

Report to Planning Applications Committee

Item

10 March 2022

Report of Area Development Manager

Subject Application no 21/00893/O World of Beds Curtis House
Curtis Road, Norwich, NR6 6RB

Reason for referral Objections

4a

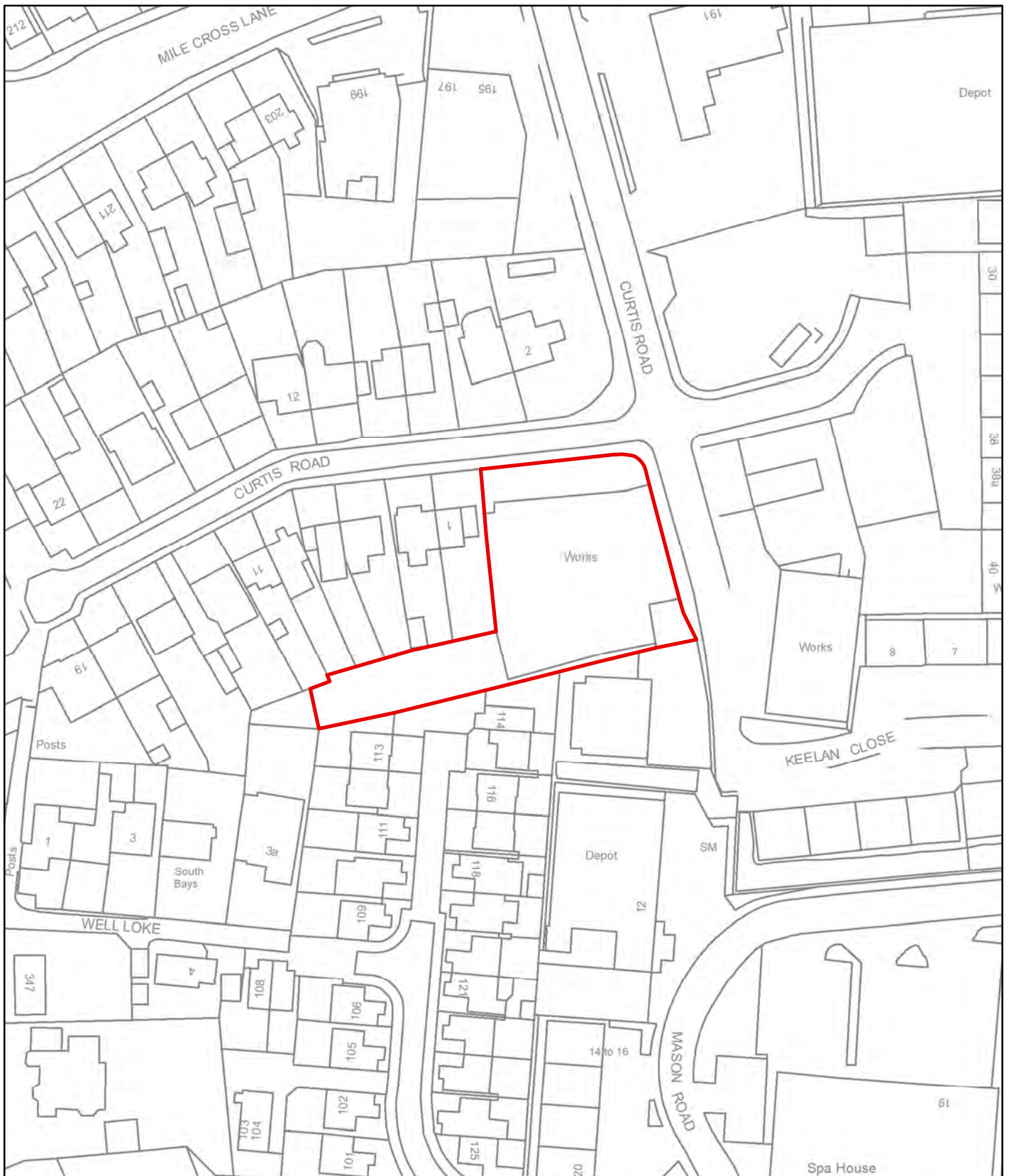
Ward	Catton Grove
Case officer	Maria Hammond mariahammond@norwich.gov.uk
Applicant	Mr Patrick Darcy

Development proposal		
Outline application including matters of access and layout for demolition of existing buildings and re-development of the site with 7 houses (mix of 2 & 3 bed).		
Representations		
Object	Comment	Support
9	0	0

Main issues	Key considerations
1	Principle – loss of retail use and development of housing
2	Design
3	Amenity – of future and neighbouring occupiers
4	Transportation
5	Ecology and trees
6	Contamination
Expiry date	15 March 2022
Recommendation	Approve

The site and surroundings

1. The site occupies land at the corner of Curtis Road, a road of mixed character off the outer ring road.
2. The road extends south off Mile Cross Lane with a defined employment area on the eastern side and at the southern end within which uses include a furniture retailer, a used car dealers and an engineering company.
3. A branch of Curtis Road extends to the west and is lined by detached and semi-detached mid-twentieth century two storey dwellings and bungalows on each side. This road also gives vehicular access to the rear of a parade of shops on Aylsham Road.



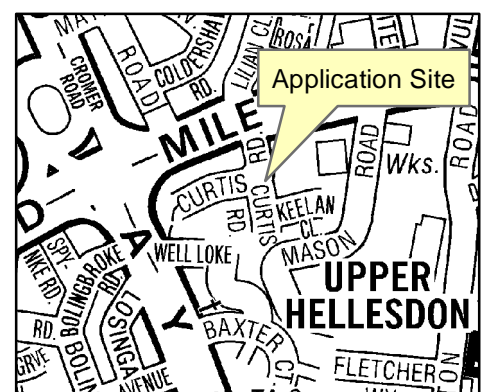
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Planning Application No 21/00893/O
 Site Address World of Beds Curtis Road

Scale 1:1,000



NORWICH
 City Council
 PLANNING SERVICES



4. The character of the residential development along Curtis Road is distinctly suburban. The mix of semi- and detached dwellings set back from the road by front gardens and with generous rear gardens results in a density consistent with the suburban character. To the southwest, the site borders a denser area of semi-detached two storey dwellings at the end of Baxter Court, accessed off Aylsham Road.
5. The application site is occupied by a single storey building covering almost the entire footprint. This was last in retail use. It is served by a hard surfaced informal parking area to the north with two vehicular access points and a further gated access at the southwest corner. A mix of flat and pitched roofs cover the single storey building that has evolved and been extended over a number of years. The walls have a mix of cladding, blockwork and facing brick.
6. The site is 'L' shaped with a finger of open space extending to the west at the rear of dwellings on Curtis Road and to the side of dwellings at the northern end of Baxter Court.

