core Strategy for Broadland, Norwich and South Norfolk adopted March 2011 amendments adopted Jan. 2014 (JCS)**
- JCS1 Addressing climate change and protecting environmental assets
 - JCS2 Promoting good design
 - JCS3 Energy and water
13. **Norwich Development Management Policies Local Plan adopted Dec. 2014 (DM Plan)**
- DM1 Achieving and delivering sustainable development
 - DM2 Ensuring satisfactory living and working conditions
 - DM3 Delivering high quality design
 - DM9 Safeguarding Norwich's heritage

Other material considerations

14. **Relevant sections of the National Planning Policy Framework March 2012 (NPPF):**
- NPPF6 Delivering a wide choice of high quality homes

- NPPF7 Requiring good design
- NPPF12 Conserving and enhancing the historic environment

Case Assessment

15. Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. Relevant development plan policies are detailed above. Material considerations include policies in the National Planning Framework (NPPF), the Councils standing duties, other policy documents and guidance detailed above and any other matters referred to specifically in the assessment below. The following paragraphs provide an assessment of the main planning issues in this case against relevant policies and material considerations.

Main issue 1: Design and Heritage

16. Key policies and NPPF paragraphs – JCS2, DM3, NPPF paragraphs 9, 17, 56 and 60-66. NPPF paragraphs – DM9, NPPF paragraphs 128-141. Regard has also been given to section 69 of the Listed Buildings and Conservation Areas Act 1990, which requires that the Council give special regard to the desirability of preserving and enhancing the character and appearance of the conservation area.
17. Concern has been raised that the proposed windows are made from materials and are of a design which does not match the original timber windows found in a number of neighbouring properties within the conservation area, most notably nos. 16 and 18 Neville Street located opposite. All of the proposed replacement windows are manufactured using white UPVC. It should first be noted that the windows currently in place at the two properties are not original, having been added many years after the original construction. The current windows are of UPVC construction and do not contain any of the ornate detailing present on original timber sashes found nearby.
18. It should also be noted that a number of similar properties within the conservation area have been recently been granted planning consent for the installation of replacement UPVC windows. No. 26 Grosvenor Road, no. 33 Grosvenor Road and no. 11 Essex Street are all examples of similar schemes where previously replaced windows were of an unsympathetic design which detracted from the character and appearance of the conservation area. The proposed replacement windows to the front and bay are all sash opening and are of a design closely matching the original timber windows. The windows to the side and rear are of a simpler style which similarly replicates the sash design.
19. The front doors currently in use are non-original featuring a simple design with 2 glazing sections which are at odds with the original timber designs which remain on the street. The existing doors are also in a poor state of repair providing only limited security. The replacement front doors are of composite construction and have been designed to closely match the appearance of the original timber doors, many of which are still in use. The design features 2 panels on the lower section, a centrally located integral letter box and a curved glazing panel to the top half. The replacement rear doors are also of composite construction and feature a simple design with a rectangular glazing panel. Although not of timber construction, the design of the doors are considered to be in keeping with the character and appearance of the conservation area.

Other matters

20. The following matters have been assessed and considered satisfactory and in accordance with relevant development plan policies, subject to appropriate conditions and mitigation: List relevant matters.

Equalities and diversity issues

21. There are no significant equality or diversity issues.

Local finance considerations

22. Under Section 70(2) of the Town and Country Planning Act 1990 the council is required when determining planning applications to have regard to any local finance considerations, so far as material to the application. Local finance considerations are defined as a government grant or the Community Infrastructure Levy.

23. Whether or not a local finance consideration is material to a particular decision will depend on whether it could help to make the development acceptable in planning terms. It would not be appropriate to make a decision on the potential for the development to raise money for a local authority.

24. In this case local finance considerations are not considered to be material to the case.

Conclusion

25. The proposed replacement windows and doors will not detract from the character and appearance of the conservation area.

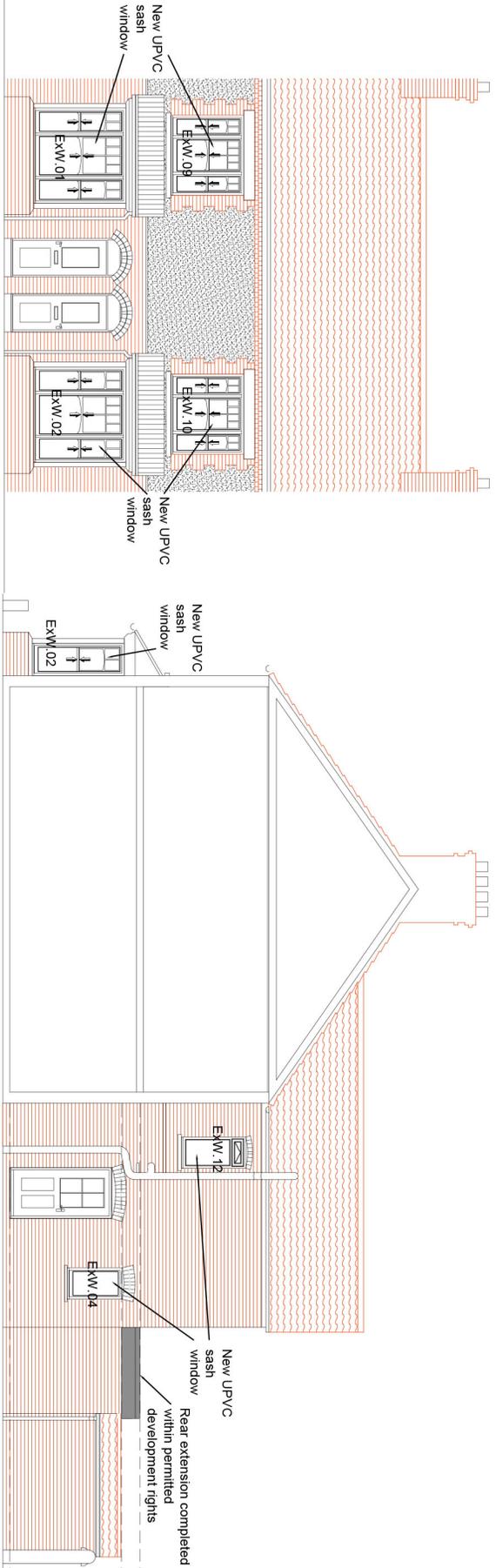
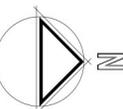
26. The development is in accordance with the requirements of the National Planning Policy Framework and the Development Plan, and it has been concluded that there are no material considerations that indicate it should be determined otherwise.

Recommendation

To approve application no. 16/00761/F - 17 And 19 Neville Street Norwich NR2 2PR and grant planning permission subject to the following conditions:

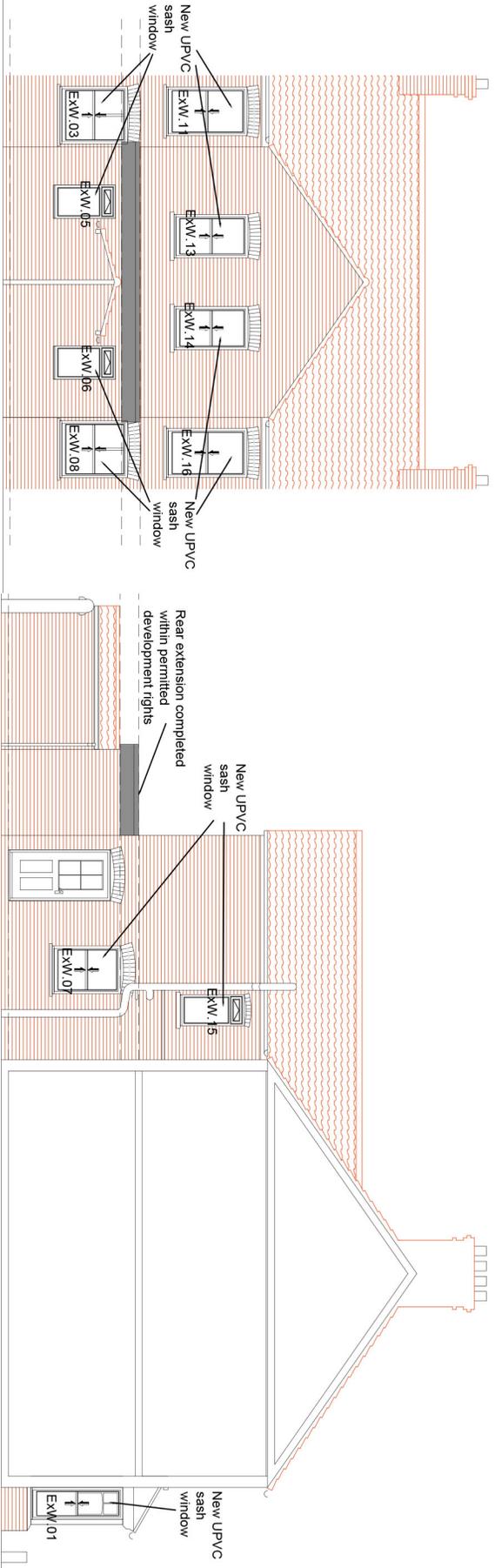
1. Standard time limit;
2. In accordance with plans;
3. All proposed windows on the front elevation of the property will be white and feature run-through sash horns, chamfered external bars, be sliding sash opening only and have the outward opening function disabled.