Constraints

7. The site is within a critical drainage catchment.
8. A defined employment area exists to the east and south.

Relevant planning history

9. The records held by the city council show the following planning history for the site.

Ref	Proposal	Decision	Date
95/00644/F	Extension to rear to provide additional storage and warehouse facility	Approved	01/08/1995
97/00744/F	Extension to provide additional storage facility	Approved	03/02/1997
0100665F	Retrospective application for cladding to north elevation of premises	Approved	19/07/2001
21/00040/O	Outline application including matters of access and layout for demolition of existing buildings and re-development of the site with 9 houses (mix of 2 & 3 bed).	Refused	15/02/2021

The proposal

10. It is proposed to demolish the existing buildings across the site and outline permission is sought for seven new dwellings. Matters of access and layout for these seven dwellings are to be considered, all other matters (scale, appearance and landscaping) are reserved and would need to be subject of subsequent applications.
11. Two pairs of semis would front Curtis Road to the north and three of these dwellings would have direct vehicular access off the road to parking spaces.

12. A car park serving the remaining dwellings would be accessed off Curtis Road to the east with a further pair of semis at the rear of this and a larger detached dwelling at the western extent of the site.
13. The dwelling at the northeast corner of the site would have a carport providing two tandem spaces along its eastern boundary and the boundaries to the car park area would be formed of walls 2.5m-2.8m in height. Both the carport structure and these walls are proposed to mitigate noise as discussed in the assessment below.

Proposal	Key facts
Scale	
Total no. of dwellings	Seven (five no. three bedroom and two no. two bedroom)
No. of affordable dwellings	None proposed or required by policy.
Total floorspace	770sqm
No. of storeys	Two and two and a half
Density	43 dwellings per hectare
Transport matters	
Vehicular access	Three dwellings accessed directly off Curtis Road to the north and a shared access for the other four from the east
No of car parking spaces	14 in total, 2 per dwelling
No of cycle parking spaces	Space in each garden
Servicing arrangements	Bin storage identified in gardens, collection by highway

Representations

14. Advertised on site and in the press. Adjacent and neighbouring properties have been notified in writing and there have been three consultation periods in total as the proposal has developed and been amended. Nine contributors have provided letters of representation citing the issues as summarised in the table below. All representations are available to view in full at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Issues raised	Response
Overlooking and overshadowing of gardens	See main issue 3
Loss of privacy	See main issue 3
Precedent to turn industrial site to residential. Conflict between users may prevent established neighbouring business operating.	See main issue 1, there is no policy objection to the principle of residential use in this location. Access and amenity issues between different users considered in issues 3 and 4 below.
Conflict and danger between pedestrians, cyclists, children playing outside, cars and lorries and forklifts accessing neighbouring industrial use. Access should all be from existing residential section of Curtis Road.	See main issue 4

Issues raised	Response
Poor access for emergency vehicles and bin lorries	See main issue 4
Where do visitors park?	See main issue 4
Traffic congestion	See main issue 4
Sun study does not shadows in January and December when light levels lowest and shadows longest	See main issue 3
Plot 7 should have hipped roof	Scale and appearance are reserved matters. The submitted roof plans are indicative only. Amenity is considered at main issue 3 below.
Poor amenity and outlook and small gardens proposed	See main issue 3
Long drive is amenity space lost to concrete	See main issue 2
Overcrowded, too cramped and doesn't respect building line	See main issue 2
Noise report taken when production reduced	See main issue 3, an additional report has been submitted
Noise pollution from added traffic, building works and additional neighbours	See main issue 3
Concern about future residents right to complain	See main issue 3 – an informative note is recommended
Risk of contamination and subsidence. Hazards from asbestos and lead paint. Risk to air quality during demolition.	See main issue 6
Disturbance to wildlife and destruction of trees	See main issue 5
Query position of doors	The position of all doors and windows would be considered at the reserved matters stage.
Neighbouring dwellings have intention to extend at rear – concern about proximity and loss of light from proposal	See main issue 3
Plot 7 may be built on again at some stage	The application as proposed needs to be determined. Any future additional development would be considered on its own merits.
Removal of boundary wall would damage plants	Works to shared boundaries should be agreed privately and are not a material planning consideration
Object to re-numbering of property	Not a material planning consideration
Impact on property value and future plans for properties	Not a material planning consideration

Consultation responses

15. Consultation responses are summarised below the full responses are available to view at <http://planning.norwich.gov.uk/online-applications/> by entering the application number.

Environmental protection

16. The proposed development, is for residential properties on a former industrial site, adjacent to other industrial sites which are a source of potential noise nuisance. The potential for contaminated land is considered as well as the potential for noise nuisance to the proposed development. I recommend the following conditions (summarised):

- No occupation of the dwellings shall take place until the habitable rooms have been provided with proprietary sound-insulating ventilators
- Scheme to manage risks associated with contamination
- If contamination not previously identified is found, submission of scheme detailing how this shall be dealt with
- Imported topsoil and subsoil

17. Informative notes (summarised):

- This property is in a situation with potential for significant background noise arising from nearby uses. Norwich City Council has therefore included measures designed to control noise in the planning permission for this property. These requirements are to provide approved acoustic wall treatment, glazing and passive/forced acoustic ventilation and other noise mitigation measures. The use of these will be taken into account by Norwich City Council when investigating any complaint of noise nuisance from an occupier of these dwellings.
- The developer is reminded that prior to any refurbishment commencing on site the building/s to be refurbished are required to be surveyed for the presence of asbestos containing materials in accordance with the Control of Asbestos Regulations 2012.

Highways (local)

18. I now have no objections in principle subject to use of recommended conditions.

19. The former commercial use of the site generated its own amount of traffic demand, so it would be difficult to substantiate an objection for a residential use of this minor scale.

20. The proposed means of access and layout is considered acceptable in highway terms and I understand that Building Control have been consulted with regard to fire service access to plot 7 that is the deepest within the site.

21. Given the amount of highway works entailed I recommend that a s278 (Small Highway Works Agreement) is undertaken to facilitate the reconstruction of footways, vehicle crossovers, footway crossing points and associated reinstatement of waiting restrictions, repositioning of a street light and retention of the street nameplate.

22. The proposal will create new vehicle accesses to Curtis Road which will need to be constructed to a TRAD 1 standard. The southern part of the site comprises of a shared vehicle access to the Curtis Road spur to the industrial estate area, this will create a parking court and connection to a long private drive and with a turning

head which is acceptable. The vehicle access will need to be constructed to a TRAD 1 standard.

23. A tandem parking garage for Plot 4 is now proposed which is acceptable. The rest of the site layout is acceptable in highway terms with regard to parking provision and layout.
24. It may be necessary for the streetlight to be re-sited at the applicants cost. Vehicle crossovers will require drainage to intercept water run off. During construction, any hoardings used over adjacent footways will require a permit. It will also be necessary during construction for contractor parking details to be arranged.
25. Any damage that may occur as a result of the vehicle access works will require reinstatement by the applicant at their cost. Also there is street furniture that will need to be protected during the construction period.
26. Conditions recommended for parking and turning areas to be constructed prior to occupation, construction parking, detailed drawings for highway works and their subsequent completion.

CNC Building Control

27. Haven't any comments to make except for fire brigade access to plot 7. We need compliance with Building Regulations: Fire brigade access to the dwelling will be required - access route that has a minimum width between kerbs of 3.7m, any gateways to be a minimum of 3.1m wide, carrying capacity of the route should be at least 12.5 tonnes and provision for turning should be made if the route exceeds 20m in length. Vehicle access should be provided to within 45m of all points within the dwelling house.