Article 35(2) Statement



Front Elevation @ 1:100

19 Neville Street Side Elevation @ 1:100



Rear Elevation @ 1:100

17 Neville Street Side Elevation @ 1:100



REV	DATE	DESCRIPTION	DRAWN	CHECKED BY
A	06/09/16	Bay window designs amended to be more sympathetic to surrounding buildings	DB	-
Revisions				

Planning

CF
Architects (England) Quality Standard

Chaplin Ferrett Limited
51 Yarmouth Road
Norwich
NR7 0ET
Tel: 01603 700000
Fax: 01603 700001
office@chaplinferrett.com
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Also at:
London
Manchester
Southampton
Great Yarmouth

This drawing is copyright of the architect and shall not be reproduced or used without the permission of the architect.

Check all dimensions and report any errors or omissions.
Do not scale from this drawing.

5185
PROJECT NO

004
REFERENCE NO

22/03/16
DATE

1:100 @ A3
SCALE

DB
DRAWN BY

CF
CHECKED BY

Handle Estates
17-19 Neville St. Refurbishment
Norwich

Proposed Elevations

The under Mentioned relates to general notes for guidance purposes only

BS 8213-1:2004
(Window Installation/safety and cleaning standard) Which makes reference to design, construction, operation and maintenance of windows, doors and glass partitions should be carried out and a written record of the assessment kept by the contractor/fabricator.

Essential considerations:

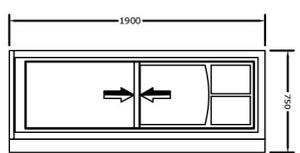
1. Design for safety in use.
2. Means of escape from fire.
3. Design for safety when cleaning (the minimum height for incorporation of transoms (safety rails in windows above floor level in high rise buildings, two storey's and above).
4. Design for safety when cleaning (the minimum height for incorporation of transoms (safety rails in windows above floor level in high rise buildings, two storey's and above).
5. Maintenance.

FIXING Note:-

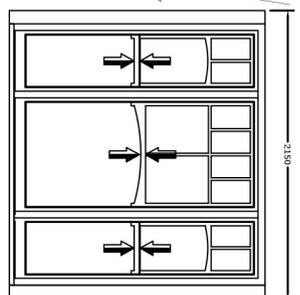
Fixing from outer frame to brickwork to be at maximum 500mm centres and at minimum 200mm from corners or transom/mullions.

Each fixing should be rated at 1.5kn shear. Fixing screws must be installed in accordance with manufacturers instructions.

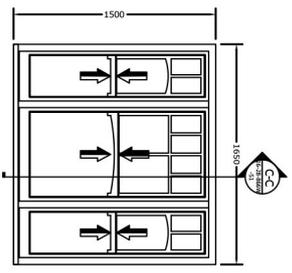
All statements, technical information and recommendations are based on experience and are given without obligation and for guidance purposes only. Their accuracy or completeness is not guaranteed. The suitability of any Rehau products for a specific application should be checked by the contractor in advance.



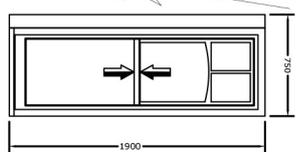
Window Types
EWW01 & EWW02
Scale 1:20



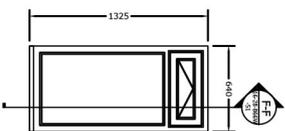
Window Types
EWW09 & EWW10
Scale 1:20



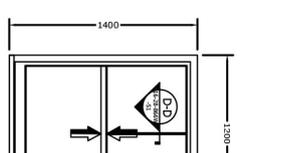
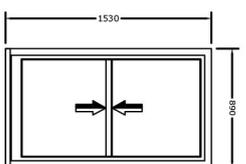
Window Types
EWW11 & EWW16
Scale 1:20