Fire Service

28. I do not propose to raise any objections providing the proposal meets the necessary requirements of the current Building Regulations 2010 – Approved Document B (volume 1&2 – 2019 edition) as administered by the Building Control Authority. Also consideration should be given to B5 Section fire mains and hydrants and Section 16 emergency vehicle access.

Ecologist

29. I have no objection to this application. The report is completed by competent professionals. However no mitigation or enhancement measures are proposed. As such I would suggest the following conditions: Small mammal access and enhancements

Citywide Services

30. Bins will need to be presented at the edge of the boundary/highway to be emptied. In the case of Number 7 and others the bins would need to be presented at the edge of the private drive. Biffa would not collect and return the bins to the storage sheds.

Norwich Society

31. The reduction in the number of units has resulted in an improved lower density scheme, and we have no objections. No comments to add on amended plans.

Tree protection officer

32. No objections from an arboricultural perspective, however, applying condition TR7 (works on site in accordance with AIA/AMS/TPP) would be appropriate, in order to avoid potential damage to T2.

Assessment of planning considerations

Relevant development plan policies

33. **Joint Core Strategy for Broadland, Norwich and South Norfolk adopted March 2011 amendments adopted Jan. 2014 (JCS)**

- JCS1 Addressing climate change and protecting environmental assets
- JCS2 Promoting good design
- JCS3 Energy and water
- JCS4 Housing delivery
- JCS5 The economy
- JCS6 Access and transportation

34. **Norwich Development Management Policies Local Plan adopted Dec. 2014 (DM Plan)**

- DM1 Achieving and delivering sustainable development
- DM2 Ensuring satisfactory living and working conditions
- DM3 Delivering high quality design
- DM5 Planning effectively for flood resilience
- DM6 Protecting and enhancing the natural environment
- DM7 Trees and development
- DM11 Protecting against environmental hazards
- DM12 Ensuring well-planned housing development
- DM16 Supporting the needs of business
- DM18 Promoting and supporting centres
- DM30 Access and highway safety
- DM31 Car parking and servicing

Other material considerations

35. **Relevant sections of the National Planning Policy Framework July 2021 (NPPF):**

- NPPF2 Achieving sustainable development
- NPPF5 Delivering a sufficient supply of homes
- NPPF6 Building a strong, competitive economy
- NPPF9 Promoting sustainable transport
- NPPF11 Making effective use of land
- NPPF12 Achieving well-designed places
- NPPF14 Meeting the challenge of climate change, flooding and coastal change
- NPPF15 Conserving and enhancing the natural environment

36. Supplementary Planning Documents (SPD)

- Landscape and trees SPD adopted June 2016

37. Advice Notes and Guidance

- Water efficiency advice note October 2015
- Internal space standards information note March 2015
- Accessible and adaptable dwellings standards October 2015

Case Assessment

38. Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. Relevant development plan policies are detailed above. Material considerations include policies in the National Planning Framework (NPPF), the Councils standing duties, other policy documents and guidance detailed above and any other matters referred to specifically in the assessment below. The following paragraphs provide an assessment of the main planning issues in this case against relevant policies and material considerations.

Main issue 1: Principle of development

39. Key policies and NPPF paragraphs – JCS4, DM12, DM16, DM18, NPPF sections 5 and 11
40. The proposal would result in the loss of the existing large retail unit. This is not within a defined centre or within the boundary of the adjacent employment area and is inconsistent with the surrounding residential and employment uses. This use in this location does not benefit from any policy protection and its loss is therefore acceptable in principle.
41. Furthermore, the retail unit is comprised of an ad hoc accumulation of low rise and low quality structures across the site which makes no positive contribution to the character and appearance of the area. Redevelopment of the site offers an opportunity to improve the appearance and make a more efficient and effective use of the land.
42. The site is not subject to any allocation, policy designation or other exception listed in Policy DM12 and new residential development is therefore acceptable in principle, subject to the considerations below.
43. A representation has raised concern about the proposal setting a precedent to turn industrial sites here into residential. Any residential proposal within the defined employment area to the south and east of the site would conflict with Policies DM12 and DM16 and not be acceptable in principle. As the application site is not within this employment area, approval of the proposal would not set any undesirable precedent that may prejudice the retention of existing employment uses which are within it.

Main issue 2: Design

44. Key policies and NPPF paragraphs – JCS2, DM3, NPPF section 12

45. Layout is the only aspect of design to be considered in this outline application. The precise scale, appearance and landscaping of the dwellings would need to be considered through a reserved matters application(s).
46. This application does, however, indicate the dwellings would be two and two and a half storeys in height. In principle, this would be consistent with the prevailing character of residential development to the west and southwest so would not be unacceptable, subject to detailed consideration at reserved matters stage and the amenity considerations outlined below.
47. Provision of a mix of semi- and detached dwellings with two and three bedrooms reflects local character. The density is slightly higher than the neighbouring dwellings on Curtis Road which have generous gardens, but similar to the more modern dwellings on Baxter Court to the southwest. Furthermore, the mixed use of the area surrounding the site means the residential character is not so strong or distinctive that the proposed density would cause any harm to local character and seven dwellings across this site is considered to represent an appropriate and efficient use of the land. It should be noted that a previous proposal for nine dwellings was considered unacceptable (21/00040/O).
48. With regards layout, the dwellings fronting Curtis Road to the north would sit approximately 1 metre forward of the prevailing building line. To push them any further back would compromise the rear garden space and the slight difference in position relative to neighbouring dwellings would not appear incongruous, especially given the position at the end of the road. This siting allows space for off-street parking and some soft landscaping to the front and private gardens to the rear; an arrangement typical of the rest of Curtis Road. Whilst these gardens would be smaller than those existing along the road, they would not be inappropriate in scale or harmful to local character.
49. The proposed layout also allows space for some soft landscaping around the corner plot which could be designed to significantly enhance this prominent road junction for the benefit of the local area. The eastern road frontage is compromised by the siting of the carport to plot 4 and 2.5 metre high boundary wall which are proposed as a necessary part of the noise mitigation measures required to protect the proposed dwellings from the impacts of the engineering firm to the southeast, as considered below.
50. Whilst the precise appearance would not be agreed until reserved matters stage, it is considered the prominent position on the eastern road frontage, proximity of the carport to the tapering road boundary and height of the wall either side of the car park opening would be somewhat incongruous and dominant in the streetscene and visually separate these dwellings from their surroundings. In this respect, the scheme would fail to maximise the opportunity redevelopment of this site represents to enhance the appearance of this site. However, the carport and wall are necessary to mitigate noise impacts to the new dwelling and the layout proposed does incorporate space for soft landscaping to soften and mitigate the appearance of these built structures. Furthermore, there are only commercial and industrial uses east and south of the site and there would be no harm to the character and appearance of the employment area.
51. The three plots accessed from the east are all set back from the road by shared access and parking for these three dwellings and the corner plot. The 'L' shaped

nature of the site means the southernmost dwelling is set back approximately 44 metres from the road and accessed by a private drive along the southern site boundary. Whilst providing a dwelling here makes efficient use of this part of the site, it is compromised by the distance from the road as considered further below in the Transport section.

52. In design terms, the layout would provide seven new dwellings in a form that would allow each to have off-street parking, adequate private rear gardens with space for cycle and refuse storage and a mix of semi- and detached dwellings that would contribute to a mix of housing in a form that would sufficiently respect the existing suburban residential character. This is a constrained site and the unusual shape and relationship with neighbouring uses has compromised the scheme in some respects, but not to any unacceptable extent, and overall the layout is considered acceptable.

Main issue 3: Amenity

53. Key policies and NPPF paragraphs – DM2, DM11, NPPF paragraphs 130 and 185-187

Amenity for future occupiers

54. The dwellings are all indicated to comply with minimum internal space standards required by Policy DM2.
55. As floor plans have not been submitted at this stage, it is not possible to assess the overall standard of amenity, access to natural light, outlook, *etc.* in detail. This will be considered at reserved matters stage and the layout proposed would not prevent a satisfactory detailed design coming forward.
56. Externally, each dwelling would have a private rear garden. These are significantly smaller than those along Curtis Road to the west, but comparable to those on Baxter Court to the southwest. They would provide modest outdoor space which would be compromised to some extent by traffic and industrial noise and overshadowing.
57. A submitted daylight test diagram identifies that the position of two storey dwellings within plots 5 and 6 south of plots 2 and 3 would result in some overshadowing at the rear of plot 2 but that use of a hipped roof could help mitigate this. The roof form and position and size of window and door openings shall need to be subsequently determined at reserved matters stage and the amenity impacts considered in detail then, however it does appear that there would be a design solution to satisfactorily mitigate any unacceptable overshadowing or loss of light to the garden and internal accommodation.
58. The application was originally supported by an assessment of noise levels taken during a national covid lockdown in April 2021. The results included a tolerance for reduced activity and journeys, but a representation identified a neighbouring engineering premises was operating at a significantly reduced capacity at the time. An additional survey was undertaken in October 2021 in cooperation with this company during their 'normal' working conditions and informed by a site-based study of their operations which include: forklift movements, multiple machines operating simultaneously in buildings east and south of the site and goods vehicles arriving between 06:00 and 22:00. A worst-case scenario was developed which was

found to result in an adverse impact on the nearest proposed garden during daytime hours and a significant adverse impact on the first floor of the closest dwelling's façade during night time hours. The site is also affected by road traffic noise.

59. It is therefore necessary for these noise impacts to be mitigated to provide an acceptable noise environment for the internal accommodation and external amenity spaces.
60. To protect habitable rooms, façade insulation with acoustically rated windows and trickle vents, plus mechanical extract ventilation is proposed to sufficiently mitigate internal noise levels to a 'low impact'. When the internal layout is developed in due course, the noise report recommends habitable rooms, and specifically bedrooms, face north and west wherever possible, away from the sources of industrial noise.
61. The rear garden of plot 4 is the closest external space to be affected by the noise, so a carport structure 3.07m high is proposed along its eastern boundary with a wall 2.5m high along the remainder of the eastern boundary either side of the car park opening. A section of the southern boundary is proposed to be a 2.8m high solid brick wall. These structures would form a barrier to the industrial noise from the south and east and reduce the daytime noise level to within a desirable range.
62. These internal and external measures are considered necessary to provide future occupants with an acceptable standard of amenity in respect of noise impacts and to integrate this development effectively into this mixed use without compromising the established operations of industrial and commercial neighbours. The need for the carport and high boundary walls is considered to outweigh any visual harm these may have. A condition should ensure that the matters reserved for subsequent approval are designed to comply with the recommendations of the noise report and subsequently implemented and maintained for the lifetime of the development. In addition, an informative note should draw future occupiers' attention to the local noise environment and how any future noise complaints would be dealt with.

Neighbour amenity

63. Objections have raised concern about overshadowing and loss of light to neighbouring dwellings.
64. Submitted diagrams indicate where a 25 degree line from ground floor windows of the closest neighbouring dwellings would intersect with the development. In accordance with Building Research Establishment guidance, daylight and sunlight levels would not be adversely affected if the development would not disrupt this line as it would allow light to continue to reach these windows. The diagrams illustrate the southwest corner of the roof of a two storey gabled roof dwelling on plot 7 would have a small impact on one front window to the nearest dwelling on Baxter Court. This shall need to be more fully assessed at reserved matters stage as the scale and form of the dwelling has only been approximated in this outline application, but the diagram identifies the layout of the proposed dwellings, which is to be determined in this application, would not result in significant or unacceptable loss of daylight to the windows of neighbouring dwellings.

65. Diagrams indicating the extent of shadows from the indicative scale and form of the proposed dwellings at 12 noon and 3pm in August have been submitted. These identify that there would be some overshadowing from the dwelling on plot 7 to the very southern extent of the 15 metre long rear gardens to 1 and 3 Curtis Road. In August, the sun is relatively high in the sky and the impact would be greater at other times of the year but these are generous gardens and it is not considered likely that there would be shadows across the whole gardens or reaching the dwellings at any time of day or year. Again, this shall need to be assessed more fully when the scale (height) and form of each dwelling is proposed at reserved matters stage, but there is sufficient information to suggest that the proposed layout would not result in any unacceptable overshadowing to neighbouring gardens,
66. Objections have also raised concern about overlooking. As appearance is not a matter to be considered in this application, the size and position of window openings and resulting overlooking and loss of privacy can only be fully assessed at reserved matters stage. However, the layout does suggest there would be windows in elevations facing towards other dwellings within and outside the development and this shall need to be carefully considered in due course.
67. Two representations have raised concern that the proposal would harm the amenity of extensions they are planning to their properties using permitted development rights. Given that these have not yet been built and are not subject to planning permissions with approved plans, it is not currently known what form or layout they may have and a detailed assessment cannot be made. Therefore, no weight can be attached to their potential future construction. Should they be implemented or be subject to an express permission when reserved matters are considered, the proposals shall be assessed accordingly. It is not considered that the potential for neighbouring dwellings to be extended in future represents any conflict with or constraint on the proposed development of this site.
68. Potential harm from noise, traffic movements, parking and pollution during construction can be satisfactorily managed by agreeing a method statement by condition.

Main issue 4: Transport

69. Key policies and NPPF paragraphs – JCS6, DM28, DM30, DM31, NPPF section 9
70. Access is a matter to be considered and the proposal provides access to off-street parking to plots 1-3 off the western Curtis Road spur, to plots 4-6 via a shared parking and turning area to the south and a private drive off this area to plot 7.
71. It is noted that representations have raised concern about the potential for conflict between residents and the size and frequency of vehicles serving the adjacent employment uses. The Highway Authority have no objection and the layout makes adequate provision for turning to plots 4-7 so vehicles can enter and exit the site in a forward gear on this busier section of the 20 mph road.
72. Each dwelling would be served with two parking spaces in accordance with standards. The carport proposed to plot 4 would provide tandem parking which is sub-optimal but makes use of the space available and necessity of a substantial boundary structure for noise mitigation and is not unacceptable for a single

dwelling. The architect has confirmed the carport would have a minimum internal space of 3m by 14m.

73. Parking elsewhere along Curtis Road is managed with double and single yellow lines and one hour parking bays which provide space for visitors to the area so it is not considered any overspill parking would create a hazard on the road. The proposed layout includes sufficient space for bin and cycle storage. Bins would need to be moved to the highway edge for collection.
74. There would be suitable access for emergency vehicles to all plots, except plot 7. Building Regulations stipulate minimum requirements for private driveways and the constraints of the site size and layout prevent compliance for the drive to plot 7. As an alternative, the applicant has been asked to consider omitting plot 7 and reorganising the layout. They have declined to do so and it is acknowledged that providing a dwelling on plot 7 is an efficient and effective use of this brownfield land in accordance with paragraphs 119 and 120(c) of the National Planning Policy Framework. The Fire Service have advised that Building Regulations would need to be complied with. As the Regulations concerning fire appliance access are unlikely to be achievable, alternative fire safety measures, such as sprinklers, would need to be employed and approved by Building Control.
75. Necessary highway works identified by the Highway Authority have been incorporated in the proposed plan and the detailed design should be agreed by condition. Subject to this and additional conditions securing parking and turning space, cycle and bin storage, construction parking (within the construction method statement) and informative notes concerning works required in the highway, the proposed access and layout are acceptable with regards transport requirements.

Main issue 5: Ecology and trees

76. Key policies and NPPF paragraphs – JCS1, DM7, NPPF section 15
77. An ecology assessment has been submitted which finds the existing buildings to have negligible bat potential and the site and surrounding area to be sub-optimal for foraging and commuting bats.
78. No other mammals were identified and whilst birds were seen on and around the site during the survey, the buildings are assessed to not be suitable for bird nesting.
79. It is noted that representations have raised concern about the impact on wildlife but the assessment concludes that no protected species or habitats would be impacted by the development. The scheme should enhance opportunities for biodiversity and this can be secured by condition.
80. An arboricultural impact assessment has been submitted which confirms there are no existing trees on the site (although it is understood there have been in the past) but there is one individual and four groups adjacent to the site which are all classified as low quality category C.
81. These could all be retained and an arboricultural method statement proposes constructing parking spaces adjacent to one with a no dig method and cellular confinement system to protect it. A condition securing compliance with this is necessary to ensure these off-site trees are not harmed by the development.

Main issue 6: Contamination

82. Key policies and NPPF paragraphs – DM11, NPPF paragraphs 183-184
83. Environmental Protection have identified that historic use of the site and the construction of the existing buildings raises for potential for contaminated land and asbestos to be present. A contamination investigation and any necessary remediation and monitoring should be required by condition to satisfactorily manage this risk and an informative note can advise of this risk of asbestos.

Compliance with other relevant development plan policies

84. A number of development plan policies include key targets for matters such as parking provision and energy efficiency. The table below indicates the outcome of the officer assessment in relation to these matters.

Requirement	Relevant policy	Compliance
Water efficiency	JCS1 & JCS3	Yes subject to condition
Sustainable urban drainage	DM3 & DM5	The proposal will reduce the impermeable area across the site and the application suggests either soakaways or attenuated storage would be used. A detailed scheme for the site should be agreed by condition

Equalities and diversity issues

85. There are no equality or diversity issues.

Local finance considerations

86. Under Section 70(2) of the Town and Country Planning Act 1990 the council is required when determining planning applications to have regard to any local finance considerations, so far as material to the application. Local finance considerations are defined as a government grant or the Community Infrastructure Levy. Whether or not a local finance consideration is material to a particular decision will depend on whether it could help to make the development acceptable in planning terms. It would not be appropriate to make a decision on the potential for the development to raise money for a local authority. In this case local finance considerations are not considered to be material to the case.

Conclusion

87. The application proposes redeveloping a vacant out-of-centre retail site with seven new dwellings. There is no policy objection to the loss of the existing use nor to the principle of residential development.
88. Matters of access and layout are to be considered and the proposal provides suitable means of vehicular and pedestrian access without any detriment to highway safety and the layout makes appropriate provision for car and cycle parking.
89. Redevelopment offers an opportunity to enhance the appearance of this prominent corner site and put the land to a more efficient and beneficial use. The site is

constrained by its irregular shape, surrounding noise sources and position between residential and employment uses but the proposed layout and density is considered to satisfactorily respond to these. The necessary external noise mitigation measures include a carefully positioned carport and high boundary walls which may otherwise not be considered appropriate, but they are required to protect occupiers from unacceptable noise impacts and reserved matters of appearance and landscape can be designed to satisfactorily mitigate any detrimental visual impacts.

90. It is not considered the means of access and layout proposed for the development of seven dwellings would result in any unacceptable impacts on neighbour amenity, highways, ecology, trees, contamination or drainage, either during construction or occupation of the development, which cannot be satisfactorily resolved either by condition on this outline permission or through consideration at reserved matters stage.
91. The development is therefore in accordance with the requirements of the National Planning Policy Framework and the Development Plan, and it has been concluded that there are no material considerations that indicate it should be determined otherwise.

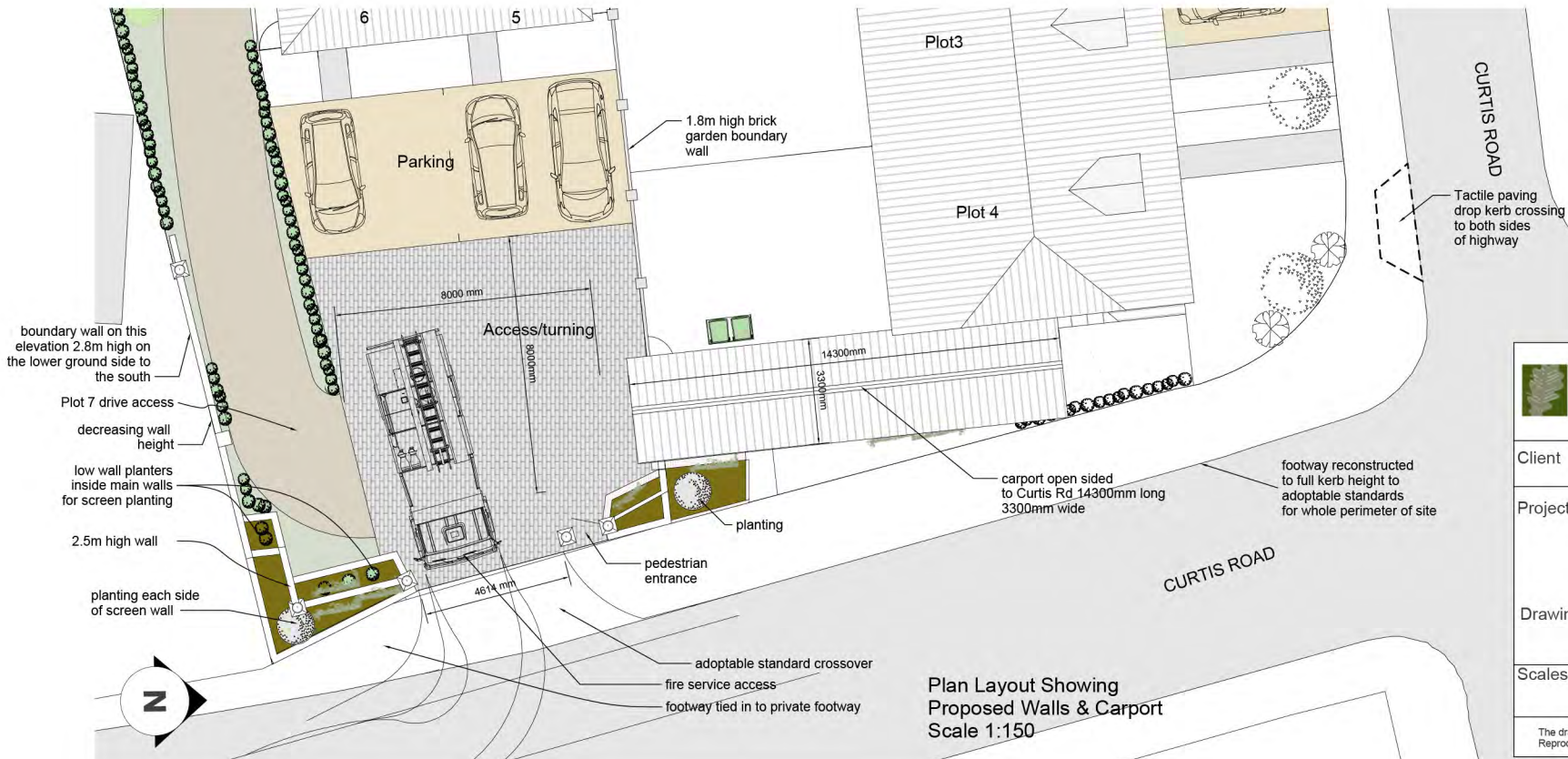
Recommendation

To approve application 21/00893/O and grant planning permission subject to the following conditions:

1. Standard time limit for approval of reserved matters;
2. In accordance with plans;
3. Contamination investigation;
4. Construction method statement;
5. Provision of sound insulation to habitable rooms;
6. Carport and boundary walls to be designed in accordance with noise impact assessment;
7. Compliance with arboricultural method statement;
8. Biodiversity enhancements to be agreed;
9. Small mammal access;
10. Detailed drawings for highway works;
11. Implementation of agreed highway works;
12. Parking and turning layout provided prior to first occupation;
13. Cycle and bin storage to be agreed and provided prior to first occupation;
14. Sustainable urban drainage system to be agreed;
15. Unidentified contamination;
16. Imported topsoil;
17. Water efficiency.

Informative notes:

1. Noise
2. Asbestos
3. Works within public highway.



Proposed s278 Small Highway Works Agreement to include

- Footway reconstructed to full kerb height and to adoptable standards
- Vehicle crossovers to site to adoptable standards
- Relocate any streetlights where necessary
- Replace and retain Street nameplate
- Provide tactile paving/drop to corner each side of highway
- Reinstate road drainage gullies
- Reinstate waiting restrictions
- Reinstate sign for single yellow line
- Tie in reinstated footway to the private footway to the south

Rev. B Tandem carport internal dimension changed to 14.0m x 3.0m
Rev. A southern boundary wall now 2.8 high



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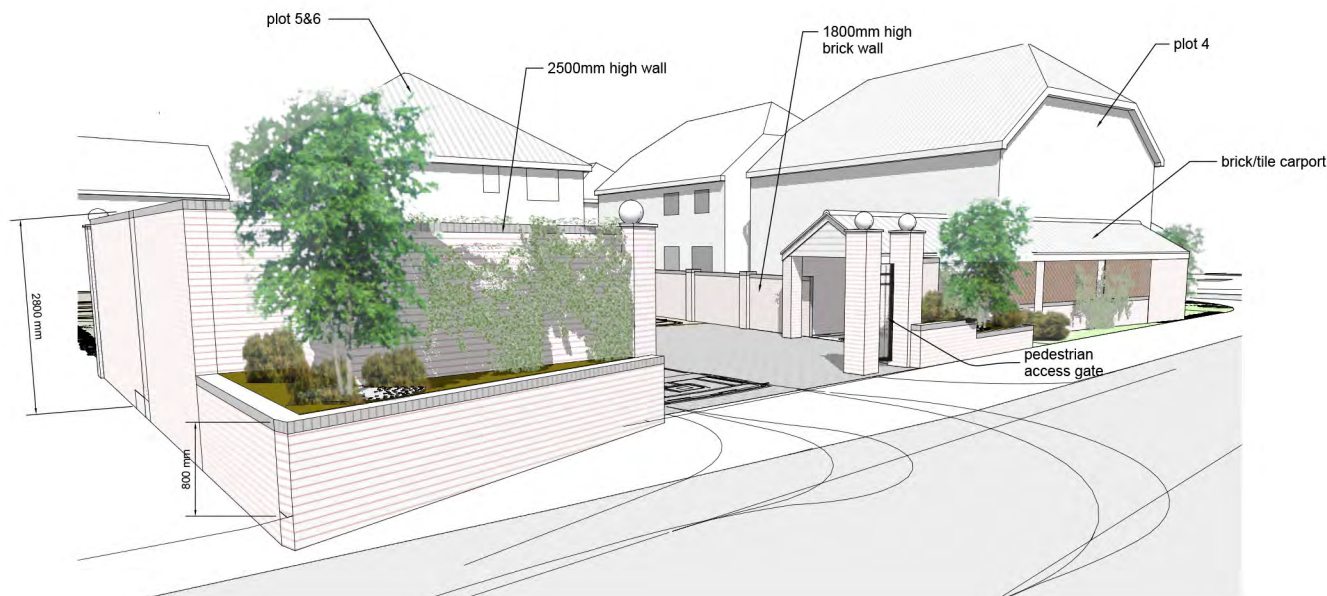
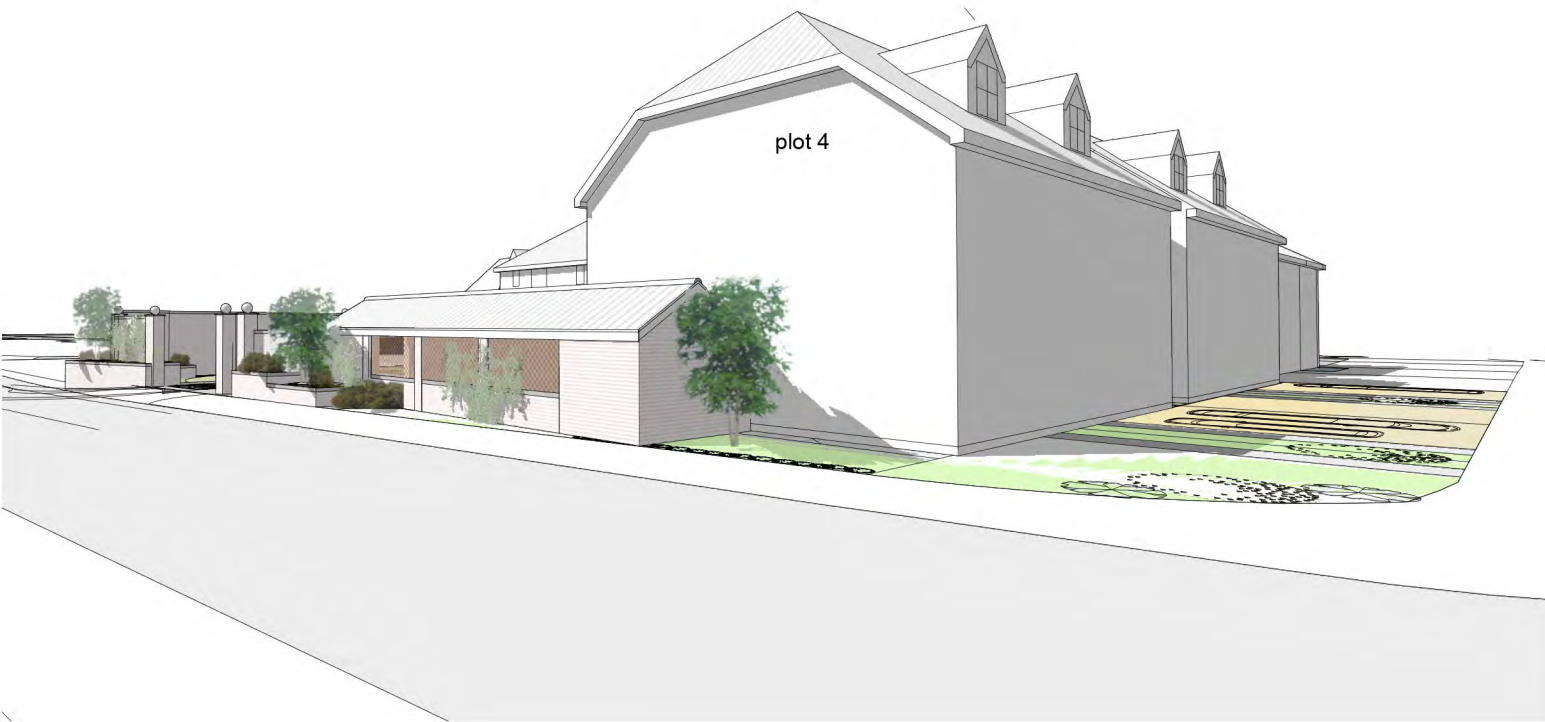
Client Eastern Retailing Ltd

Project Curtis Road Norwich
NR6 6RB
Redevelopment of Shop & Warehouse
to Provide 7 Two Storey Houses

Drawing Boundary Treatment /Acoustic
Screen Wall

Scales 1:100 & 1:150 @A3 Dwg. No 251120/20 B

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Client	Eastern Retailing Ltd	
Project	Curtis Road Norwich NR6 6RB Redevelopment of Shop & Warehouse to Provide 7 Two Storey Houses	
Drawing	Isometric View of Boundary Treatment /Acoustic Screen Wall	
Scales	NTS @ A3	Dwg. No 251120/21
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Curtis Road

- Proposed s278 Small Highway Works Agreement to include
- Footway reconstructed to full kerb height and to adoptable standards
 - vehicle crossovers to site to adoptable standards
 - Relocate any streetlights where necessary
 - Replace and retain Street nameplate
 - Provide tactile paving/drop to corner each side of highway
 - Reinstate road drainage gullies
 - Reinstate waiting restrictions
 - Reinstate sign for single yellow line
 - Tie in reinstated footway to the private footway to the south

adoptable standard footway crossings

foot path

tactile paving & drop kerb crossing

Reconstructed footway to perimeter of site to adoptable standards see S278 agreement

- Private paths 450 x 450mm pre-cast concrete paving slabs
- Private garden areas
- Permeable brick weave parking in contrasting colour
- permeable brick weave shared private access for plots 4,5,6 & 7
- Waste bin storage area pre-cast concrete paving slabs
- 1.8m high timber access gate with rear garden enclosed with close boarded timber fence
- Indicates dwelling front entrance

0m 1 2 3 4 5 6 7 8 9 10

New TRAD1 footpath crossing asphalt finish to adoptable standard

Fire appliance access to turning area suitable for minimum 12.5 tonnes load capacity allows plot 7 access within 45metres

for acoustic wall details see drawing 251120/20B

rev. F 23.03.22 TRAD 1 footpath crossing to car parking
rev. E 10.02.22 plot 4 carport & acoustic wall added
rev. D 23.8.21 - accommodation schedule updated
rev. C 4.07.2021 layout change plot 7 & fire service access shown
rev. B 22.4.2021 layout changes & plots reduced to 7
rev. A - drive access dimensions

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Project Curtis Road, Norwich
NR6 6RB
Redevelopment of Shop & Warehouse to Provide 7 Two Storey Houses

Drawing Site Plan

Scales 1: 200 @A3 Dwg. No 251120/03 F

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Site Plan 1:200

Accommodation Schedule

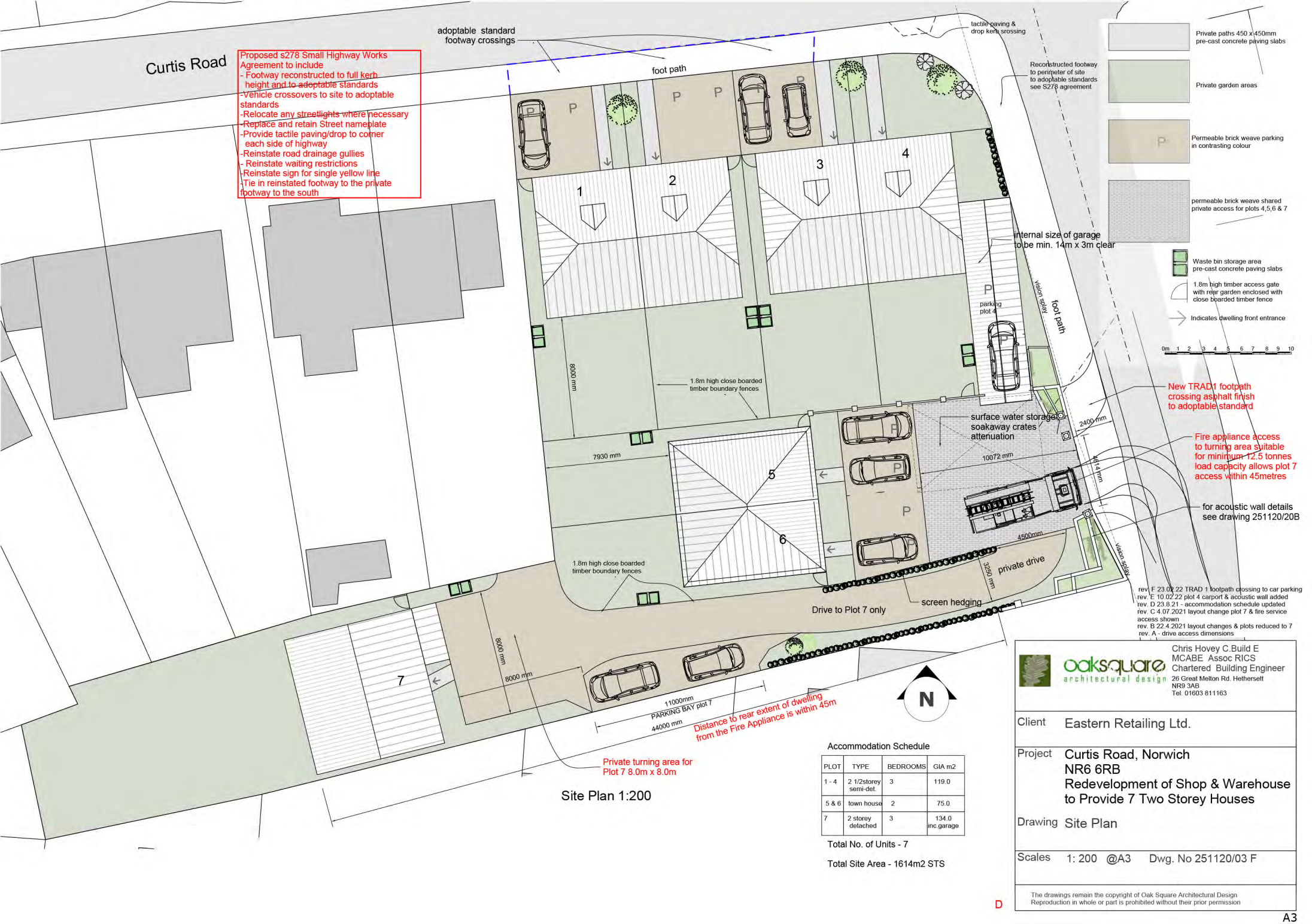
PLOT	TYPE	BEDROOMS	GIA m2
1 - 4	2 1/2storey semi-det.	3	119.0
5 & 6	town house	2	75.0
7	2 storey detached	3	134.0 inc garage

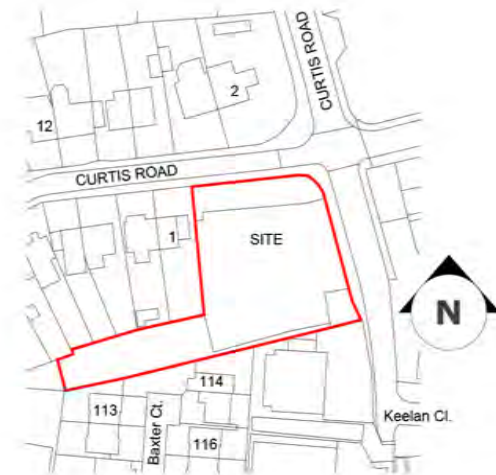
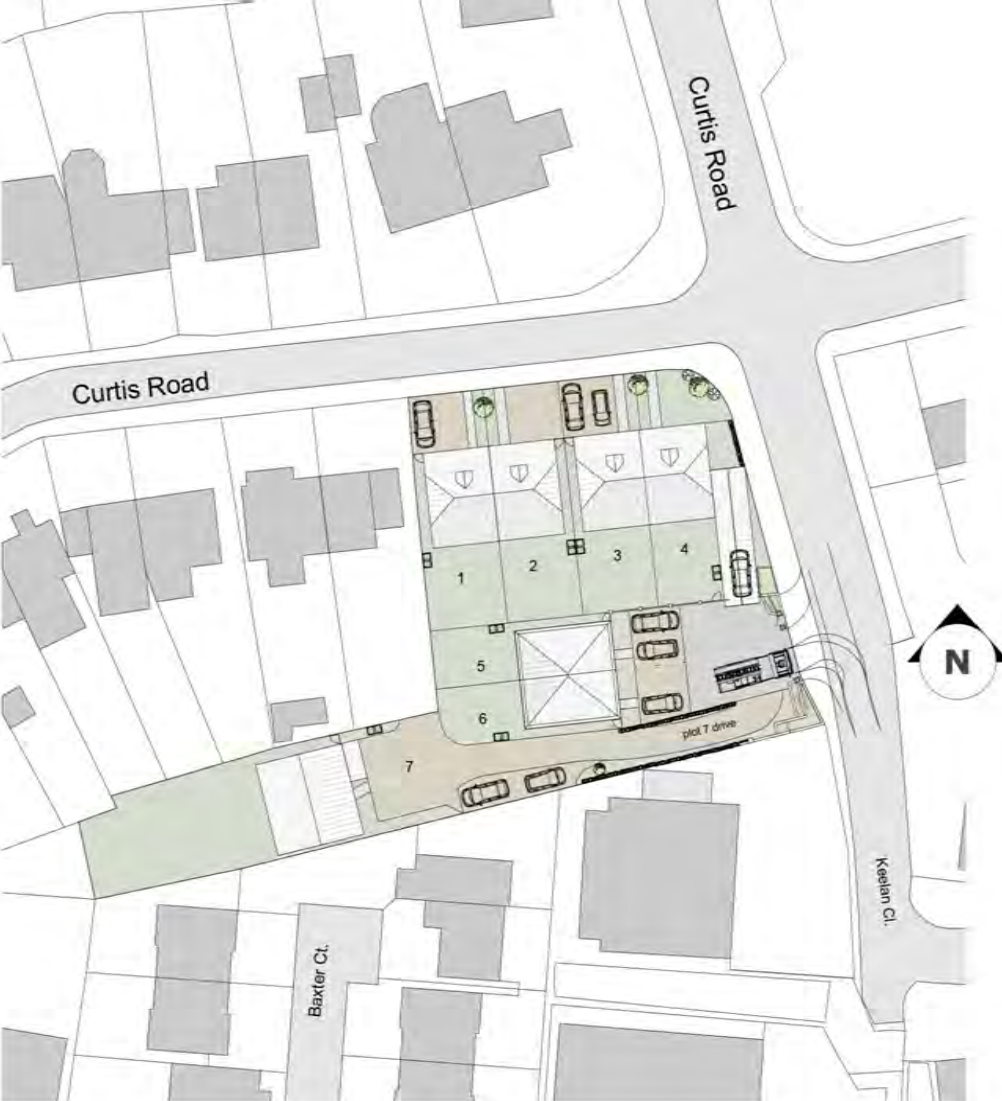
Total No. of Units - 7

Total Site Area - 1614m2 STS

Distance to rear extent of dwelling from the Fire Appliance is within 45m

Private turning area for Plot 7 8.0m x 8.0m





rev. C - 22.4.2021 Layout changes
rev. B - Layout
rev. A - drive access dimensions



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Client	Eastern Retailing Ltd.
Project	Curtis Road, Norwich NR6 6RB Redevelopment of Shop & Warehouse to Provide 7 Two Storey Houses
Drawing	Location & Block Plan
Scales	1:500 & 1:1250 @A3 Dwg. No 251120/01 C

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