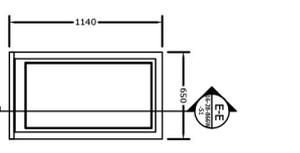
Window Types
EWW12 & EWW15
Scale 1:20



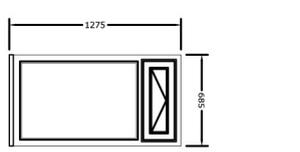
Window Types
EWW13 & EWW14
Scale 1:20



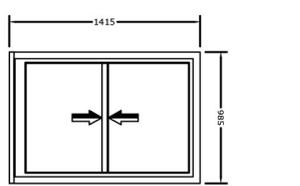
Window Types
EWW03 & EWW08
Scale 1:20



Window Types
EWW04
Scale 1:20



Window Types
EWW05 & EWW06
Scale 1:20



Window Types
EWW07
Scale 1:20

REV	DATE	DESCRIPTION	DRAWN	CHECKED BY
A	16/09/16	By window design amended to be more sympathetic to surrounding buildings	DB	-

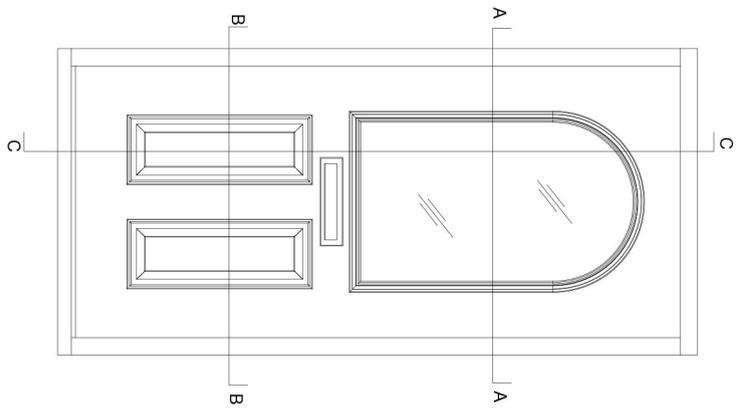
Planning

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Hendrie Estates
17-19 Newille St. Refurbishment
Norwich
Window Elevations for
17-19 Newille Street

5185
120
A
May 16
1:20 @ A1
DB

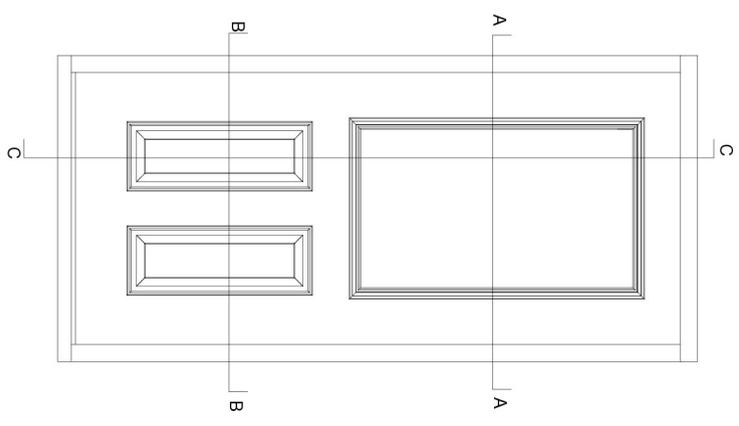
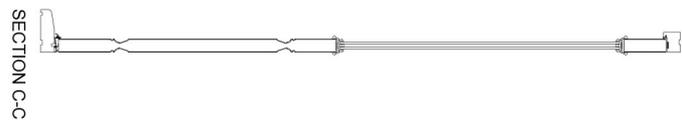
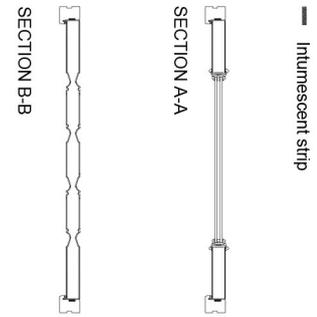
The Castle doorset range is a high quality composite door offer that is manufactured from fibreglass, and has a woodgrain finish to give that natural look of real timber.



FRONT DOOR
JELD-WEN

FRONT ELEVATION
AND SECTION
COLOUR: BLACK

FD30 FIRE CASTLE BELOIR COMPOSITE DOORSET



REAR DOOR
JELD-WEN

FRONT ELEVATION
AND SECTION
COLOUR: BLACK

FD30 FIRE CASTLE TILSBURY COMPOSITE DOORSET

REV	DATE	DESCRIPTION	DRAWN	CHECKED BY
Revisions				
CF				

Planning

Client: Hendrie Estates
Address: 17-19 Neville St. Refurbishment
 Norwich

Project: Proposed Door Elevations

Drawings: 5185

Project Ref: 122

Submission No: 03/10/16

Scale: 1:20 @ A3

Drawn by: DB

Checked by:

